Town of La Conner Planning Department
Received 10-17-23

IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA CONNER, WASHINGTON

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LEGEND

EXISTING RIGHT OF WAY LINE	PROPOSED PROPERTY BOUNDARY
ASBUILT ROAD CENTERLINE	PROPOSED ROAD RIGHT OF WAY
EXISTING ROAD CENTERLINE	PROPOSED ROAD CENTERLINE
EXISTING PROPERTY LINE	PROPOSED PROPERTY LINE (INTERIOR)
EXISTING PROPERTY CORNER	PROPOSED SIGN
EXISTING STORM DRAIN	PROPOSED STORM DRAIN
EXISTING TYPE 2 CB	PROPOSED STORM CATCH BASIN TYPE II
EXISTING TYPE 1 CB	PROPOSED STORM CATCH BASIN
EXISTING SANITARY SEWER	PROPOSED SANITARY SEWER
EXISTING SANITARY MANHOLE	PROPOSED SANITARY MANHOLE
EXISTING WATERLINE	PROPOSED SANITARY SEWER CLEANOUT
EXISTING WATER VALVE	PROPOSED WATERLINE
EXISTING FIRE HYDRANT	PROPOSED WATER VALVE
EXISTING GAS LINE	PROPOSED FIRE HYDRANT
EXISTING POWERLINE	PROPOSED REDUCER
EXISTING FENCELINE	PROPOSED BLOW-OFF ASSEMBLY
EXISTING EDGE OF ASPHALT	PROPOSED UTILITY TRENCH
EXISTING CURB & GUTTER	PROPOSED GAS LINE
EXISTING CONCRETE	PROPOSED POWERLINE
EXISTING ASPHALT	PROPOSED CURB AND GUTTER
EXISTING GRAVEL	PROPOSED CONCRETE
EXISTING CONTOUR	PROPOSED ASPHALT
PROPOSED CONTOUR	PROPOSED GRAVEL

LEGAL DESCRIPTION

THE EAST 3 FEET OF LOT 2 AND ALL OF LOTS 3, 6 AND 7, BLOCK 9, "CALHOUN ADDITION TO THE TOWN OF LA CONNER," AS PER PLAT RECORDED IN VOLUME 1 OF PLATS, PAGE 14, RECORDS OF SKAGIT COUNTY, WASHINGTON.

SITUATE IN THE COUNTY OF SKAGIT, STATE OF WASHINGTON.

NOTE: LEGAL DESCRIPTION FROM EXHIBIT "A" OF STATUTORY WARRANTY DEED RECORDED 08/26/2021 UNDER SKAGIT COUNTY AUDITOR'S FILE NUMBER 202108260088.

SITE INFORMATION

SITE PARCEL NUMBER: P74143
SITE ADDRESS: 306 CENTER STREET; LA CONNER, WA 98257

BUILDING UNITS: 5 AIRBNB LODGING UNITS, 14 APARTMENT DWELLING UNITS

UNDERGROUND UTILITY NOTE

UNDERGROUND UTILITY LOCATIONS SHOWN HEREON ARE BASED ON SURFACE INDICATORS, UTILITY MAPS PROVIDED BY THE CLIENT, SKAGIT COUNTY, AND APS UTILITY SERVICE WAS ALSO USED FOR THEIR LOCATION.

SURVEY/DATUM INFORMATION

BASIS OF BEARING
N 88° 25' 54" W BETWEEN THE FOUND NAILS IN THE INTERSECTIONS OF 3RD AND 4TH ST WITH CENTER ST.

FIELD EQUIPMENT
THIS SURVEY WAS ACCOMPLISHED BY FIELD TRAVERSE WITH A "TRIMBLE S5" AND A TRIMBLE R10-2 GPS RECEIVER, STANDARD ERROR DISTANCE +/- 2CM (+1 PPM), AND MEETS OR EXCEEDS STANDARDS AS SET FORTH IN W.A.C. CH. 332-130.

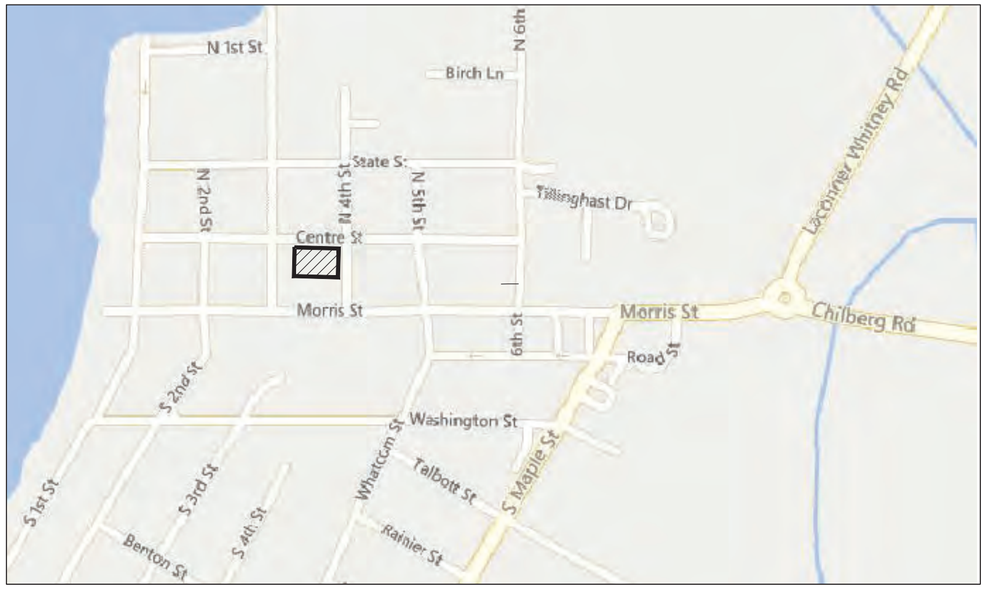
HORIZONTAL DATUM
WASHINGTON STATE PLANE NORTH ZONE 4601 (NAD 83/2012) USING W.S.R.N.

VERTICAL DATUM
THE VERTICAL DATUM FOR THIS PROJECT IS NAVD 88 BASED ON W.S.R.N. GPS TIE. THE ELEVATION OF THE SOUTHWEST TOP HOLD DOWN BOLT ON THE FIRE HYDRANT AT THE INTERSECTION OF 4TH ST. AND CENTER ST. HAS AN ELEVATION OF 9.47' NAVD AS SHOWN HEREON.

BASE FLOOD ELEVATION = 8.0' NGVD 29 + 3.8' = 11.8' NAVD 88

UNDERGROUND UTILITY LOCATIONS
UNDERGROUND UTILITY LOCATIONS SHOWN HEREON ARE BASED ON SURFACE INDICATORS.

RESEARCH
1) R.O.S. AFN 9407190146
2) R.O.S. AFN 200904210003
PLAT OF CALHOUN ADDITION TO THE TOWN OF LACONNER VOL. 1, PAGE 14



VICINITY MAP
NOT TO SCALE

UTILITIES SERVING THE SITE

WATER:
LA CONNER PUBLIC WORKS
CONTACT: BRIAN LEASE
(360) 466-3933

POWER:
PUGET SOUND ENERGY
CONTACT: MIKE JUDY
(425) 324-0223

CABLE:
COMCAST
CONTACT: SHANE TURNER
(360) 316-9405

TELEPHONE:
ZIPLY FIBER
CONTACT: DENNIS KELLER
(360) 757-4530

GAS:
CASCADE NATURAL GAS
CONTACT: TED McCAMMANT
(360) 708-4689

SANITARY SEWER:
TOWN OF LA CONNER
CONTACT: BRIAN LEASE
(360) 466-3933

PERMIT QUANTITIES

THE FOLLOWING GRADING QUANTITIES ARE FOR PERMITTING PURPOSES ONLY AND ARE NOT TO BE USED IN THE BIDDING PROCESS:

(PARCEL ONLY)
CUT/STRIP: 850 CY FILL: 900 CY
(ROW ONLY)
CUT/STRIP: 600 CY FILL: 600 CY
(PARCEL + ROW)
CUT/STRIP: 1450 CY FILL: 1500 CY

IMPERVIOUS SURFACE COVERAGE
(PARCEL ONLY)
EXISTING: 5967 SF - 39.0%
PROPOSED: 12012 SF - 78.5%
(ROW ONLY)
EXISTING: 14937 SF - 60.0%
PROPOSED: 16483 SF - 65.9%
(PARCEL + ROW)
EXISTING: 20904 SF - 51.9%
PROPOSED: 28495 SF - 70.7%

TOWN OF LA CONNER GENERAL NOTES

1. ALL CONSTRUCTION SHALL CONFORM TO THE REQUIREMENTS OF THE TOWN OF LA CONNER THAT ARE CONTAINED IN A BOUND VOLUME ENTITLED "INFRASTRUCTURE IMPROVEMENTS PROJECT MANUAL."
2. THE LOCATION OF THE EXISTING UTILITIES SHOWN ON THE DRAWING IS APPROXIMATE. LOCATION AND PROTECTION OF UNDERGROUND UTILITIES SHALL BE IN ACCORDANCE WITH CHAPTER 19.122 RCW. CALL 800-424-5555 AT LEAST TWO BUSINESS DAYS BEFORE ANY EXCAVATION.
3. REPLACE ANY DAMAGED OR DESTROYED MONUMENTS.
4. THIS PLAN AND PROFILE INFORMATION HAS BEEN FURNISHED BY THE DEVELOPER OR HIS ENGINEER.
5. MINIMUM SEPARATION BETWEEN SANITARY SEWERS AND POTABLE WATER LINES SHALL BE 10 FEET HORIZONTALLY (MEASURED SIDE TO SIDE) AND 18 INCHES VERTICALLY FROM BOTTOM OF WATERLINE TO CROWN OF SEWER. MINIMUM VERTICAL SEPARATION FOR PERPENDICULAR OR OBLIQUE CROSSINGS SHALL BE 3 FEET FROM OUTSIDE EDGES. SITUATIONS THAT REQUIRE LESS THAN MINIMUM SEPARATION SHALL BE CONSTRUCTED IN ACCORDANCE WITH DOE'S "CRITERIA FOR SEWAGE WORKS DESIGN", LATEST EDITION.
6. THE DEVELOPER SHALL BE RESPONSIBLE FOR ACQUIRING ALL NECESSARY EASEMENTS AND AGREEMENTS PRIOR TO CONSTRUCTION.
7. A PLUG SHALL BE PLACED IN THE OUTLET PIPE OF THE EXISTING MANHOLE TO WHICH NEW PIPE IS TO BE CONNECTED OR IN THE OUTLET OF THE FIRST NEW MANHOLE. THIS PLUG SHALL REMAIN IN PLACE AND MAY NOT BE REMOVED WITHOUT THE PERMISSION OF THE TOWN OF LA CONNER. REMOVAL WILL RESULT IN FORFEITURE OF THE SYSTEM ISOLATION DEPOSIT.\

CAUTION - EXTREME HAZARD - OVERHEAD ELECTRICAL SERVICE LINES ARE GENERALLY NOT SHOWN ON THE DRAWINGS. ELECTRICAL LINES SHOWN ON THE DRAWINGS ARE LOCATED BY POINT-TO-POINT, POWER-POLE-TO-POWER-POLE CONNECTION. THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXTENT OF ANY HAZARD CREATED BY OVERHEAD ELECTRICAL POWER IN ALL AREAS AND SHALL FOLLOW PROCEDURES DURING CONSTRUCTION AS REQUIRED BY LAW AND REGULATION. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL MEET WITH UTILITY OWNERS AND DETERMINE THE EXTENT OF HAZARD AND REMEDIAL MEASURES, AND SHALL TAKE WHATEVER PRECAUTIONS MAY BE REQUIRED.

UNDERGROUND FIRE LINE REQUIREMENTS:

1. THE ENTIRE UNDERGROUND FIRE LINE WILL REQUIRE A FULL HYDRO-TEST AT 200 PSI FOR TWO (2) HOURS. CONTACT THE FIRE MARSHAL BEFORE PERFORMING THIS TEST.
2. EACH FIRE HYDRANT AND FDC RISER ON THE FIRE SYSTEM WILL REQUIRE A ONE (1) MINUTE FLUSH FROM THE PUMPER PORT OR MANIFOLD WITH A DIFFUSER OR UNTIL THERE ARE NO PARTICULATES IN THE SYSTEM.
3. THE PUMPER PORT ON THE HYDRANTS SHALL HAVE A FIVE (5) INCH STORTZ HARD FACED COUPLER INSTALLED PRIOR TO FINAL APPROVAL.
4. A COPY OF NFPA U FORM FILLED OUT MUST BE PROVIDED TO THE FIRE MARSHAL PRIOR TO FINAL.
5. ALL FITTINGS AND PIPING FROM THE VAULT TO THE BUILDING WILL NEED TO BE INSPECTED BY THE FIRE MARSHAL PRIOR TO COVER.
6. THREE (3) SETS OF PLANS, MATERIAL CUT SHEETS AND FIRE FLOW CALCULATIONS WILL NEED TO BE APPROVED PRIOR TO ANY WORK BEING DONE.
7. ALL WORK BEING DONE FROM THE DOUBLE CHECK VALVE ASSEMBLY TO THE RISER WILL NEED TO BE INSTALLED BY A U LICENSED CONTRACTOR.
8. THE FIRE LINE MUST BE BACKFILLED WITH EITHER SAND OR PEA GRAVEL. NATIVE MATERIALS CANNOT BE USED AS BACKFILL MATERIALS.

OWNER:
KSA INVESTMENTS, LLC
BRANDON ATKINSON
16559 COUNTRY CLUB DRIVE
BURLINGTON, WA 98233
EM: brandon.kate.atkinson@gmail.com

CONTRACTOR:
FABER CONSTRUCTION
DALE KING
6951 HANNEGAN ROAD
LYNDEN, WA 98264
PH: 360.354.3500
EM: dale@faberconstruction.com

ENGINEER/SURVEYOR:
SOUND DEVELOPMENT GROUP, LLC
PAT SEVERIN / JAY STANDISH
1111 CLEVELAND AVENUE, SUITE 202
MOUNT VERNON, WA 98273
PH: 360.404.2010
EM: pat@sdg-llc.com / jay@sdg-llc.com

ARCHITECT:
CHARLES MORGAN & ASSOCIATES, LLC
CHARLIE MORGAN / MISTY KLINCK
7301 BEVERLY LANE
EVERETT, WA 98203
PH: 425.353.2888
EM: charliejr@cmaarch.com / misty@cmaarch.com

LANDSCAPE ARCHITECT:
ECCOSDESIGN, LLC
PATRIK DYLAN, PLA
505 SOUTH FIRST STREET, SUITE B
MOUNT VERNON, WA 98273
PH: 360.419.7400
EM: eccosdesign@gmail.com

Sound Development Group
ENGINEERING, SURVEYING & LAND DEVELOPMENT SERVICES
P.O. Box 1705 • 1111 Cleveland Avenue, Suite 202
Mount Vernon, WA 98273 Tel: 360-404-2010

SHEET REVISIONS:

NO.	DATE	DESCRIPTION	APPROVED
1	9.22.23	ARCHITECT REVISION	P.L.S.

CALL 48 HOURS BEFORE YOU DIG 1.800.424.5555

PROJECT:
CENTER STREET MIXED-USE
FOR
KSA INVESTMENTS, LLC

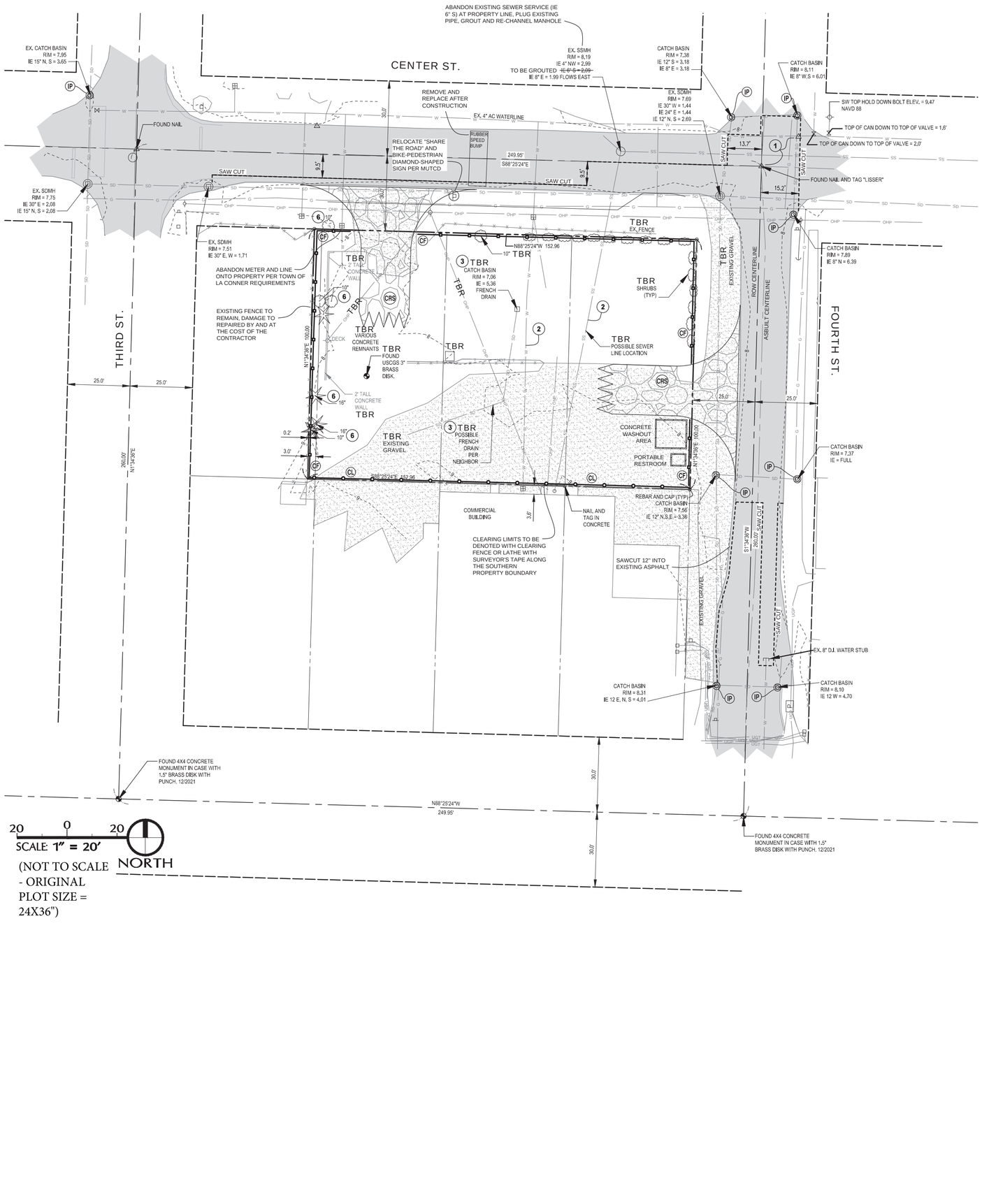
SHEET DESCRIPTION:
COVER SHEET

PATRICK L. SEVERIN
STATE OF WASHINGTON
32226
REGISTERED
PROFESSIONAL ENGINEER

SCALE:	NO SCALE
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG

SHEET NUMBER:
C1.0

IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA CONNER, WASHINGTON



(NOT TO SCALE - ORIGINAL PLOT SIZE = 24X36")

TESC PLAN LEGEND

CODE	SYMBOL	DESCRIPTION
CRS		CONSTRUCTION ROAD STABILIZATION, PER DETAIL A/1.2, IF EXISTING GRAVEL IS NOT SUFFICIENT FOR SEDIMENT REMOVAL FROM WHEELS. INSTALL RUMBLE STRIP PLATES AT ENTRANCE AND EXIT, AS REQUIRED BY THE FILL, GRADE AND/OR CLEARING PERMIT APPROVED ON 8/15/23 BY THE TOWN OF LA CONNER
CF		ORANGE CONSTRUCTION FENCE (BMP C103) OR EQUIVALENT HIGH VISIBILITY FENCE AT CESCL DISCRETION DURING DRY SEASON, SILT FENCE DURING WET SEASON
CL		CLEARING LIMITS - CLEARING / ORANGE CONSTRUCTION FENCE (BMP C103) OR LATHE AND SURVEYOR'S TAPE
VEG	VEG	RETAIN EXISTING VEGETATION
IP		INLET PROTECTION PER DETAIL B/1.2. TO BE INSTALLED ON ALL PROPOSED AND EXISTING CBs WITH OPEN GRATES
SP	SP	SILT PROTECTION - STRAW WADDLES
PS	PS	PERMANENT SEEDING AND PLANING, BMP C120 AND BMP T5.13
DC	DC	DUST CONTROL
TBR		TO BE REMOVED

TESC NOTES:

1. SILT FENCE SHALL BE INSTALLED IF WARRANTED BY SITE CONDITIONS AS DETERMINED BY THE PROJECT CESCL, ENGINEER OR CITY INSPECTOR. THE SILT FENCE SHALL BE PLACED AS REQUIRED TO PREVENT SILT LADEN RUNOFF FROM LEAVING THE SITE. SEE DETAIL C/2.1 FOR SILT FENCE INSTALLATION.
2. TRENCH SPOILS ARE TO BE PLACED ON THE UPHILL SIDE OF THE TRENCH WHERE FEASIBLE.
3. WHEN FEASIBLE, NO MORE THAN 500 LF OF TRENCH SHALL BE OPENED AT ONE TIME. TRENCH SPOILS ARE TO BE PLACED ON THE UPHILL SIDE OF THE TRENCH; ALL DEWATERING, ALTHOUGH NOT EXPECTED, WILL BE DISCHARGED TO THE SEDIMENT TRAP. SEDIMENT TRAP, IF NEEDED, IS TO BE CONSTRUCTED PER DETAILS ON SHEET C3.4 WITH A SEDIMENT TRAP SURFACE AREA OF 35 SF.
4. CONSTRUCTION ACCESS WILL BE PROVIDED THROUGH THE TWO EXISTING GRAVELED ACCESS POINTS, ONE AS AN ENTRANCE AND THE OTHER AS AN EXIT. SHOULD THE EXISTING GRAVEL AREAS NOT RETAIN SEDIMENTS FROM VEHICLE WHEELS, A CONSTRUCTION ACCESS IS TO BE INSTALLED PER DETAIL A/1.2. REFER TO THE TESC PLAN NOTES ON SHEET C1.2 FOR INFORMATION ON STREET SWEEPING AND WASHING.
5. ALL BMP'S ARE TO BE REMOVED WITHIN 30 DAY'S AFTER FINAL SITE STABILIZATION.
6. NO CONCRETE TRUCKS WILL BE ALLOWED TO WASHOUT ON-SITE. THE CONCRETE WASHOUT AREA HAS BEEN DENOTED FOR CONCRETE POURING AND FINISHING TOOLS ONLY.
7. NO VEHICLE MAINTENANCE IS ALLOWED ON-SITE.
8. ALL BMP'S SHALL BE INSPECTED AND MAINTAINED ON A REGULAR BASIS, AS DETERMINED BY THE TIME OF YEAR AND RAINFALL EVENTS.
9. INLET PROTECTION IS TO BE INSTALLED ON ALL STORM STRUCTURES WITH OPEN GRATES WITHIN 300' OF THE SITE, WHICH MAY NOT BE SHOWN ON THE PLANS.

EXISTING CONDITIONS NOTES

1. CONTRACTOR TO MINIMIZE IMPACT TO ALL FOUND MONUMENTS AND TO SCHEDULE RESTORATION OF MONUMENT BY A LICENSED PROFESSIONAL LAND SURVEYOR AT THE COMPLETION OF CONSTRUCTION, AS NECESSARY AND APPROPRIATE. EXISTING USCGS 3" BRASS DISK IS TO BE REMOVED AND WILL NOT BE REPLACED, AS PRE-AUTHORIZED BY USCGS.
2. CONTRACTOR TO POTHOLE AND CONFIRM UTILITY LINES DO NOT SERVE OTHER PROPERTIES. IF NOT, CONTRACTOR TO CALL FOR DISCONNECTION, CAPPING AND ABANDONMENT/REMOVAL OF UTILITIES ENTERING THE SITE FROM THE NORTH AND SOUTH PROPERTY LINES. UNDERGROUND AND ABOVE-GROUND UTILITIES ARE TO BE DISCONNECTED AT THE DIRECTION OF THE ASSOCIATED UTILITY PURVEYOR. ALL NECESSARY WORK (TRENCHING, BACKFILL, COMPACTION, ETC) IS THE RESPONSIBILITY OF THE CONTRACTOR, UNLESS UTILITY PURVEYOR SPECIFIES OTHERWISE.

   WATER AND SANITARY SEWER LINES ARE TO REMAIN LIVE UNTIL UTILITY IMPROVEMENTS ARE MADE ON FOURTH STREET, PER UTILITY PLAN, AND SERVICE IS RESTORED TO THE EXISTING LINES AND METER.
3. CONTRACTOR TO POTHOLE STORM LINE AND CONFIRM IF THE STORM LINE/TRENCH DRAIN IS LIMITED TO THE SUBJECT PROPERTY. SHOULD THE FEATURE BE LIMITED TO THIS PROPERTY, THE STORM LINE AND TRENCH DRAIN IS TO BE REMOVED. SHOULD THE FEATURE NOT BE LIMITED TO THE SUBJECT PROPERTY, THE ENGINEER IS TO BE CONTACTED IMMEDIATELY FOR RE-ROUTING PLANS.
4. CONTRACTOR TO LOCATE ALL NEXT INLINE VALVES PRIOR TO CONSTRUCTION AND/OR WORK ON WATER SYSTEM.
5. CONTRACTOR TO COORDINATE ALL UTILITY AND ROAD SHUT-DOWN PERIODS WITH THE TOWN OF LA CONNER, EMERGENCY DEPARTMENTS, UTILITY PURVEYORS AND AFFECTED PROPERTIES (COMMERCIAL AND RESIDENTIAL). CONTRACTOR TO PROVIDE TRAFFIC CONTROL PLAN, APPROVED BY THE TOWN OF LA CONNER, PRIOR TO BEGINNING CONSTRUCTION.
6. LANDSCAPING AND EXISTING SHRUBS ALONG THE WESTERN PROPERTY BOUNDARY ARE TO BE REMOVED. CONTRACTOR TO COORDINATE WITH NEIGHBORS SHOULD EXISTING FEATURES, THAT ARE TO REMAIN (I.E. FENCE LINES), ARE AFFECTED DURING THE REMOVAL PROCESS.

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1	9.22.23	ARCHITECT REVISION	P.L.S.

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PROJECT:
CENTER STREET MIXED-USE
FOR
KSA INVESTMENTS, LLC

SHEET DESCRIPTION:
EXISTING CONDITIONS, DEMOLITION AND TESC PLAN

PATRICK L. SEVERIN STATE OF WASHINGTON 32226 REGISTERED PROFESSIONAL ENGINEER

SCALE:	1" = 20'
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG

SHEET NUMBER:
C1.1

IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M.   LA CONNER, WASHINGTON

ESC-1 STANDARD ESC NOTES

APPROVAL OF THIS EROSION AND SEDIMENT CONTROL (ESC) PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G., SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).

1. AS DIRECTED BY PUBLIC WORKS DIRECTOR PRIOR TO COMMENCING CONSTRUCTION, ALL CRITICAL AREAS, INCLUDING WETLAND BUFFERS, STREAM CORRIDOR, LANDFILL AREAS, AND STEEP SLOPES SHALL BE CONTINUOUSLY DEMARCATED IN THE FIELD USING FLAGGING TAPE OR FENCING.
2. EROSION CONTROL METHODS AND MATERIALS SHALL MEET REQUIREMENTS OF SECTION 8-01 OF THE APWA/WASHINGTON STATE DEPARTMENT OF TRANSPORTATION 2022 STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION, AND REQUIREMENTS SET FORTH IN VOLUME ii OF THE "STORMWATER MANAGEMENT MANUAL FOR THE PUGET SOUND BASIN (THE TECHNICAL MANUAL)", BY THE WASHINGTON STATE DEPARTMENT OF ECOLOGY, CURRENT EDITION. THE CONTRACTOR SHALL FOLLOW RECOMMENDATIONS MADE BY SUPPLIERS AND MANUFACTURERS OF MATERIALS AND EQUIPMENT USED.
3. THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES IN SUCH A MANNER AS TO ENSURE THAT SEDIMENT-LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM OR VIOLATE APPLICABLE WATER STANDARDS, AND MUST BE INSTALLED AND IN OPERATION PRIOR TO ANY GRADING OR LAND CLEARING. WHEREVER POSSIBLE, MAINTAIN NATURAL VEGETATION FOR SILT CONTROL.
4. THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED (E.G., ADDITIONAL SUMPS, RELOCATION OF DITCHES AND SILT FENCES, ETC.) AS NEEDED FOR UNEXPECTED STORM EVENTS. ADDITIONALLY, MORE ESC FACILITIES MAY BE REQUIRED TO ENSURE COMPLETE SILTATION CONTROL. THEREFORE, DURING THE COURSE OF CONSTRUCTION, IT SHALL BE THE OBLIGATION AND RESPONSIBILITY OF THE CONTRACTOR TO ADDRESS ANY NEW CONDITIONS THAT MAY BE CREATED BY HIS ACTIVITIES AND TO PROVIDE ADDITIONAL FACILITIES OVER AND ABOVE THE MINIMUM REQUIREMENTS AS MAY BE NEEDED.
5. ANY PERMANENT RETENTION/DETENTION FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE PERMANENT FACILITY IS TO FUNCTION ULTIMATELY AS AN INFILTRATION OR DISPERSION SYSTEM, THE FACILITY SHALL NOT BE USED AS A TEMPORARY SETTLING BASIN. NO UNDERGROUND DETENTION TANK, DETENTION VAULT, OR SYSTEM, WHICH BACKS UNDER OR INTO A POND, SHALL BE USED AS A TEMPORARY SETTLING BASIN.
6. WHERE SEEDING FOR TEMPORARY EROSION CONTROL IS REQUIRED, FAST GERMINATING GRASSES SHALL BE APPLIED AT AN APPROPRIATE RATE (EXAMPLE: ANNUAL OR PERENNIAL RYE APPLIED AT APPROXIMATELY 80 POUNDS PER ACRE).
7. WHERE STRAW MULCH IS REQUIRED FOR TEMPORARY EROSION CONTROL, IT SHALL BE APPLIED AT A MINIMUM THICKNESS OF TWO (2) INCHES.
8. ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH THE TOWN OF LA CONNER STANDARDS AND SPECIFICATIONS.
9. THE ESC FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAILS ON THE APPROVED PLANS. LOCATIONS MAY BE MOVED TO SUIT FIELD CONDITIONS, SUBJECT TO APPROVAL BY THE TOWN OF LA CONNER PUBLIC WORKS DIRECTOR AND TOWN OF LA CONNER INSPECTOR.
10. A COPY OF THE APPROVED EROSION CONTROL PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
11. ALL LOTS ADJOINING OR HAVING ANY NATIVE GROWTH PROTECTION EASEMENTS (NGPE) SHALL HAVE A FOUR (4) FOOT HIGH TEMPORARY CONSTRUCTION FENCE (CYCLONE OR PLASTIC MESH) SEPARATING THE LOT (OR BUILDABLE PORTIONS OF THE LOT) FROM THE AREA RESTRICTED BY THE NGPE AND SHALL BE INSTALLED PRIOR TO ANY GRADING OR CLEARING AND REMAIN IN PLACE UNTIL A DWELLING IS CONSTRUCTED AND OWNERSHIP TRANSFERRED TO THE FIRST OWNER/OCCUPANT.
12. A CLEARING CONTROL FENCE SHALL BE INSTALLED AT THE DRIP LINE OF TREES TO BE SAVED WHEREVER THE TREE CANOPIES EXTEND INTO THE AREA TO BE CLEARED. ON-SITE SUPERVISION SHALL BE PROVIDED WHENEVER EQUIPMENT MUST OPERATE UNDER TREE CANOPIES, AND FENCING SHALL BE REPLACED WHEN NECESSARY. THE CLEARING CONTROL FENCE SHALL CONSIST OF A SINGLE STRAND OF WIRE ATTACHED ABOUT THREE (3) FEET HIGH TO WOOD LATH AT A TWELVE (12) FOOT MAXIMUM SPACING AND FLAGGED WITH SURVEY RIBBON AT FOUR (4) FOOT INTERVALS.
13. OFF-SITE STREETS MUST BE KEPT CLEAN AT ALL TIMES. IF DIRT IS DEPOSITED ON THE PUBLIC STREET SYSTEM, THE STREET SHALL BE IMMEDIATELY CLEANED WITH A POWER SWEEPER OR OTHER EQUIPMENT. ALL VEHICLES SHALL LEAVE THE SITE BY WAY OF THE CONSTRUCTION ENTRANCE AND SHALL BE CLEANED OF ALL DIRT THAT WOULD BE DEPOSITED ON THE PUBLIC STREETS.
14. ANY CATCH BASINS COLLECTING RUNOFF FROM THE SITE, WHETHER THEY ARE ON OR OFF THE SITE, SHALL HAVE THEIR GRATES COVERED WITH FILTER FABRIC DURING CONSTRUCTION.
15. THE WASHED GRAVEL BACKFILL ADJACENT TO THE FILTER FABRIC FENCE SHALL BE REPLACED AND THE FILTER FABRIC CLEANED IF IT IS NONFUNCTIONAL BY EXCESSIVE SILT ACCUMULATION AS DETERMINED BY THE TOWN OF LA CONNER. ALSO, ALL INTERCEPTOR SWALES SHALL BE CLEANED IF SILT ACCUMULATION EXCEEDS ONE-QUARTER DEPTH.
16. ROCK FOR EROSION PROTECTION OF ROADWAY DITCHES, WHERE REQUIRED, MUST BE OF SOUND QUARRY ROCK, PLACED TO DEPTH OF ONE (1) FOOT AND MUST MEET THE FOLLOWING SPECIFICATIONS: 4"-8" ROCK/40%-70% PASSING; 2"-4" ROCK/30%-40% PASSING; AND 1"-2" ROCK/10%-20% PASSING.
17. IF ANY PART(S) OF THE CLEARING LIMIT, BOUNDARY OR TEMPORARY EROSION/SEDIMENTATION CONTROL PLAN IS/ARE DAMAGED, IT SHALL BE REPAIRED IMMEDIATELY.
18. ALL PROPERTIES ADJACENT TO THE PROJECT SITE SHALL BE PROTECTED FROM SEDIMENT DEPOSITION AND RUNOFF. DO NOT FLUSH CONCRETE BY-PRODUCTS OR TRUCKS NEAR OR INTO THE STORM DRAINAGE SYSTEM. IF EXPOSED AGGREGATE IS FLUSHED INTO THE STORM SYSTEM, IT COULD MEAN RECLEANING THE ENTIRE DOWNSTREAM STORM SYSTEM, OR POSSIBLY RELAYING THE STORM LINE.
19. THE IMPLEMENTATION OF THESE ESC PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT AND UPGRADING OF THESE FACILITIES IS THE RESPONSIBILITY OF THE APPLICANT/ESC SUPERVISOR UNTIL ALL CONSTRUCTION IS APPROVED.
20. THE ESC SUPERVISOR NAME IS: ______________________.
24 HR. CONTACT NUMBER IS: ______________________.
21. THE APPLICANT'S NAME IS: ______________________.
24 HR. CONTACT NUMBER IS: ______________________.
22. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED BY A CONTINUOUS LENGTH OF SURVEY TAPE (OR FENCING, IF REQUIRED) PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE CLEARING LIMITS SHALL BE PERMITTED. THE CLEARING LIMITS SHALL BE MAINTAINED BY THE APPLICANT/ESC SUPERVISOR FOR THE DURATION OF CONSTRUCTION.
23. THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/ESC SUPERVISOR AND MAINTAINED TO ENSURE CONTINUED PROPER FUNCTIONING. WRITTEN RECORDS SHALL BE KEPT OF WEEKLY REVIEWS OF THE ESC FACILITIES DURING THE WET SEASON (OCT. 1 TO APRIL 30) AND OF THE MONTHLY REVIEWS DURING THE DRY SEASON (MAY 1 TO SEPT. 30).
24. ANY AREAS OF EXPOSED SOILS, INCLUDING ROADWAY EMBANKMENTS, THAT WILL NOT BE DISTURBED FOR TWO DAYS DURING THE WET SEASON OR SEVEN DAYS DURING THE DRY SEASON SHALL BE IMMEDIATELY STABILIZED WITH THE APPROVED ESC METHODS (E.G., SEEDING, MULCHING, PLASTIC COVERING, ETC.)
25. ANY AREA NEEDING ESC MEASURES NOT REQUIRING IMMEDIATE ATTENTION SHALL BE ADDRESSED WITHIN FIFTEEN (15) DAYS.
26. THE ESC FACILITIES ON ACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN FORTY-EIGHT (48) HOURS FOLLOWING A STORM EVENT.
27. AT NO TIME SHALL MORE THAN ONE (1) FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO THE DOWNSTREAM SYSTEM.
28. OFF-SITE STORMWATER AND/OR GROUNDWATER TO BE DIVERTED AWAY FROM SLOPES & DISTURBED AREAS WITH INTERCEPTOR DIKES, PIPES, OR SWALES. OFF-SITE STORMWATER SHALL BE MANAGED DIFFERENTLY FROM STORMWATER GENERATED ON-SITE.
29. EXCAVATED MATERIAL TO BE PLACED ON UPHILL SIDE OF TRENCH.
30. STABILIZED CONSTRUCTION ENTRANCES AND ROADS SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES, SUCH AS WASH PADS, MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
31. ANY PERMANENT FLOW CONTROL FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE FACILITY IS TO FUNCTION AS ULTIMATELY AS AN INFILTRATION SYSTEM, THE TEMPORARY FACILITY MUST BE GRADED SO THAT THE BOTTOM AND SIDES ARE AT LEAST THREE FEET ABOVE THE FINAL GRADE OF THE PERMANENT FACILITY.
32. PRIOR TO THE BEGINNING OF THE WET SEASON (OCT. 1), ALL DISTURBED AREAS SHALL BE REVIEWED TO IDENTIFY WHICH ONES CAN BE SEEDED IN PREPARATION FOR THE WINTER RAINS. DISTURBED AREAS SHALL BE SEEDED WITHIN ONE WEEK OF THE BEGINNING OF THE WET SEASON. A SKETCH MAP OF THOSE AREAS TO BE SEEDED AND THOSE AREAS TO REMAIN UNCOVERED SHALL BE SUBMITTED TO THE TOWN OF LA CONNER INSPECTOR. THE TOWN OF LA CONNER INSPECTOR CAN REQUIRE SEEDING OF ADDITIONAL AREAS IN ORDER TO PROTECT SURFACE WATERS, ADJACENT PROPERTIES, OR DRAINAGE FACILITIES.

ESC-2 WET SEASON SPECIAL PROVISIONS
(OCTOBER 1 - APRIL 30)

1. THE ALLOWED TIME THAT A DISTURBED AREA CAN REMAIN UNWORKED WITHOUT COVER MEASURES IS REDUCED TO TWO DAYS, RATHER THAN SEVEN.
2. STOCKPILES AND STEEP CUT AND FILL SLOPES ARE TO BE PROTECTED IF UNWORKED FOR MORE THAN 12 HOURS.
3. COVER MATERIALS SUFFICIENT TO COVER ALL DISTURBED AREAS SHALL BE STOCKPILED ON SITE.
4. ALL AREAS THAT ARE TO BE UNWORKED DURING THE WET SEASON SHALL BE SEEDED WITHIN ONE WEEK OF THE BEGINNING OF THE WET SEASON.
5. MULCH IS REQUIRED TO PROTECT ALL SEEDED AREAS.
6. FIFTY LINEAR FEET OF SILT FENCE (AND THE NECESSARY STAKES) PER ACRE OF DISTURBANCE MUST BE STOCKPILED ON SITE.
7. CONSTRUCTION ROAD AND PARKING LOT STABILIZED ARE REQUIRED FOR ALL SITES UNLESS THE SITE IS UNDERLAIN BY COARSE-GRAINED SOIL.
8. SEDIMENT RETENTION IS REQUIRED UNLESS NO OFFSITE DISCHARGED IS ANTICIPATED FOR THE SPECIFIC DESIGN FLOW.
9. SURFACE WATER CONTROLS ARE REQUIRED UNLESS NO OFFSITE DISCHARGE IS ANTICIPATED FOR THE SPECIFIED DESIGN FLOW.
10. PHASING AND MORE CONSERVATIVE BMP'S MUST BE EVALUATED FOR CONSTRUCTION ACTIVITY NEAR SURFACE WATERS.
11. ANY RUNOFF GENERATED BY DEWATERING SHALL BE TREATED THROUGH CONSTRUCTION OF A SEDIMENT TRAP WHEN THERE IS SUFFICIENT SPACE OR BY RELEASING THE WATER TO A WELL-VEGETATED, GENTLY SLOPING AREA. SINCE PUMPS ARE USED FOR WATERING, IT MAY BE POSSIBLE TO PUMP THE SEDIMENT-LADEN WATER WELL AWAY FROM THE SURFACE WATER SO THAT VEGETATION CAN BE MORE EFFECTIVELY UTILIZED FOR TREATMENT. A STRAW BALE FILTER SHALL BE PLACED AROUND THE DISCHARGE FROM THE DEWATERING PUMP. IF THERE IS NOT SPACE FOR A SEDIMENT TRAP OR 25 FEET OF SUITABLE VEGETATION, OTHER FILTRATION METHODS SHALL BE REQUIRED.
12. THE FREQUENCY OF MAINTENANCE REVIEW INCREASES FROM MONTHLY TO WEEKLY.
13. SOILS SHALL BE STABILIZED AT THE END OF THE SHIFT, BEFORE A HOLIDAY OR WEEKEND, IF NEEDED, BASED ON THE WEATHER FORECAST.

ESC-3 SENSITIVE AREAS SPECIAL PROVISIONS

1. WHENEVER POSSIBLE, PHASE ALL OR PART OF THE PROJECT SO THAT IT OCCURS DURING THE DRY SEASON. IF THIS IS IMPOSSIBLE, NOVEMBER THROUGH FEBRUARY SHALL BE AVOIDED SINCE THIS IS THE MOST LIKELY PERIOD FOR LARGE, HIGH-INTENSITY STORMS.
2. ALL PROJECTS SHALL BE COMPLETED AND STABILIZED AS QUICKLY AS POSSIBLE. LIMITING THE SIZE AND DURATION OF A PROJECT IS PROBABLY THE MOST EFFECTIVE FORM OF EROSION CONTROL.
3. WHERE APPROPRIATE, SANDBAGS OR AN EQUIVALENT BARRIER SHALL BE CONSTRUCTED BETWEEN THE PROJECT AREA AND THE SURFACE WATER IN ORDER TO ISOLATE THE CONSTRUCTION AREA FROM HIGH WATER THAT MIGHT RESULT DUE TO PRECIPITATION.
4. ADDITIONAL PERIMETER PROTECTION SHALL BE CONSIDERED TO REDUCE THE LIKELIHOOD OF SEDIMENT ENTERING THE SURFACE WATERS. SUCH PROTECTION MIGHT INCLUDE MULTIPLE SILT FENCES, SILT FENCES WITH A HIGHER AOS, CONSTRUCTION OF A BERM, OR A THICK LAYER OF ORGANIC MULCH UPSLOPE OF A SILT FENCE.
5. IF WORK IS TO OCCUR WITHIN THE ORDINARY HIGH WATER MARK OF A STREAM, MOST PROJECTS MUST ISOLATE THE WORK AREA FROM THE STREAM BY DIVERTING THE STREAM OR CONSTRUCTING A COFFERDAM. CERTAIN SMALL PROJECTS THAT PROPOSE ONLY A SMALL AMOUNT OF GRADING MAY NOT REQUIRE ISOLATION SINCE DIVERSIONS TYPICALLY RESULT IN DISTURBANCE AND THE RELEASE OF SOME SEDIMENT TO THE STREAM. FOR SUCH SMALL PROJECTS, THE POTENTIAL IMPACTS FROM CONSTRUCTION WITH AND WITHOUT A DIVERSION MUST BE WEIGHED.
6. IF A STREAM MUST BE CROSSED, A TEMPORARY BRIDGE SHALL BE CONSIDERED RATHER THAN ALLOWING EQUIPMENT TO UTILIZE THE STREAMBED FOR A CROSSING.
7. ANY RUNOFF GENERATED BY DEWATERING SHALL BE TREATED THROUGH CONSTRUCTION OF A SEDIMENT TRAP WHEN THERE IS SUFFICIENT SPACE OR BY RELEASING THE WATER TO A WELL-VEGETATED, GENTLY SLOPING AREA. SINCE PUMPS ARE USED FOR DEWATERING, IT MAY BE POSSIBLE TO PUMP THE SEDIMENT-LADEN WATER WELL AWAY FROM THE SURFACE WATER SO THAT VEGETATION CAN BE MORE EFFECTIVELY UTILIZED FOR TREATMENT. A STRAW BALE FILTER SHALL BE PLACED AROUND THE DISCHARGED FROM THE DEWATERING PUMP. IF THERE IS NOT SPACE FOR A SEDIMENT TRAP OR 25 FEET OF SUITABLE VEGETATION, OR FILTRATION METHODS SHALL BE REQUIRED.

ESC-4 FINAL SITE STABILIZATION NOTES

1. ALL DISTURBED AREAS OF THE SITE SHALL BE VEGETATED OR OTHERWISE PERMANENTLY STABILIZED. AT A MINIMUM, DISTURBED AREAS SHALL BE SEEDED AND MULCHED WITH A HIGH LIKELIHOOD THAT SUFFICIENT COVER WILL DEVELOP SHORTLY AFTER FINAL APPROVAL. MULCH WITHOUT SEEDING IS NOT ADEQUATE TO ALLOW FINAL APPROVAL OF THE PERMIT, EXCEPT FOR SMALL AREAS OF MULCH USED FOR LANDSCAPING. THE ONLY EXCEPTIONS TO THESE REQUIREMENTS ARE LOTS WITHIN A PLAT THAT ARE TO BE DEVELOPED UNDER AN APPROVED RESIDENTIAL PERMIT IMMEDIATELY FOLLOWING PLAT APPROVAL. IN THESE CASES, MULCH AND/OR TEMPORARY SEEDING ARE ADEQUATE FOR COVER.
2. STRUCTURAL MEASURES SUCH AS, BUT NOT LIMITED TO, SILT FENCES, PIPE SLOPES, DRAINS, CONSTRUCTION ENTRANCES, STORM DRAIN INLET PROTECTION, AND SEDIMENT TRAPS AND PONDS SHALL BE REMOVED FROM THE SITE. MEASURES THAT WILL QUICKLY DECOMPOSE, SUCH AS BRUSH BARRIERS AND ORGANIC MULCHES, MAY BE LEFT IN PLACE. IN THE CASE OF SILT FENCES, IT MAY BE BEST TO REMOVE FENCES IN CONJUNCTION WITH THE SEEDING, SINCE IT MAY BE NECESSARY TO BRING MACHINERY BACK IN TO REMOVE THEM. THIS WILL RESULT IN DISTURBED SOILS THAT WILL AGAIN REQUIRE PROTECTION. THE DOES INSPECTOR MUST APPROVE AN APPLICANT'S PROPOSAL TO REMOVE FENCING PRIOR TO THE ESTABLISHMENT OF VEGETATION. IN SOME CASES, SUCH AS RESIDENTIAL BUILDING FOLLOWING PLAT DEVELOPMENT, IT MAY BE APPROPRIATE TO LEAVE SOME OR ALL ESC MEASURES FOR USE DURING SUBSEQUENT DEVELOPMENT. THIS SHALL BE DETERMINED ON A SITE-SPECIFIC BASIS.
3. ALL PERMANENT SURFACE WATER FACILITIES, INCLUDING CATCH BASINS, MANHOLES, PIPES, DITCHES, CHANNELS, R/D FACILITIES AND WATER QUALITY FACILITIES, SHALL BE CLEANED. ANY OFFSITE CATCH BASIN THAT REQUIRED PROTECTION DURING CONSTRUCTION SHALL ALSO BE CLEANED.
4. IF ONLY THE INFRASTRUCTURE OF THE SITE HAS BEEN DEVELOPED (E.G. SUBDIVISIONS AND SHORT PLAT) WITH BUILDING CONSTRUCTION TO OCCUR UNDER A DIFFERENT PERMIT, THEN THE SENSITIVE AREA BUFFERS, SENSITIVE AREA TRACTS OR SENSITIVE AREA SETBACK AREA SHALL BE CLEARLY MARKED AS DESCRIBED IN SECTION D.4.1 IN ORDER TO ALERT FUTURE BUYERS AND BUILDERS.

ESC-5 ROAD AND UTILITIES E.S.C. NOTES

1. PHASING THE PROJECT SO THAT THE SITE IS WORKED PROGRESSIVELY FROM END TO END, RATHER THAN CLEARING AND GRUBBING THE ENTIRE LENGTH OF THE PROJECT. THIS RESULTS IN SMALLER EXPOSED AREAS FOR SHORTER DURATIONS, THUS REDUCING THE EROSION RISK.
2. MULCHING AND VEGETATING CUT AND FILL SLOPES AS SOON AS THEY ARE GRADED. FREQUENTLY, THIS IS DONE AT THE END OF CONSTRUCTION WHEN PAVING OR UTILITY INSTALLATION IS COMPLETE. VEGETATING THESE AREAS AT THE START OF THE PROJECT STABILIZED THOSE AREAS MOST SUSCEPTIBLE TO EROSION.
3. PROTECTING ALL CATCH BASIN INLETS WITH CATCH BASIN INSERTS WHEN THESE DO NOT DRAIN TO PONDS OR TRAPS. THIS WILL NOT PROVIDE THE SAME LEVEL OF PROTECTION AS A SEDIMENT POND OR TRAP, BUT CAN REMOVE MOST OF THE SAND-SIZED MATERIAL ENTRAINED IN THE RUNOFF.
4. PHASING THE PROJECT SO THAT ALL CLEARING AND GRADING IN SENSITIVE AREA BUFFERS OCCURS IN THE DRY SEASON. THIS SUBSTANTIALLY REDUCES THE CHANCE OF EROSION AND ALLOWS FOR RAPID REVEGETATION IN THE LATE SUMMER AND EARLY FALL.
5. USING FLOCCULENT TO REDUCE THE TURBIDITY OF WATER RELEASED FROM SEDIMENT PONDS, WHEN APPROVED BY THE DEPARTMENT OF ECOLOGY.
6. HIRING A PRIVATE CONSULTANT WITH EXPERTISE IN ESC TO REVIEW AND MONITOR THE SITE.

ESC-6 CONSTRUCTION SEQUENCE

1. ATTEND PRE-CONSTRUCTION MEETING.
2. FLAG OR FENCE CLEARING LIMITS.
3. POST SIGN WITH NAME AND PHONE NUMBER OF ESC SUPERVISOR.
4. INSTALL CATCH BASIN PROTECTION IF REQUIRED.
5. GRADE AND INSTALL CONSTRUCTION ENTRANCE(S).
6. INSTALL PERIMETER PROTECTION (SILT FENCE, BRUSH BARRIER, ETC).
7. CONSTRUCT SEDIMENT PONDS AND TRAPS.
8. GRADE AND STABILIZE CONSTRUCTION ROADS.
9. CONSTRUCT SURFACE WATER CONTROLS (INTERCEPTOR DIKES, PIPE SLOPE DRAINS, ETC.) SIMULTANEOUSLY WITH CLEARING AND GRADING FOR PROJECT DEVELOPMENT.
10. MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE TOWN OF LA CONNER STANDARDS AND MANUFACTURER'S RECOMMENDATIONS.
11. RELOCATE SURFACE WATER CONTROLS AND EROSION CONTROL MEASURES OR INSTALL NEW MEASURES SO THAT AS SITE CONDITIONS CHANGE THE EROSION AND SEDIMENT CONTROL IS ALWAYS IN ACCORDANCE WITH THE TOWN OF LA CONNER EROSION AND SEDIMENT CONTROL STANDARDS.
12. COVER ALL AREAS THAT WILL BE UNWORKED FOR MORE THAN SEVEN DAYS OR TWO DAYS DURING THE WET SEASON WITH STRAW, WOOD FIBER MULCH, COMPOST, PLASTIC SHEETING OR EQUIVALENT.
13. STABILIZE ALL AREAS THAT REACH FINAL GRADE WITHIN SEVEN DAYS.
14. SEED OR SOD ANY AREAS TO REMAIN UNWORKED FOR MORE THAN 30 DAYS.
15. UPON COMPLETION OF THE PROJECT, ALL DISTURBED AREAS MUST BE STABILIZED AND BMP'S REMOVED IF APPROPRIATE.

SEEDING

SEED MIXES: THE SEED MIXES LISTED BELOW INCLUDE RECOMMENDED MIXES FOR TEMPORARY SEEDING. THESE MIXES, WITH THE EXCEPTION OF THE WETLAND MIX, SHALL BE APPLIED AT A RATE OF 130 LBS/ACRE. THIS RATE CAN BE REDUCED IF SOIL AMENDMENTS OR SLOW RELEASE FERTILIZERS ARE USED.

TEMPORARY EROSION CONTROL SEED MIX	% WEIGHT	% PURITY	% GERMINATION
CHESINGS OR RED FESQUE FESTUCA RUBRA VAR. COMMUTATA OR FESTUCA RUBRA	40	98	90
ANNUAL OR PERENNIAL RYE LOLIUM MULTIFLORUM OR LOLIUM PERENN	40	98	90
REDTOP OR COLONIAL BENTGRASS AGROSTIS ALBA OR AGROSTIS TENUIS	10	92	85
WHITE DUTCH CLOVER TRIFOLIUM REPENS	10	98	90

BIOSWALE SEED MIX (MODIFIED BRIARGREEN, INC., HYDROSEEDING GUIDE WETLANDS SEED MIX)	% WEIGHT	% PURITY	% GERMINATION
TALL OR MEADOW FESCUE FESTUCA ARUNDINACEA OR FESTUCA ELATIOR	68	98	90
SEASIDE/CREEPING BENTGRASS AGROSTIS PALUSTRIS	10	98	85
MEADOW FOXTAIL ALEPOCURUS PRATENSIS	10	90	80
ALSIKE CLOVER TRIFOLIUM HYBRIDUM	6	98	90
REDTOP AGROSTIS ALBA	6	92	85

WETLANDS SEED MIX	% WEIGHT	% PURITY	% GERMINATION
RED FESCUE FESTUCA RUBRA	35	90	90
REDTOP AGROSTIS ALBA	35	92	85
MEADOW FOXTAIL ALEPOCURUS PRATENSIS	30	90	80

SEEDING MAINTENANCE STANDARDS

1. ANY SEEDED AREAS THAT FAIL TO ESTABLISH AT LEAST 80 PERCENT COVER WITHIN ONE MONTH SHALL BE RESEEDED. IF RESEEDING IS INEFFECTIVE, AN ALTERNATE METHOD, SUCH AS SODDING OR NETS/BLANKETS, SHALL BE USED. IF WINTER WEATHER PREVENTS ADEQUATE GRASS GROWTH, THIS TIME LIMIT MAY BE RELAXED.
2. AFTER ADEQUATE COVER IS ACHIEVED, ANY AREAS THAT EXPERIENCE EROSION SHALL BE RESEEDED AND PROTECTED BY MULCH.
3. SEEDED AREAS SHALL BE SUPPLIED WITH ADEQUATE MOISTURE, BUT NOT WATERED TO THE EXTENT THAT IT CAUSES RUNOFF.

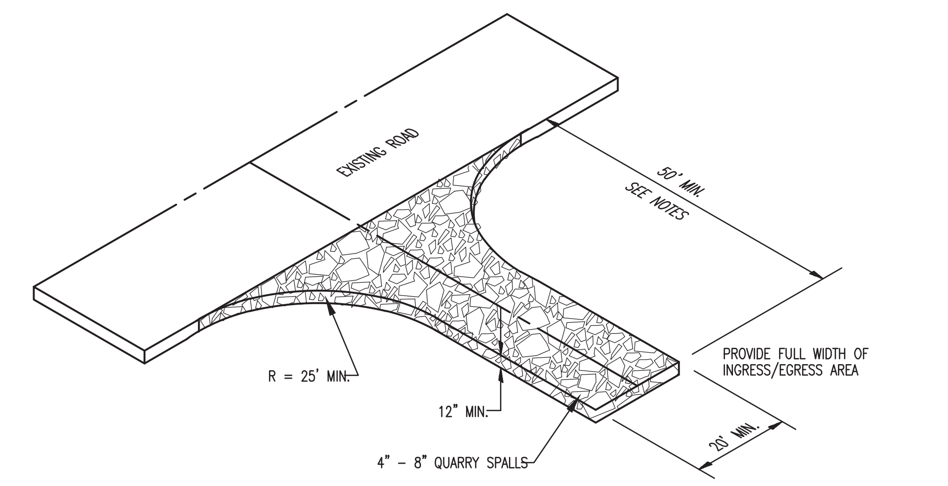
MULCHING

MULCH MATERIAL	QUALITY STANDARDS	APPLICATION RATES
STRAW	AIR-DRIED: FREE FROM UNDESIRABLE SEED AND COARSE MATERIAL	2"-3" THICK; 2-3 BALES 2-3 BALES PER 1000 SQ. FT. OR 2-3 TONS PER ACRE.
WOOD FIBER CELLULOSE	NO GROWTH INHIBITING FACTORS	APPROX. 25-30 LBS. PER 1000 SQ. FT. OR 1000-1500 LBS PER ACRE
COMPOST	NO VISIBLE WATER OR DUST DURING HANDLING. MUST BE PURCHASED FROM SUPPLIER WITH A SOLID WASTE HANDLING PERMIT.	2" THICK MIN.; APPROX. 100 TONS PER ACRE (APPROX. 800 LBS PER YARD)
CHIPPED SITE VEGETATION	AVERAGE SIZE SHALL BE SEVERAL INCHES.	2" MINIMUM THICKNESS

NOTE: MULCHING TO BE UTILIZED AS REQUIRED TO PREVENT EROSION AS DIRECTED BY THE ENGINEER.

MULCHNG MAINTENANCE STANDARDS

1. THE THICKNESS OF THE COVER MUST BE MAINTAINED.
2. ANY AREAS THAT EXPERIENCE EROSION SHALL BE REMULCHED AND/OR PROTECTED WITH A NET OR BLANKET. IF THE EROSION PROBLEM IS DRAINAGE RELATED, THEN THE PROBLEM SHALL BE FIXED AND THE ERODED AREA REMULCHED.



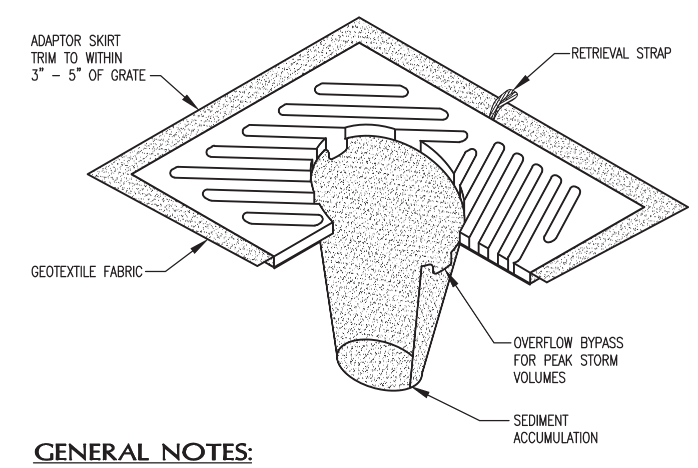
GENERAL NOTES:

1. ALL MUD AND DEBRIS TRACKED ONTO EXISTING ASPHALTED ROADWAYS SHALL BE SWEPT/CLEANED UP AFTER EACH DAY'S WORK, OR AS REQUESTED BY THE PROJECT ENGINEER AND/OR SKAGIT COUNTY ENGINEERING DEPT.

TEMPORARY CONSTRUCTION ENTRANCE MAINTENANCE STDS.:

1. QUARRY SPALLS SHALL BE ADDED IF THE PAD IS NO LONGER IN ACCORDANCE WITH THE SPECIFICATIONS.
2. IF THE ENTRANCE IS NOT PREVENTING SEDIMENT FROM BEING TRACKED ONTO PAVEMENT, THEN ALTERNATIVE MEASURES TO KEEP THE STREETS FREE OF SEDIMENT SHALL BE USED. THIS MAY INCLUDE STREET SWEEPING OR AN INCREASE IN THE DIMENSIONS OF THE ENTRANCE.
3. ANY SEDIMENT THAT IS TRACKED ONTO PAVEMENT SHALL BE REMOVED IMMEDIATELY BY SWEEPING. THE PAVEMENT SHALL NOT BE CLEANED BY WASHING DOWN THE STREET, EXCEPT WHEN SWEEPING IS INEFFECTIVE AND THERE IS A THREAT TO PUBLIC SAFETY. IF IT IS NECESSARY TO WASH THE STREETS, THE CONSTRUCTION OF A SMALL SUMP OR POND SHALL BE CONSTRUCTED AND THE SEDIMENT WASHED INTO THE SUMP OR POND.
4. ANY ROCK SPALLS THAT ARE LOOSENED FROM THE PAD AND END UP ON THE ROADWAY SHALL BE REMOVED IMMEDIATELY.
5. THE AREA OF THE ENTRANCE SHOULD BE CLEARED OF ALL VEGETATION ROOTS AND OTHER OBJECTIONABLE MATERIAL. THE GRAVEL SHALL BE PLACED TO THE SPECIFIED DIMENSIONS. ANY DRAINAGE FACILITIES REQUIRED BECAUSE OF WASHING SHOULD BE CONSTRUCTED ACCORDING TO SPECIFICATIONS IN THE PLANS. IF WASH RACKS ARE USED, THEY SHOULD BE INSTALLED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.
6. IF CONDITIONS ON THE SITE ARE SUCH THAT MOST OF THE MUD IS NOT REMOVED FROM VEHICLE TIRES BY CONTACT WITH THE GRAVEL, THEN THE TIRES MUST BE WASHED BEFORE VEHICLES ENTER A PUBLIC ROAD. WASH WATER MUST BE CARRIED AWAY FROM THE ENTRANCE TO A SETTLING AREA TO REMOVE SEDIMENT. A WASH RACK MAY ALSO BE USED TO MAKE WASHING MORE CONVENIENT AND EFFECTIVE.
7. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOW OF MUD ONTO PUBLIC RIGHTS-OF-WAY. THIS MAY REQUIRE A PERIODIC TOP DRESSING WITH 2-INCH STONE, AS CONDITIONS DEMAND, AND REPAIR AND/OR CLEAN OUT ANY STRUCTURES USED TO TRAP SEDIMENT. ALL MATERIALS SPILLED, DROPPED WASHED OR TRACKED FROM VEHICLES ONTO ROADWAY OR INTO STORM DRAINS MUST BE REMOVED IMMEDIATELY.

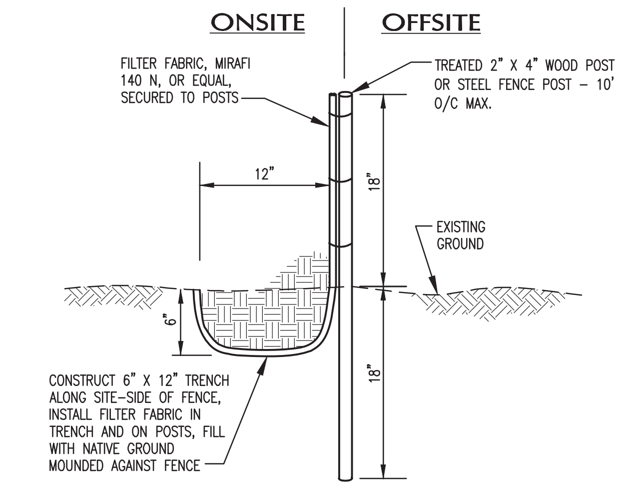
TEMPORARY CONSTRUCTION ENTRANCE DETAIL (A / 1.2)
NOT TO SCALE



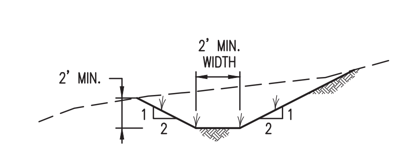
GENERAL NOTES:

1. INSERT SHALL BE INSTALLED PRIOR TO CLEARING AND GRADING ACTIVITY, OR UPON PLACEMENT OF A NEW CATCH BASIN.
2. SEDIMENT SHALL BE REMOVED FROM THE UNIT WHEN IT BECOMES ONE THIRD FULL .
3. SEDIMENT REMOVAL SHALL BE ACCOMPLISHED BY REMOVING THE INSERT, EMPTYING, AND RE-INSERTING IT INTO THE CATCH BASIN.

CATCH BASIN INLET PROTECTION DETAIL (B / 1.2)
NOT TO SCALE



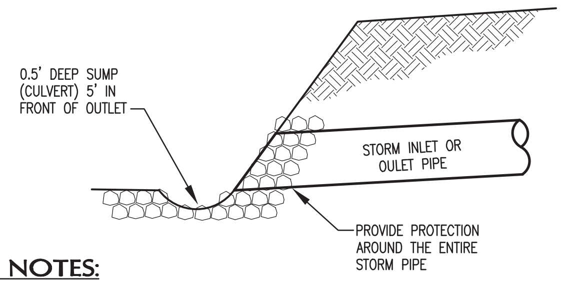
SILT FENCE DETAIL (C / 1.2)
NOT TO SCALE



GENERAL NOTES:

1. ALL TEMPORARY DITCHES SHALL HAVE POSITIVE DRAINAGE TO CONSTRUCTED ROAD DITCHES AS INDICATED.
2. DITCH TO BE GRAVEL LINED WHEN SLOPE GREATER THAN 5.00%.

INTERCEPTOR SWALE DETAIL (D / 1.2)
NOT TO SCALE



GENERAL NOTES:

1. QUARRY SPALL PADS SHALL BE CONSTRUCTED TO DISPERSE INLET FLOWS TO REDUCE INLET AND OUTLET VELOCITIES, AND TO PREVENT CONCENTRATED EROSION.
2. SPALLS SHALL BE CAST TO A MINIMUM 8" DPETH, A MINIMUM OF 3x PIPE DIAMETER LENGTH, AND A 2x PIPE DIAMETER WIDTH. THE SPALLS SHALL BE MECHANICALLY COMPACTED SUCH THAT THE TOP OF SPALLS IS SET FLUSH WITH THE PIPE INVERT ELEVATION.
3. QUARRY SPALLS SHALL CONFORM TO SECTION 9-13.6 OF THE 2020 STANDARD SPECIFICATIONS.

TYPICAL INLET/OUTLET PROTECTION DETAIL (F / 1.2)
NOT TO SCALE

Sound Development Group
ENGINEERING, SURVEYING & LAND DEVELOPMENT SERVICES
P.O. Box 1705 • 1111 Cleveland Avenue, Suite 202
Mount Vernon, WA 98273   Tel: 360-404-2010

SHEET REVISIONS:

NO.	DATE	DESCRIPTION	APPROVED
1	9.22.23	ARCHITECT REVISION	P.L.S.

CALL 48 HOURS BEFORE YOU DIG 1.800.424.5555

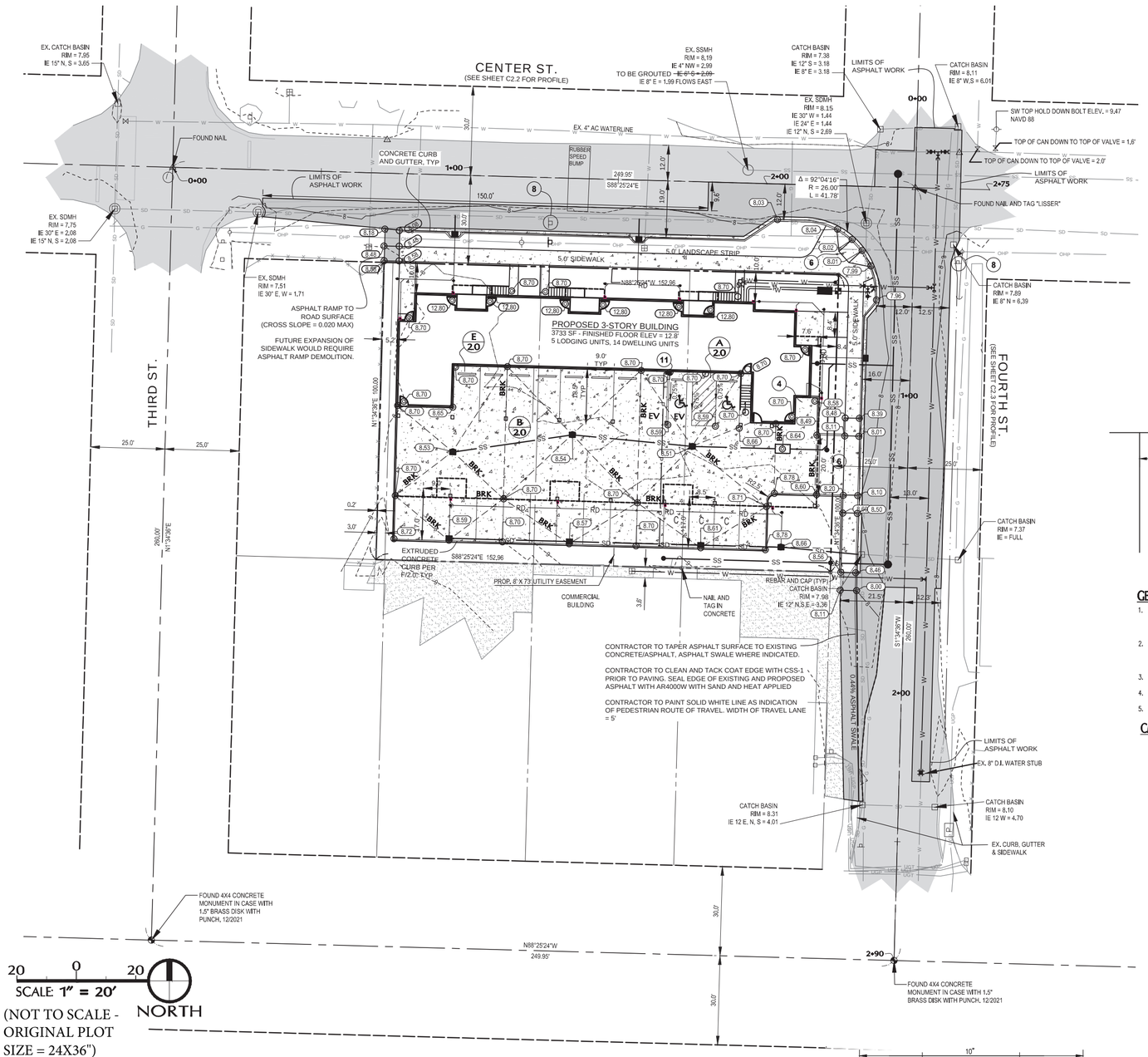
PROJECT:
CENTER STREET MIXED-USE FOR KSA INVESTMENTS, LLC

SHEET DESCRIPTION:
TESC PLAN NOTES & DETAILS

PATRICK L. SEVERIN — STATE OF WASHINGTON — 32226 — REGISTERED PROFESSIONAL ENGINEER

SCALE:	AS NOTED
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG
SHEET NUMBER:	C1.2

IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA CONNER, WASHINGTON



SHEET NOTES

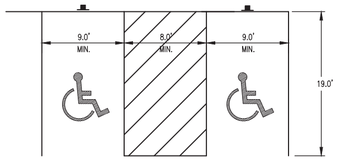
1. CONTRACTOR TO PROTECT EXISTING TREES ALONG WESTERN PROPERTY LINE AND ASSOCIATED ROOT SYSTEM. THE PROJECT LANDSCAPE ARCHITECT AND/OR ARBORIST IS TO BE CONTACTED IMMEDIATELY SHOULD ANY CONCERNS ARISE REGARDING THE TREES.
2. THE 5.0' LANDSCAPE STRIP BORDERING THE NORTHERN SIDEWALK IS TO NOT ADVERSELY IMPACT SIDEWALK LIGHTING. FOR ADDITIONAL DETAILS, REFER TO THE LANDSCAPE PLAN.
3. CONTRACTOR TO MINIMIZE IMPACT TO ALL FOUND MONUMENTS AND TO SCHEDULE RESTORATION OF MONUMENT(S) BY A LICENSED PROFESSIONAL LAND SURVEYOR AT THE COMPLETION OF CONSTRUCTION, AS NECESSARY AND APPROPRIATE.
4. BUILDING DIMENSIONS: SEE ARCHITECTURE PLANS
   PROPOSED UNITS: 5 LODGING UNITS, 14 DWELLING UNITS
   GROUND LEVEL ELEV: 8.70' (NO OCCUPANCY AT THIS ELEVATION)
   FINISHED FLOOR (UNIT) ELEV: 12.8' (OCCUPANCY AT THIS ELEVATION)

   BUILDING FOOTPRINT AREA: 3,733 SF - 0.0857 AC
   BUILDING COVERAGE (INCL. COVERED PARKING AND OVERHANGS): 9,473 SF - 0.2175 AC

   PARKING STALLS PROVIDED: 11 STANDARD STALLS, 3 COMPACT STALLS, 8 LONG STALLS, 2 ACCESSIBLE STALLS (24 TOTAL)

   DUMPSTER AREA DIMENSIONS: 7.5' X 9' (CONSTRUCTED PER DETAIL D/2.0)
5. FOURTH STREET IS TO HAVE A FULL STREET ASPHALT OVERLAY WITH GRADING AS SHOWN ON PLANS. CENTER STREET IS TO HAVE A QUARTER-HALF STREET OVERLAY. OVERLAY EXTENTS ARE OUTLINED WITH SAWCUT LINES AND/OR EDGE OF ASPHALT LINES. CONTRACTOR IS TO PAVE OVERLAY AND CONSTRUCT ADDITIONAL ROAD WIDTH PER TOWN OF LA CONNER STANDARD DETAIL G3/3.0. CONCRETE CURB AND GUTTER PER WSDOT STANDARD PLAN F-1a.

   CONCRETE PAVEMENT WITHIN THE PROPERTY BOUNDARY IS TO BE PER DETAIL B/2.0.
6. ALL SIDEWALKS WITHIN THE RIGHT-OF-WAY ARE TO BE CONSTRUCTED PER WSDOT STANDARD PLAN F-3 WITH A TYPE 1 DRIVEWAY ENTRANCE OFF OF FOURTH STREET PER WSDOT STANDARD PLAN F-4. PERPENDICULAR CURB RAMP AT INTERSECTION OF CENTER AND FOURTH STREET PER WSDOT STD PLAN F-40.15-04 ON SHEET C4.0
7. CONTRACTOR TO COORDINATE ALL UTILITY AND ROAD SHUT-DOWN PERIODS WITH THE TOWN OF LA CONNER, EMERGENCY DEPARTMENTS, UTILITY PURVEYORS AND AFFECTED PROPERTIES (RESIDENTIAL AND COMMERCIAL). CONTRACTOR TO PROVIDE TRAFFIC CONTROL PLAN, APPROVED BY THE TOWN OF LA CONNER, PRIOR TO BEGINNING CONSTRUCTION.
8. CONTRACTOR TO RELOCATE ALL EXISTING SIGNS AFFECTED BY CONSTRUCTION PER MUTCD, INCLUDING BUT NOT LIMITED TO THE "SHARE THE ROAD" AND BIKE-PEDESTRIAN DIAMOND-SHAPED SIGN ON CENTER STREET AND THE STOP SIGN IN THE SOUTHEAST CORNER OF THE CENTER AND FOURTH STREET INTERSECTION.
9. CONTRACTOR TO REPAINT SKIP YELLOW CENTER STRIPE ON CENTER AND FOURTH STREET, AND TO PAINT/REPAINT AFFECTED STOP BARS AT THE CENTER AND FOURTH STREET INTERSECTION. PLACED PER MUTCD.
10. CONTRACTOR TO FOLLOW TOWN OF LA CONNER'S TYPICAL TRENCH SECTION DETAIL FOR ALL TRENCH RESTORATION WORK WITHIN THE RIGHT-OF-WAY (DETAIL G1/3.0) AND SDG DETAIL E/3.2 FOR ALL ON-SITE TRENCHES.
11. CONTRACTOR TO INSTALL TWO ELECTRIC VEHICLE CHARGING STATIONS AND 2" POWER CONDUIT FOR FUTURE CHARGING STATIONS PER THE ELECTRICAL PLAN

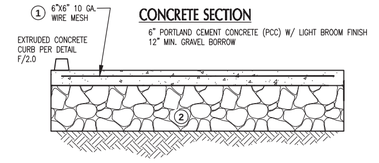


TYPICAL HANDICAP STALL AGAINST CURB & GUTTER OR SIDEWALK

GENERAL NOTES:

1. CONCRETE PARKING AREAS OUTSIDE OF LIMITS OF GARAGE SHALL HAVE 6" CONCRETE OVER 12" GRAVEL BASE COMPACTED TO 95% OF MAX. DENISITY.
2. DUMMY JOINTS SHALL BE PLACED NOT TO EXCEED 15' O.C. NOR LESS THAN 10' O.C. THEY SHALL NOT BE LESS THAN 3/16" IN THICKNESS AND SHALL EXTEND 2-1/4" DEEP.
3. CEMENT CONCRETE SHALL BE CLASS 3000 AIR ENTRAINED
4. ALL JOINTS SHALL BE CLEANED AND EDGED.
5. SUBGRADE COMPACTION SHALL BE 95% MODIFIED PROCTOR.

CMV TYPICAL PARKING STALL DIMENSIONS A/2.0
NOT TO SCALE

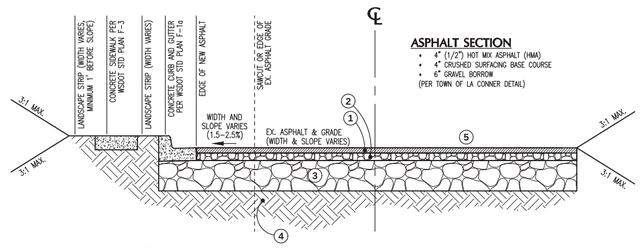


CONCRETE SECTION

GENERAL NOTES:

1. ALL CONCRETE SHALL BE PORTLAND CEMENT CONCRETE (PCC). W/ LIGHT BROOM FINISH. CONTROL JOINTS SHALL BE SPACED MAXIMUM DISTANCES OF 10 FEET APART, CENTER-TO-CENTER, IN BOTH THE LONGITUDINAL AND TRANSVERSE DIRECTIONS. CRACK CONTROL JOINTS MAY BE CREATED BY PLACING AN INSERT OR GROOVE INTO THE FRESH CONCRETE SURFACE DURING FINISHING, OR BY SAWCUTTING THE CONCRETE AFTER IT HAS INITIALLY SET-UP. WE RECOMMEND THE DEPTH OF THE CRACK CONTROL JOINTS SHALL BE APPROXIMATELY 1/4 THE THICKNESS OF THE CONCRETE; OR ABOUT 1 1/2 INCHES DEEP FOR THE RECOMMENDED CONCRETE THICKNESS OF 6 INCHES. CONTROL JOINTS SHALL BE SEALED WITH AN APPROPRIATE SEALANT TO HELP RESTRICT WATER INFILTRATION INTO THE JOINTS.
2. GRAVEL BORROW, SHALL SUPPORT ALL PAVEMENT. GRAVEL BORROW WITH LESS THAN 5% PASSING THE 200 SIEVE, SHALL CONFORM TO SECTION 9-03.14 OF THE 2023 STANDARD SPECIFICATIONS COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY IN ACCORDANCE WITH ASTM D-1557 TESTING EXISTING GRAVEL MATERIAL MAY BE UTILIZED AS GRAVEL BORROW AS DETERMINED BY THE GEOTECHNICAL ENGINEER.
3. THE EXCAVATED SUBGRADE SHALL BE FREE OF TOPSOIL, ORGANICS, AND OTHER DELETERIOUS MATERIAL, COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY IN ACCORDANCE WITH ASTM D-1557 TESTING, PREPARED CONFORMING TO SECTION 2-06.3(1) OF THE 2023 STANDARD SPECIFICATIONS. AFTER SUBGRADE EXCAVATION, PRIOR TO THE PLACEMENT OF ANY STRUCTURAL FILL, CRUSHED ROCK, AND GRAVEL, THE CONTRACTOR SHALL SCHEDULE THEIR RETAINED GEOTECHNICAL ENGINEERING COMPANY TO INSPECT AND APPROVE ALL SUBGRADE CONDITIONS. THE CONTRACTOR SHALL PROVIDE A FULL-LOADED GRAVEL TRUCK FOR THIS INSPECTION PHASE.

CONCRETE PAVEMENT SECTION DETAIL B/2.0
NOT TO SCALE



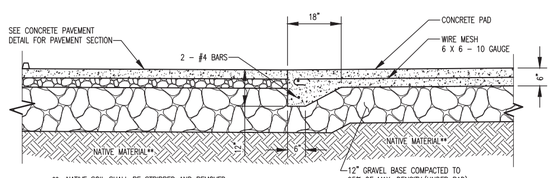
ASPHALT SECTION
- 4" (1/2") HOT MIX ASPHALT (HMA)
- 4" CRUSHED SURFACING BASE COURSE
- 6" GRAVEL BORROW

(PER TOWN OF LA CONNER DETAIL)

GENERAL NOTES:

1. ALL ASPHALT SHALL BE 1/2" HOT MIX ASPHALT CONFORMING TO SECTION 5-04 OF THE 2023 STANDARD SPECIFICATIONS, COMPACTED TO A MINIMUM OF 91% RICE DENSITY. WHERE PROPOSED ASPHALT ABUTS EXISTING ASPHALT, THE EXISTING ASPHALT SHALL BE SAW CUT FULL DEPTH AND TACK COATED IMMEDIATELY BEFORE PAVING. ALL SURFACE JOINTS SHALL BE SEALED WITH AR 4000 W AND SAND, APPLIED WITH HEAT.
2. CRUSHED SURFACING BASE COURSE SHALL CONFORM TO SECTION 9-03.9(3) OF THE 2023 STANDARD SPECIFICATIONS, COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY IN ACCORDANCE WITH ASTM D-1557 TESTING. ALL CRUSHED SURFACING SHALL BE SPRAYED WITH SOIL RESIDUAL HERBICIDE A MAXIMUM OF 24 HOURS PRIOR TO PAVING, ACCORDING TO SECTION 5-04.3(5)D OF THE 2023 STANDARD SPECIFICATIONS.
3. GRAVEL BASE, A MINIMUM OF 6 INCH COMPACTED DEPTH SHALL SUPPORT ALL PAVEMENT. GRAVEL BORROW WITH LESS THAN 5% PASSING THE 200 SIEVE, SHALL CONFORM TO SECTION 9-03.14 OF THE 2023 STANDARD SPECIFICATIONS COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY IN ACCORDANCE WITH ASTM D-1557 TESTING EXISTING GRAVEL MATERIAL MAY BE UTILIZED AS GRAVEL BORROW AS DETERMINED BY THE GEOTECHNICAL ENGINEER.
4. THE EXCAVATED SUBGRADE SHALL BE FREE OF TOPSOIL, ORGANICS, AND OTHER DELETERIOUS MATERIAL, COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY IN ACCORDANCE WITH ASTM D-1557 TESTING, PREPARED CONFORMING TO SECTION 2-06.3(1) OF THE 2023 STANDARD SPECIFICATIONS.
5. IN ALL AREAS OUTSIDE OF THE INDICATED ROAD WIDENING ON FOURTH STREET, CONTRACTOR TO MATCH EXISTING SUBGRADE SECTIONS, BACKFILL TRENCHES PER DETAIL G1/3.0 AND REFINISH ASPHALT SURFACE. SEE TOWN OF LA CONNER DETAIL G3/3.0 FOR ADDITIONAL INFORMATION.

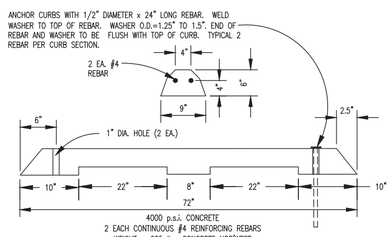
TYPICAL STREET PAVEMENT SECTIONS C/2.0
NOT TO SCALE



** NATIVE SOIL SHALL BE STRIPPED AND REMOVED TO THE SATISFACTION OF THE ENGINEER PRIOR TO PLACEMENT OF GRAVEL BALLAST

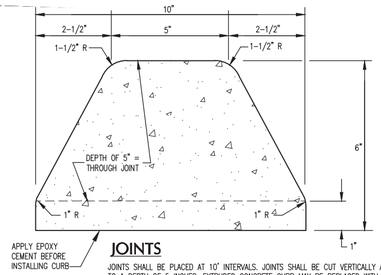
GENERAL NOTE:
SEE ARCHITECTURAL PLANS FOR ENCLOSURE DETAILS AND CONSTRUCTION.

CONCRETE DUMPSTER PAD DETAIL D/2.0
NOT TO SCALE



4000 p.s.i. CONCRETE
2 EACH CONTINUOUS #4 REINFORCING REBARS
WEIGHT = 205 lbs. CONCRETE NORTHWEST PRODUCT OR EQUAL

CEMENT CONCRETE PARKING CURB DETAIL E/2.0
NOT TO SCALE



JOINTS
JOINTS SHALL BE PLACED AT 10' INTERVALS. JOINTS SHALL BE CUT VERTICALLY AND TO A DEPTH OF 5 INCHES. EXTRUDED CONCRETE CURB MAY BE REPLACED WITH CAST-IN-PLACE VERTICAL CURB OF EQUIVALENT DIMENSIONS (LENGTH & WIDTH)

EXTRUDED CONCRETE CURB DETAIL F/2.0
NOT TO SCALE

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ENGINEERING, SURVEYING & LAND DEVELOPMENT SERVICES
P.O. Box 1705 • 1111 Cleveland Avenue, Suite 202
Mount Vernon, WA 98273 Tel: 360-404-2010

SHEET REVISIONS:

NO.	DATE	DESCRIPTION	APPROVED
1	9.22.23	ARCHITECT REVISION	P.L.S.

CALL 48 HOURS BEFORE YOU DIG 1.800.424.5555

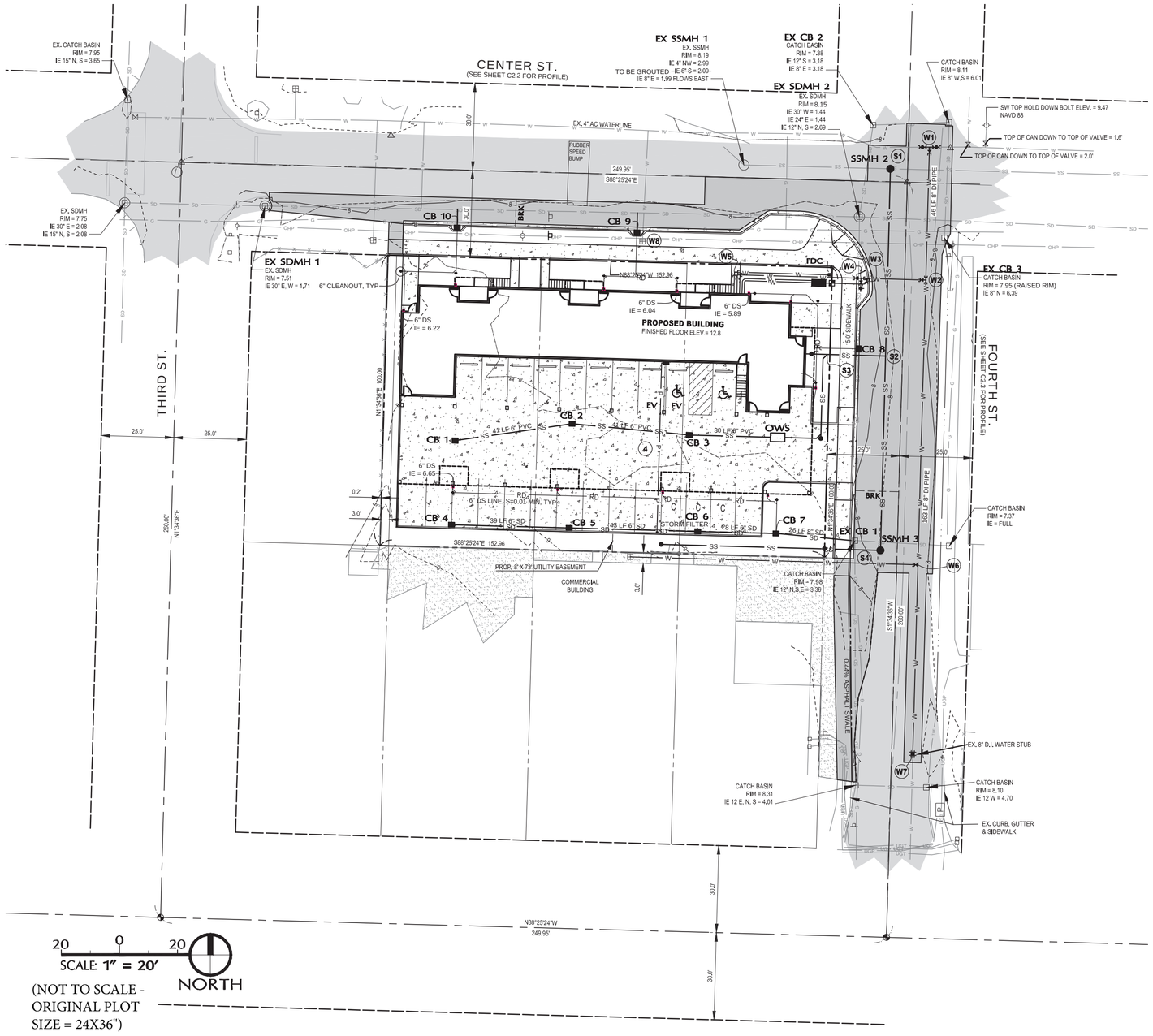
PROJECT:
CENTER STREET MIXED-USE
FOR
KSA INVESTMENTS, LLC

SHEET DESCRIPTION:
GRADING AND DIMENSIONAL PLAN

PATRICK L. SEVERN — STATE OF WASHINGTON — 32226 — REGISTERED PROFESSIONAL ENGINEER

SCALE:	1" = 20'
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG
SHEET NUMBER:	C2.0

IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA CONNER, WASHINGTON



GENERAL UTILITY NOTES

1. CONTRACTOR TO LOCATE AND VERIFY ALL SIZES, LOCATIONS, INVERTS AND MATERIALS OF EXISTING UTILITIES. NOTIFY ENGINEER IMMEDIATELY SHOULD ANY DISCREPANCIES OCCUR.
2. ALL UTILITY STATIONING AND OFFSETS ON THIS SHEET ARE BASED ON THE FOURTH STREET ALIGNMENT, UNLESS SPECIFICALLY STATED OTHERWISE.
3. ALL UTILITIES ARE TO BE CONNECTED AND INSTALLED BY THE CONTRACTOR UNLESS STATED OTHERWISE BY THE TOWN OF LA CONNER. CONTRACTOR IS RESPONSIBLE FOR ALL SAWCUTS, EXCAVATION, REMOVAL OF MATERIALS, CONNECTIONS, BACKFILL, COMPACTION, RESTORATION, RESURFACING AND OTHER WORK AS NECESSARY FOR PROJECT COMPLETION. CONTRACTOR TO PROVIDE A TRAFFIC CONTROL PLAN, APPROVED BY THE TOWN OF LA CONNER. CONTRACTOR TO PROVIDE DOCUMENTATION FOR MATERIAL SOURCES AND THE DUMP SITE, SUBJECT TO THE APPROVAL OF THE TOWN OF LA CONNER.
4. ELECTRIC VEHICLE (EV) CHARGING STATION AND 1-2" CONDUIT FOR ADDITIONAL FUTURE CHARGING STATIONS TO BE INSTALLED BY THE CONTRACTOR WHERE INDICATED ON THESE PLANS, SEE ELECTRICAL PLANS FOR ADDITIONAL INFORMATION.

WATERLINE NOTES

- ALL JOINTS TO BE RESTRAINED UNDER THE FOLLOWING WATERLINE NOTES: W1-2, W4-5, W7. THRUST-BLOCKING PER DETAIL W4/3.1
- FIXTURES TO BE INSTALLED WITHIN THE BUILDING'S MECHANICAL/RISER ROOM INCLUDE A 6" DOUBLE-CHECK DETECTOR ASSEMBLY (FIRE LINE) PER TOWN OF LA CONNER STANDARDS. SEE MECHANICAL PLAN FOR ADDITIONAL INFORMATION.
- CONTRACTOR TO VERIFY FIRE LINE SIZE WITH SPRINKLER DESIGNER PRIOR TO INSTALLATION (W3,W5).
- CONTRACTOR TO INSTALL ALL WATERLINE ELEMENTS WITH REASONABLE SEPARATION FROM OTHER UTILITIES, AND PER DETAIL A/3.1 FOR SEWER.
- CONTRACTOR TO INSTALL BLOW-OFF ASSEMBLIES, PERFORM PRESSURE AND BACTERIOLOGICAL TESTING, AND OBTAIN THE TOWN OF LA CONNER'S APPROVAL PRIOR TO CONNECTING THE PROPOSED WATERLINE TO ANY PORTION OF THE EXISTING WATER DISTRIBUTION SYSTEM (SEE W1 AND W7)

W1 STA 0+17.77, 7.31 LT
1 - 8" D.I. TEE, FL CUT INTO EXISTING WATER MAIN
3 - 8" D.I. GATE VALVES, FL X MJ PER DETAIL W6/3.2
2 - 8" X 4" D.I. REDUCERS, PE X MJ (W,E)
2 - 4" D.I. PUPS (W,E)
2 - 4" TRANSITION COUPLINGS, DI X AC (W,E)
46 LF 8" D.I. PIPE TO TEE (S)

W2 STA 0+63.48, 7.30 LT
1 - 8" D.I. TEE, MJ X FL
1 - 8" D.I. GATE VALVE, FL X MJ (W) PER DETAIL W6/3.2
21 LF 8" D.I. PIPE TO HYDRANT TEE AND METER SERVICE (W)
163 LF 8" D.I. PIPE TO SLEEVE (S)

W3 1 - 2" SERVICE TAP INTO FIRE LINE BEFORE TEE
1 - 2" GATE VALVE PER DETAILS W2/3.1 & W6/3.2
13 LF 2" WATER SERVICE LINE TO METER PER DETAIL W2/3.1
1 - 1.5" SERVICE METER PER TOWN OF LA CONNER DETAIL W2/3.1
1 LF 2" WATER SERVICE LINE TO RPBA
1 - 2" REDUCED PRESSURE BACK FLOW ASSEMBLY PER DETAIL C/3.2 WITH HOTBOX MODEL HB2, POWER TO BE PROVIDED WITH 29 LF 1" CONDUIT AND BENDS AS NECESSARY
28 LF 2" WATER SERVICE LINE TO BUILDING PER DETAIL W2/3.1, BENDS AS NECESSARY

W4 1 - 8" X 6" D.I. TEE AFTER SERVICE TAP (NOTE W3), MJ X FL
1 - 6" D.I. GATE VALVE, FL X MJ (S OF TEE) PER W6/3.2
4 LF 6" D.I. PIPE TO HYDRANT
1 - 6" HYDRANT ASSEMBLY PER DETAIL W3/3.1
1 - 8" X 6" D.I. REDUCER, PE X MJ (W OF TEE)
43 LF 8" D.I. PIPE TO BEND
1 - 8" 90° D.I. BEND, MJ
6 LF 8" D.I. PIPE TO BUILDING

W5 7 LF 4" D.I. PIPE FROM BUILDING TO BEND
1 - 4" 90° D.I. BEND, MJ
32 LF 4" D.I. PIPE TO FDC
1 - 4" FIRE DEPARTMENT CONNECTION PER DETAIL D/3.2

W6 1 - 1.5" SERVICE TAP INTO PROPOSED 8" WATERLINE PER DETAIL W2/3.1
1 - 1.5" GATE VALVE PER DETAILS W2/3.1 AND W6/3.2
98 LF 1.5" WATER SERVICE LINE (W)
CONNECT LINE TO EXISTING METER SETTER AND INSTALL 1" METER PER DETAIL W2/3.1

W7 STA 2+26.56, 7.30' LT
1 - 8" D.I. SLEEVE TO EXISTING 8" D.I. STUB

W8 EXISTING METER TO BE MOVED TO THE LANDSCAPE STRIP AND USED AS THE LANDSCAPE METER FOR THE PROJECT.

SANITARY SEWER NOTES

- ALL INVERTS ARE APPROXIMATE, AS CONFLICTS WITH EXISTING UTILITIES MAY ARISE. CONTRACTOR TO FOLLOW GENERAL UTILITY NOTE 1 AND IS TO CONTACT THE ENGINEER IMMEDIATELY REGARDING UTILITY CONFLICTS.
- CONTRACTOR TO INSTALL ALL SEWER ELEMENTS WITH MINIMUM SEPARATIONS FROM WATER LINES PER DETAIL A/2.1.
- CONTRACTOR TO INSTALL CLEANOUTS PER TOWN OF LA CONNER DETAIL S7/3.1
- CONTRACTOR TO INSTALL TYPE 1 MANHOLES (PER NOTES BELOW) PER DETAILS S1/3.0, S3/3.0, S5/3.0

S1 SADDLE 48" STANDARD MANHOLE (SSMH 2) ONTO EXISTING 8" SEWER LINE, IE E,W = 1.67±, CHANNEL MANHOLE PER DETAIL S4/3.0
INSTALL 65 LF 8" PVC PIPE FROM MANHOLE TO NOTE S2, IE N = 3.58, S = 0.004 MIN

S2 INSTALL 8"X6" PVC TEE-WYE, IE = 3.97±
INSTALL 67 LF 8" PVC PIPE, S = 0.004 MIN (S TO NOTE S4)
INSTALL 20 LF 6" PVC PIPE, S = 0.010 MIN (W)
INSTALL 6" PVC TEE-WYE, IE = 5.53±, SEE NOTE S3 FOR S OF TEE-WYE
INSTALL 6 LF 6" PVC PIPE, S = 0.010 MIN (W OF TEE-WYE)
INSTALL 6" PVC TEE-WYE WITH CLEANOUT PER DETAILS S6/3.1 & S7/3.1
INSTALL 2 LF 6" PVC PIPE TO BUILDING, S = 0.010 MIN.

S3 INSTALL 26 LF PVC PIPE, S = 0.010 MIN (S OF TEE-WYE)
INSTALL 6" PVC TEE-WYE WITH CLEANOUT, IE = 5.78±
INSTALL 13 LF PVC PIPE, S = 0.010 MIN
INSTALL 25-SA UTILITY VAULT OR EQUIVALENT
INSTALL COALESCING PLATE OIL-WATER SEPARATOR (25-CPS), IE TO SEWER = 5.91, IE TO CB 3 = 5.91

S4 INSTALL 48" STANDARD MANHOLE (SSMH 3), IE 8" OUT (N) = 4.53, IE 6" IN (W) = 4.63, CHANNEL MANHOLE PER DETAIL S1/3.0
INSTALL 17 LF 6" PVC PIPE, S = 0.010 MIN (W)
INSTALL 6" PVC TEE-WYE WITH CLEANOUT, IE = 5.00±
INSTALL 56 LF 6" PVC PIPE, S = 0.010 MIN
INSTALL 6" PVC TEE-WYE WITH CLEANOUT, IE = 6.22±
CONNECT TO EXISTING SEWER STUB WITH TEE-WYE AND CLEANOUT AND PER GENERAL UTILITY NOTE 1 AND STREET SIDE SEWER DETAIL S7/3.1. MAINTAIN 2' OF COVER FOR LENGTH OF PIPE TO FOURTH STREET. MAINTAIN 5' HORIZONTAL AND 1.5' VERTICAL SEPARATION FROM WATER SERVICE LINE PER DETAIL A/2.1.

STORMWATER NOTES

- CATCH BASINS 1-3 ARE A PART OF THE SANITARY SEWER NETWORK, AS THEIR LOCATION WITHIN THE PROPOSED BUILDING ROOF LINE REQUIRES.
- CONTRACTOR TO RAISE THE RIM ELEVATION ON EXISTING CATCH BASINS 1 AND 2 (EX CB 1,2) AND EXISTING STORM MANHOLE 2 (EX SDMH 2) TO THE ELEVATIONS SPECIFIED BELOW
- BEFORE INSTALLING CATCH BASINS 9 AND 10, CONTRACTOR IS TO LOCATE THE EXISTING UNDERGROUND GAS LINES AND CONTACT THE ENGINEER IMMEDIATELY SHOULD THE LOCATION OF THE GAS LINE CONFLICT WITH CATCH BASIN PLACEMENT OR STATED INVERTS.
- CONTRACTOR TO INSTALL TYPE I CATCH BASINS PER WSDOT STANDARD PLAN B-1 WITH GRATES PER WSDOT STANDARD PLAN B/2d
- CONTRACTOR TO COORDINATE DOWNSPOUT LOCATIONS WITH ARCHITECT. DOWNSPOUT LINES WILL CONNECT TO CATCH BASINS 7-10 AT THE INVERTS STATED BELOW, INDICATED WITH (DS) . DOWNSPOUT INVERTS AT THE BUILDING, FURTHEST FROM THE CATCH BASIN, ARE PROVIDED. CONTRACTOR TO DETERMINE PIPE LENGTHS AND FITTINGS. ADJUSTMENTS TO INVERT ELEVATIONS TO ACCOMMODATE FOR EXISTING UTILITIES MAY BE MADE IN THE FIELD IF, AND ONLY IF, AT LEAST 1.5' OF COVER OVER THE DOWNSPOUT LINES IS MAINTAINED AND MINIMUM PIPE SLOPES ARE FOLLOWED (S = 0.010 MIN FOR 6" PIPES). CONFIRMATION WITH ENGINEER IS RECOMMENDED PRIOR TO INVERT CHANGES. CLEANOUTS TO BE INSTALLED PER DETAILS A/3.2 AND B/3.2, AS APPROPRIATE.

CB 1 SEWER, BURLINGTON CB, STD GRATE
RIM = 8.53
IE 6" PVC OUT, E = 7.03

CB 2 SEWER, BURLINGTON CB, STD GRATE
RIM = 8.54
IE 6" PVC IN, W = 6.62
IE 6" PVC OUT, E = 6.62

CB 3 SEWER, BURLINGTON CB, STD GRATE
RIM = 8.51
IE 6" PVC IN, W = 6.21
IE 6" PVC OUT, E = 6.21

CB 4 TYPE I CB, STD GRATE
RIM = 8.58
IE 6" PVC OUT, E = 6.58

CB 5 TYPE I CB, STD GRATE
RIM = 8.57
IE 6" PVC IN, W = 6.19
IE 6" PVC OUT, E = 6.19

CB 6 1-CARTRIDGE STORMFILTER
SEE DETAIL ON SHEET C4.0
RIM = 8.57
IE 6" PVC IN, W = 5.76
IE 6" PVC OUT, E = 5.76

CB 7 TYPE I CB, STD GRATE
RIM = 8.31
IE 6" PVC IN, N (DS) = 5.44
IE 6" PVC IN, W = 5.44
IE 8" PVC OUT, SE = 5.44

EX CB 1 TYPE I CB, STD GRATE
RIM = 7.96 (RAISED RIM)
IE 12" PVC, N,S,E = 3.36
IE 6" PVC IN, NW = 5.21

CB 8 TYPE 1 CB, STD GRATE
RIM = 7.81
IE 12" PVC IN, S = 2.92
IE 6" PVC IN, W (DS) = 5.29
IE 12" PVC OUT, N = 2.92

EX SDMH 2 48" MANHOLE, STD GRATE
RIM = 8.15 (RAISED RIM)
IE 30" PVC IN, W = 1.44
IE 12" PVC IN, N,S = 2.69
IE 30" PVC OUT, E = 1.44

CB 9 TYPE 1L CB, STD GRATE
RIM = 7.72
IE 6" PVC IN, S (DS) = 5.72
IE 6" PVC OUT, N = 2.80

CB 10 TYPE 1L CB, STD GRATE
RIM = 7.79
IE 6" PVC IN, S (DS) = 5.79
IE 6" PVC OUT, N = 2.88

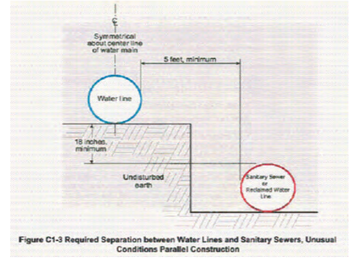


Figure C1-3 Required Separation between Water Lines and Sanitary Sewers, Unusual Conditions Parallel Construction

DOE MINIMUM SEPARATIONS
NOT TO SCALE
A 2.1

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ENGINEERING, SURVEYING & LAND DEVELOPMENT SERVICES
P.O. Box 1705 • 1111 Cleveland Avenue, Suite 202
Mount Vernon, WA 98273 Tel: 360-404-2010

SHEET REVISIONS:

NO.	DATE	DESCRIPTION	APPROVED
1	9.22.23	ARCHITECT REVISION	P.L.S.

CALL 48 HOURS BEFORE YOU DIG 1.800.424.5555

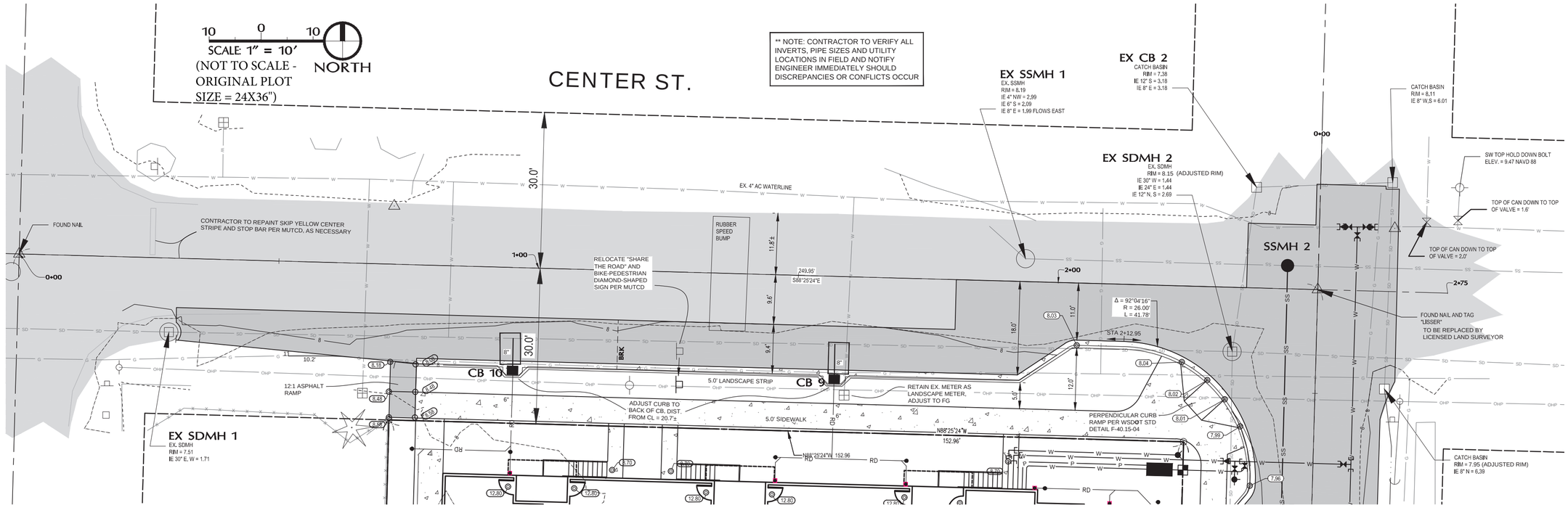
PROJECT:
CENTER STREET MIXED-USE
FOR
KSA INVESTMENTS, LLC

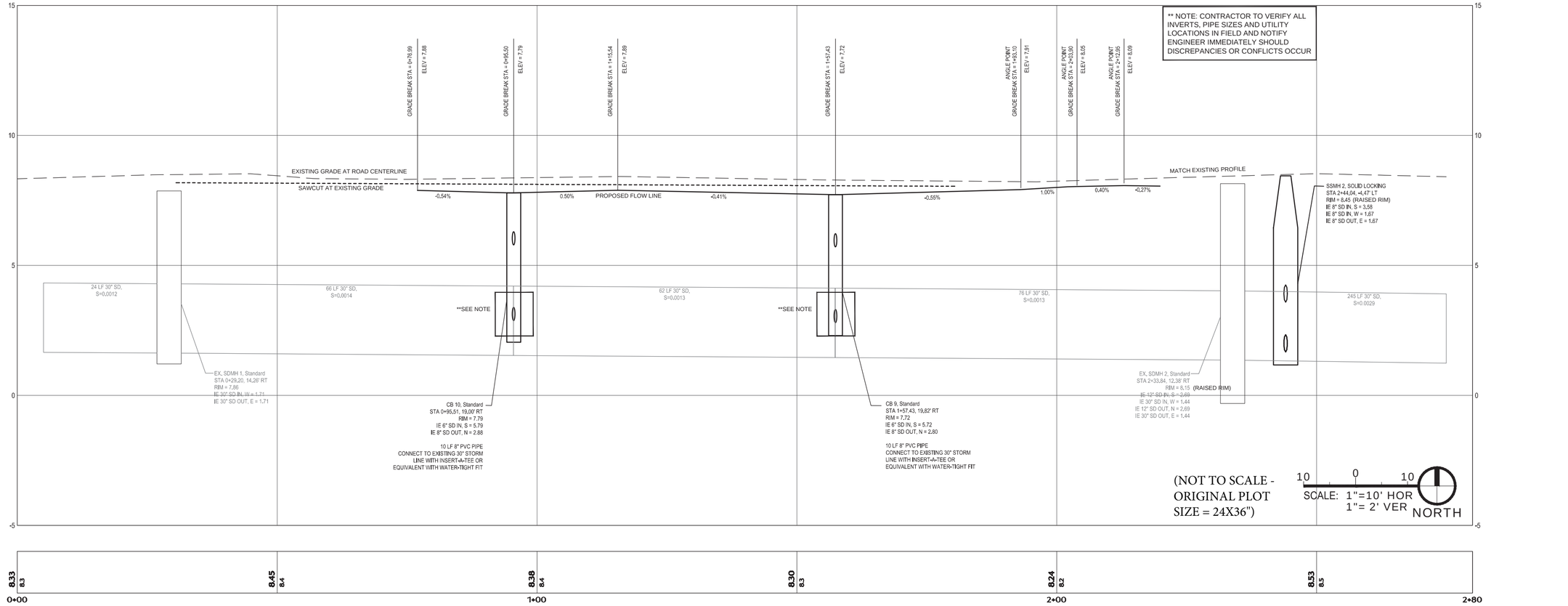
SHEET DESCRIPTION:
UTILITY PLAN

PATRICK L. SEVERIN
STATE OF WASHINGTON
22226
REGISTERED
PROFESSIONAL ENGINEER

SCALE:	1" = 20'
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG
SHEET NUMBER:	C2.1

IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA CONNER, WASHINGTON





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NO.	DATE	DESCRIPTION	APPROVED
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PROJECT:
CENTER STREET MIXED-USE
FOR
KSA INVESTMENTS, LLC

SHEET DESCRIPTION:
CENTER STREET ROADWAY IMPROVEMENTS
PLAN & PROFILE

PATRICK L. SEVERIN
STATE OF WASHINGTON
32226
REGISTERED PROFESSIONAL ENGINEER

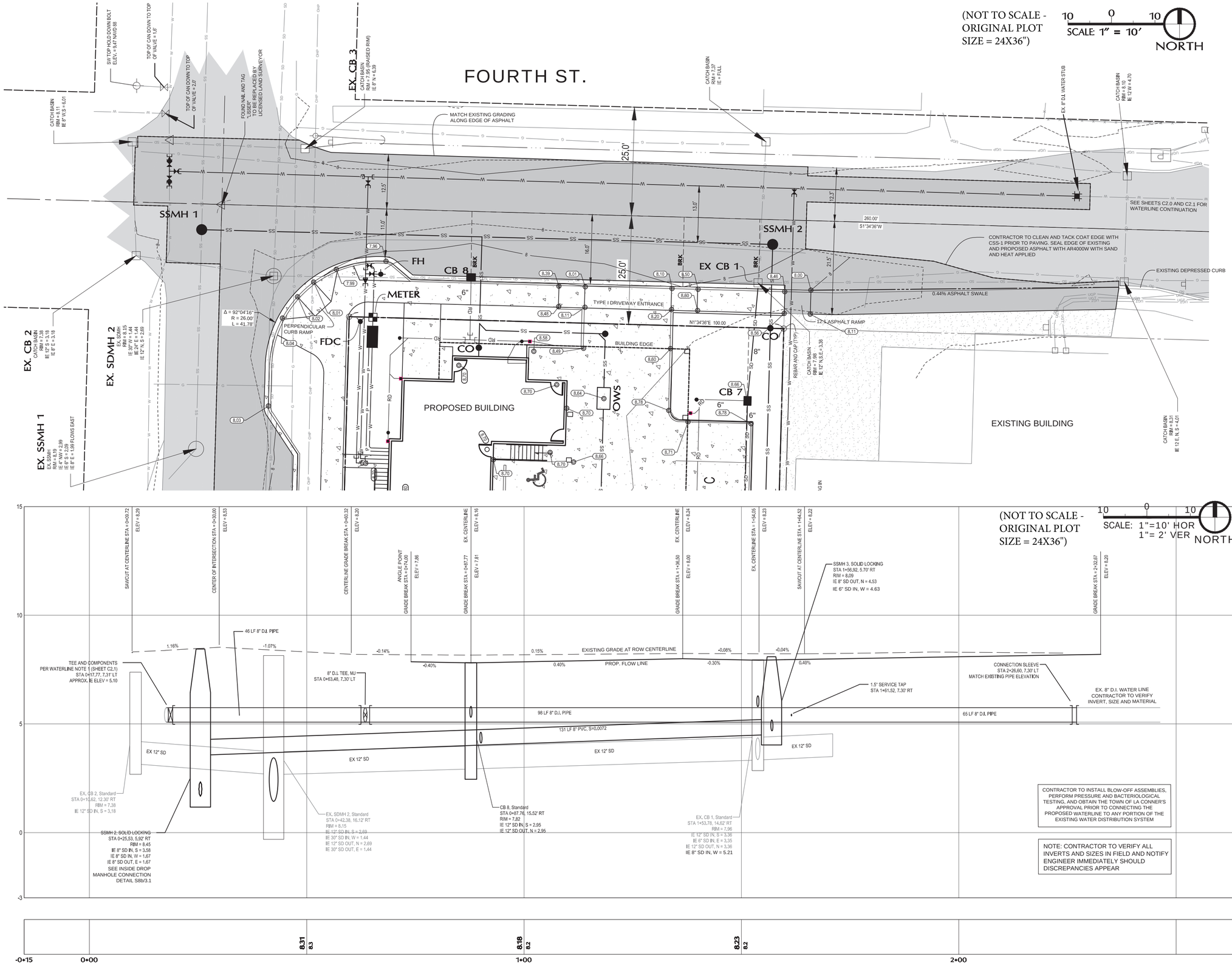
SCALE:	AS NOTED
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG

SHEET NUMBER:
C2.2

IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA CONNER, WASHINGTON

(NOT TO SCALE - ORIGINAL PLOT SIZE = 24X36")

10 0 10
SCALE: 1" = 10'
NORTH



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PATRICK L. SEVERIN — STATE OF WASHINGTON — 32226 — REGISTERED PROFESSIONAL ENGINEER

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DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
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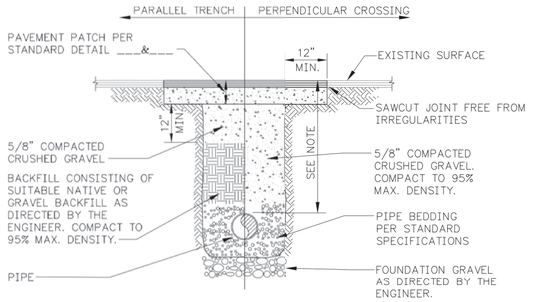
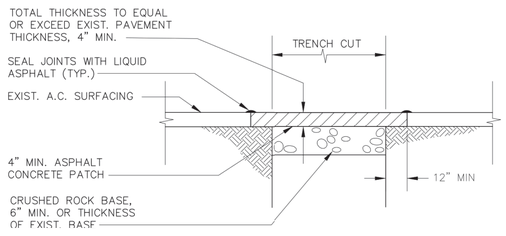
DWG NAME:	21098PLN.DWG
SHEET NUMBER:	C2.3



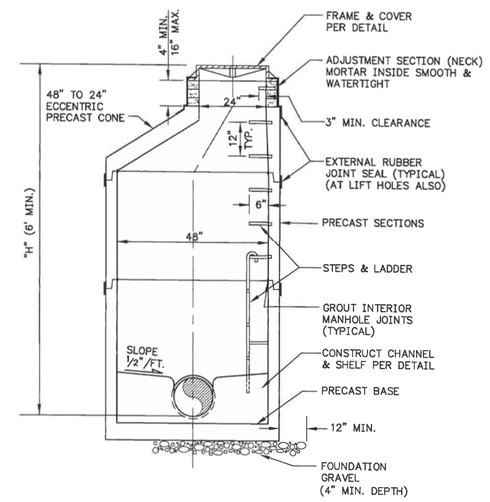
G1 3.0 TYPICAL TRENCH SECTION



NOTE: ASPHALT TREATED BASE MAY BE SUBSTITUTED FOR THE FIRST LIFT ASPHALT CONCRETE AND CRUSHED BASE. SEE SPECIFICATIONS.

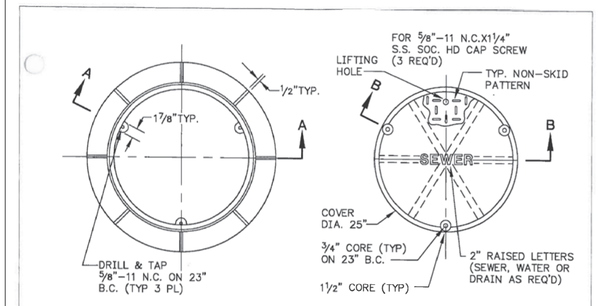
G3 3.0 ASPHALT CONCRETE PATCHING

REV 12/01



NOTES:
1. WALL OPENING REQUIREMENTS FOR PIPE:
   A. HOLE SIZE AS REQUIRED FOR KOR-N-SEAL WEDGE KORBAND PIPE CONNECTOR OR GASKETED COLLAR.
   B. 8" MINIMUM BETWEEN HOLES
   C. MAXIMUM PIPE SIZE 21"

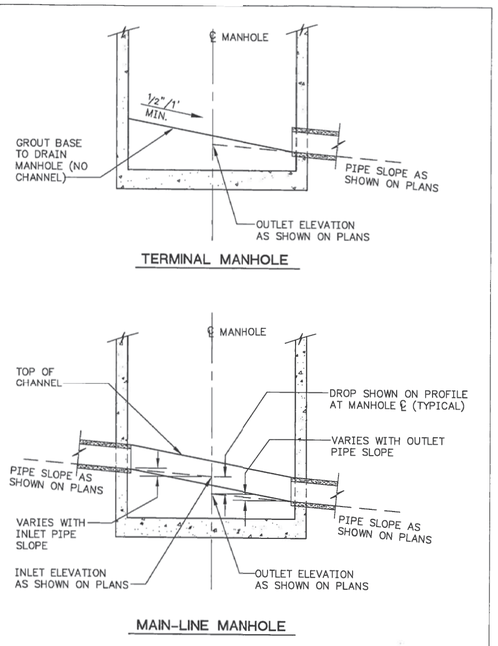
S1 3.0 TYPE I - 48" MANHOLE



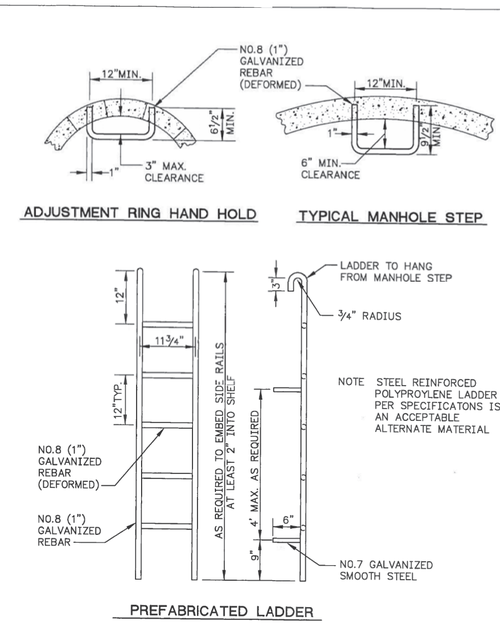
SECTION A-A SECTION B-B

NOTES:
1. DRILL LUG HOLES FULL DEPTH
2. BOLT COVER TO LUGS IN RING
3. PROVIDE 7/8" LIFTING HOLE
4. DUCTILE IRON COVER, CAST IRON FRAME, SS HEX CAP SCREWS
5. SEE SPECIFICATION 02605 FOR ADDITIONAL REQUIREMENTS
6. FRAME AND COVER PER APWA

S3 3.0 MANHOLE FRAME AND COVER



S4 3.0 MANHOLE CHANNELING



S5 3.0 MANHOLE STEPS AND LADDER

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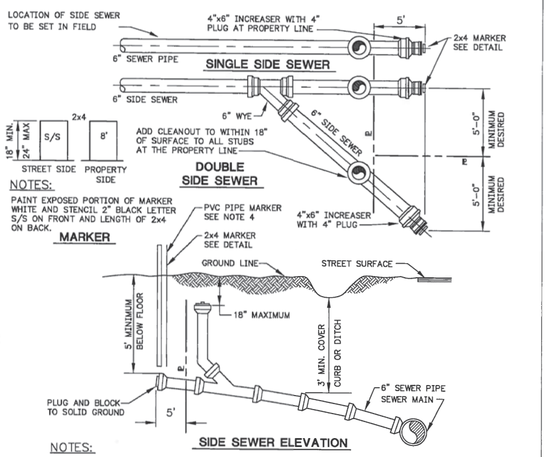
CENTER STREET MIXED-USE
FOR
KSA INVESTMENTS, LLC

SHEET DESCRIPTION:
SITE & SEWER DETAILS

PATRICK L. SEVERIN STATE OF WASHINGTON 32226 REGISTERED PROFESSIONAL ENGINEER

SCALE:	AS NOTED
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG
SHEET NUMBER:	C3.0

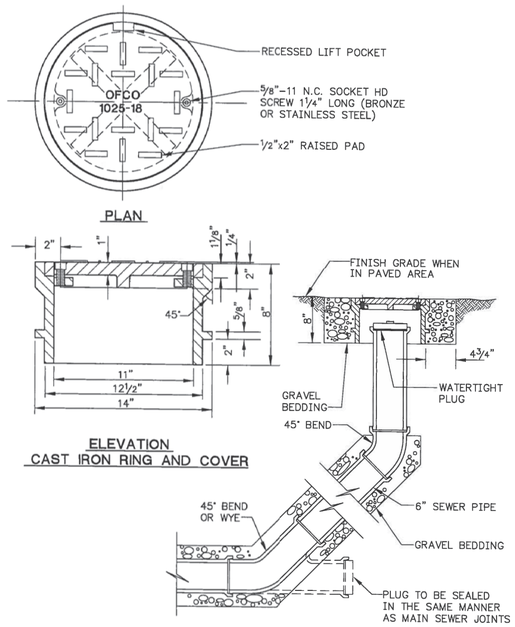


NOTES:

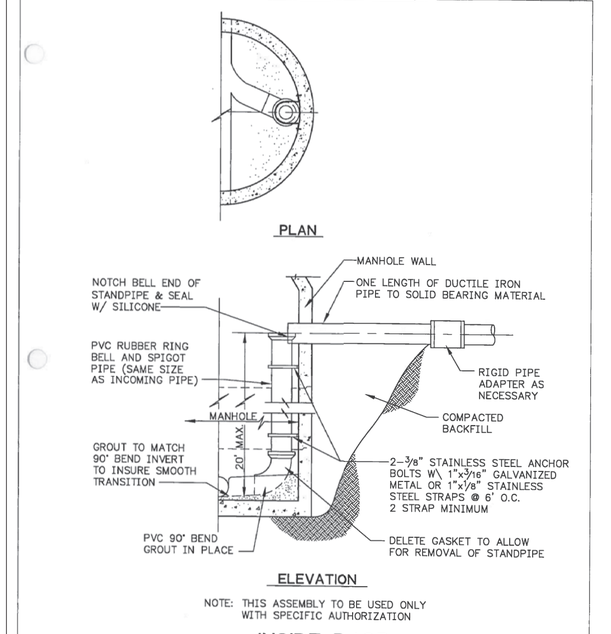
1. UNLESS OTHERWISE SHOWN ON PLAN, SIDE SEWER SHALL BE SLOPED 2% UP FROM SEWER LINE OR SHALL BE MIN. 3' COVER AT PROPERTY LINE OR 3.5' LOWER THAN THE LOWEST HOUSE ELEVATION, WHICHEVER IS LOWER.
2. 6" MINIMUM SIZE PIPE TO PROPERTY LINE. MAXIMUM DEFLECTION PER JOINT IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS.
3. MAXIMUM SLOPE = 2 VERTICAL : 1 HORIZONTAL
4. MINIMUM SLOPE = 2%
5. A 1¼" WHITE PVC PIPE, ASTM 2241 SDR 21 200 PSI SHALL BE PLACED VERTICALLY AT THE END OF EACH STUB AND SHALL RISE TWO FEET ABOVE FINISH GRADE LEVEL OR AS APPROVED BY SWSSD. BOTH ENDS OF THE PVC PIPE SHALL HAVE CAPS GLUED ON AND THE PIPE INTERIOR KEPT CLEAN FOR THE PURPOSE OF FUTURE DEPTH MEASUREMENT.
6. PIPE SHALL BE PVC ASTM D3034.

S6 / 3.1 STREET SIDE SEWER

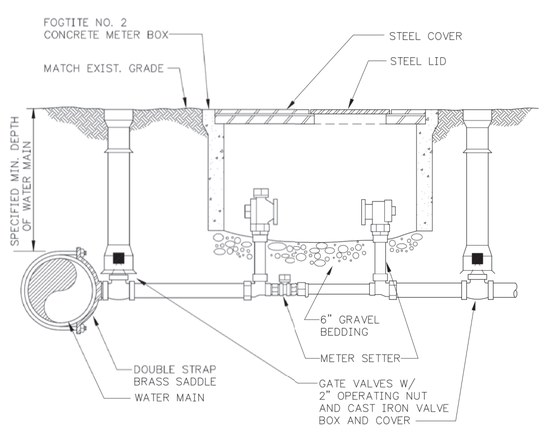
REV 02/06



S7 / 3.1 SEWER CLEANOUT



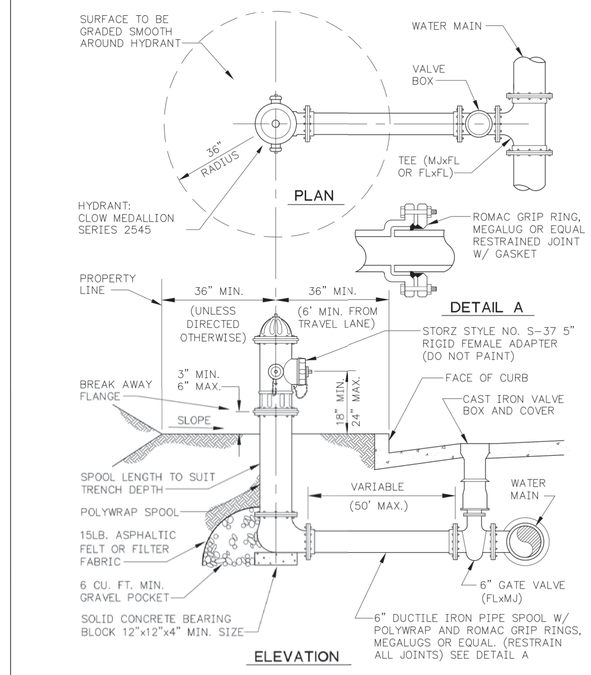
S8b / 3.1 INSIDE DROP MANHOLE CONNECTION



PARTS FOR 1½" AND 2" SERVICE

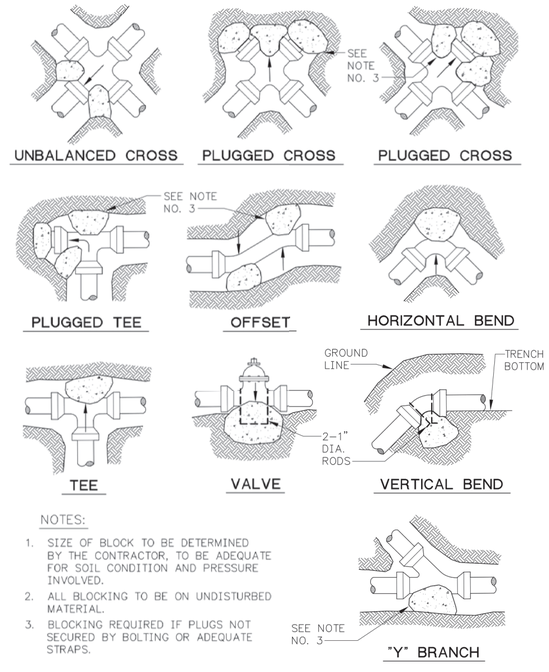
1 – DOUBLE STRAP BRASS SADDLE WITH BRONZE STRAP
4 – FORD PACKJOINT COUPLINGS/PLASTIC PIPE
SETTERS: 1½" FORD VBH6612BX LENGTH WITH LOCKING BYPASS
2" FORD VBH7712BX LENGTH WITH LOCKING BYPASS
(SETTER LENGTH WILL BE DETERMINED BY TOWN)
CONCRETE METER BOX WITH STEEL COVER AND METER LID FOGTITE NO. 2
2 – GATE VALVES: 2 INCH–RESILIENT WEDGE SEATED (APWA C–509)
1½ INCH–BRASS
2 – CAST IRON VALVE BOX WITH COVER
WATER SERVICE LINE 200 PSI IPS PE 3408, ASTM D–2239

W2 / 3.1 1½" AND 2" WATER SERVICE



W3 / 3.1 FIRE HYDRANT ASSEMBLY

REV: 7/24/18



NOTES:

1. SIZE OF BLOCK TO BE DETERMINED BY THE CONTRACTOR, TO BE ADEQUATE FOR SOIL CONDITION AND PRESSURE INVOLVED.
2. ALL BLOCKING TO BE ON UNDISTURBED MATERIAL.
3. BLOCKING REQUIRED IF PLUGS NOT SECURED BY BOLTING OR ADEQUATE STRAPS.

W4 / 3.1 CONCRETE THRUST BLOCKING

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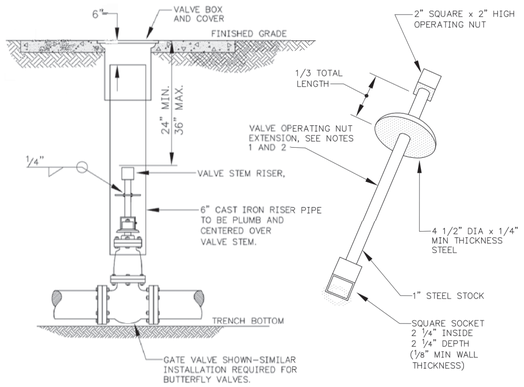
SHEET DESCRIPTION:
SEWER & WATER DETAILS

PATRICK L. SEVERIN — STATE OF WASHINGTON — 32226 — REGISTERED PROFESSIONAL ENGINEER

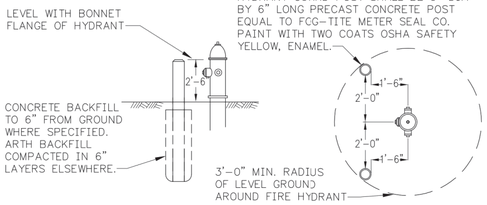
SCALE:	AS NOTED
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG
SHEET NUMBER:	C3.1



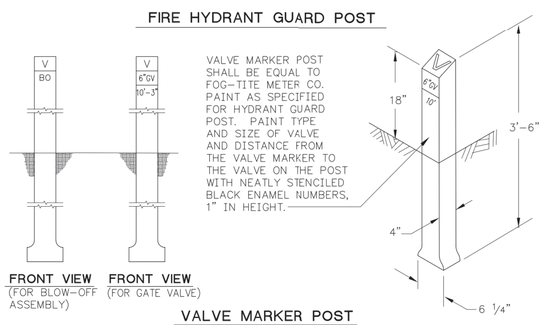
NOTES:

1. VALVE OPERATING NUT EXTENSIONS ARE REQUIRED WHEN THE VALVE NUT IS MORE THAN THREE (3) FEET BELOW FINISHED GRADE. EXTENSIONS ARE TO BE A MINIMUM OF ONE (1) FOOT LONG. ONLY ONE EXTENSION WILL BE ALLOWED PER VALVE.
2. ALL VALVE OPERATING NUT EXTENSIONS ARE TO BE MADE OF STEEL, SIZED AS NOTED, AND PAINTED WITH TWO (2) COATS OF METAL PAINT.

W6 / 3.2 VALVE OPERATING NUT EXTENSION



FIRE HYDRANT GUARD POST

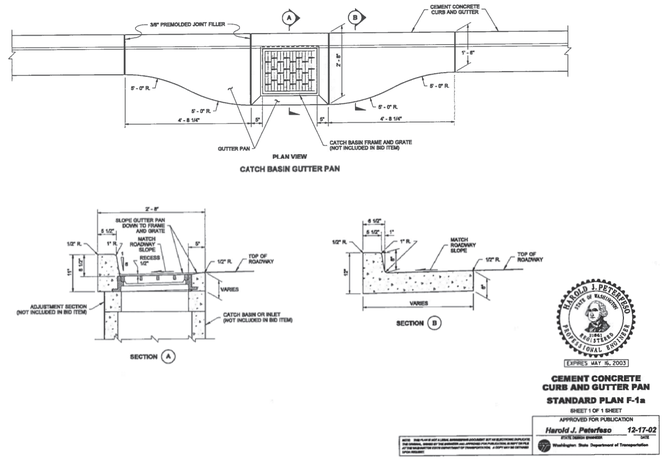


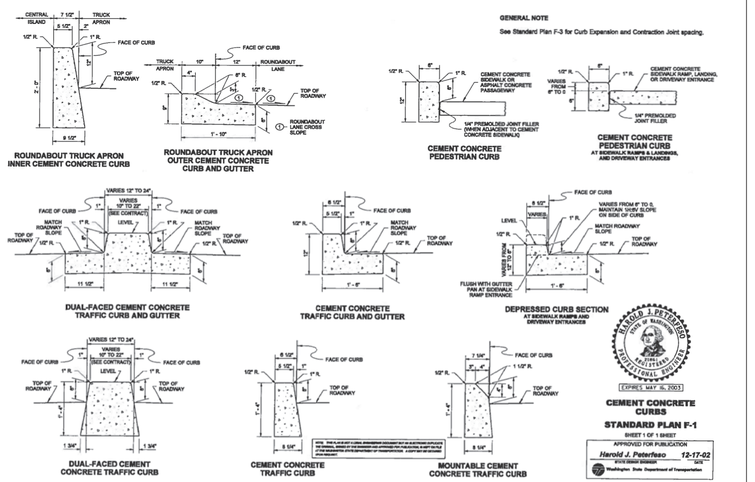
VALVE MARKER POST

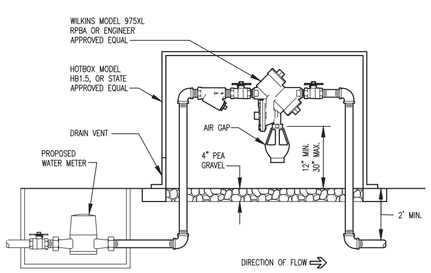
NOTES:

1. GUARD POSTS TO BE INSTALLED ONLY AS DIRECTED BY THE TOWN.
2. VALVE MARKERS TO BE USED FOR BLOW OFF AND MAINLINE VALVES OUTSIDE PAVED AREAS.

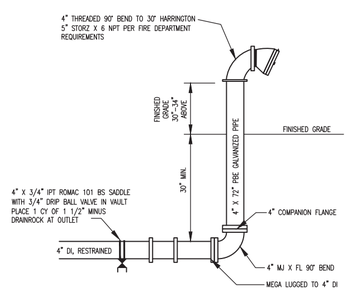
W8 / 3.2 VALVE MARKER POST
HYDRANT GUARD POST







REDUCED PRESSURE BACKFLOW ASSEMBLY DETAIL C / 3.2
NOT TO SCALE



FIRE DEPT. CONNECTION DETAIL D / 3.2
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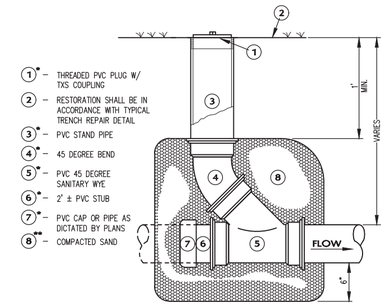
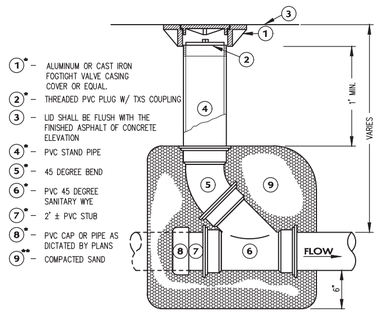


CLEANOUT DETAIL OUTSIDE ASPHALT OR CONCRETE AREAS A / 3.2
NOT TO SCALE



CLEANOUT DETAIL WITHIN ASPHALT OR CONCRETE AREAS B / 3.2
NOT TO SCALE

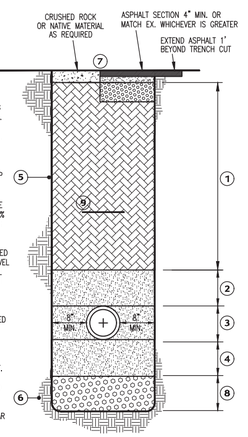
GENERAL NOTES:

- NOTE-1 RESTORATION SHALL CONSIST OF REMOVING ALL ROCKS GREATER THAN 1" IN DIAMETER, BLENDING THE TOP OF THE TRENCH WITH THE SURROUNDING GRADE AND REMOVING ALL SPOILS FROM THE SITE.

  IF THE EXISTING GRAVEL SHOULDER IS DISTURBED, REPAIR ACCORDING TO THE FOLLOWING REQUIREMENTS:

  A. 6" OF 5/8" MINUS CRUSHED SURFACING TOP COURSE PER SECTION 9-03.9(3), TO BE COMPACTED TO 95% OF MAXIMUM DENSITY.
  B. GRAVEL BORROW AS NECESSARY TO PREPARE ROAD PER SECTION 4-02 COMPACTED TO 95% OF MAXIMUM DENSITY. PLACE IN LOOSE LIFTS NOT EXCEEDING EIGHT INCHES.

- NOTE-2 PIPE BEDDING AND COVER MATERIAL SHALL BE USED IN ALL TRENCHES REGARDLESS OF LOCATION. GRAVEL BORROW TRENCH BACKFILL SHALL BE USED IN ALL TRENCHES UNDER CRUSHED ROCK, CONSTRUCTION TRAFFIC AREAS, AND WITHIN FOUR FEET OF THE ABOVE-MENTIONED CONDITIONS.
- NOTE-3 ANY SPECIAL CONDITIONS MUST FIRST BE APPROVED BY THE ENGINEER.
- NOTE-4 SAWCUT EXISTING ASPHALT 1' BEYOND EDGE OF TRENCH. TACK COAT FACE OF SAWCUT AND SEAL JOINT WITH CSS-1 AND SAND, APPLIED WITH HEAT.
- NOTE-5 ALL DENSITIES FOR GRAVEL SHALL BE PROCTORED USING ASTM D-1557. ASPHALT SHALL BE PROCTORED USING STATE APPROVED METHODS. DENSITIES SHALL BE DETERMINED USING A NUCLEAR DENSOMETER CONFORMING TO ASTM D-2950.
- NOTE-6 AT LOCATIONS WITHIN THE EXISTING ASPHALT ROADWAY OR WHERE THE EDGE OF THE UTILITY TRENCH IS LESS THAN 4' FROM THE EDGE OF ASPHALT, THE TRENCH SHALL BE BACKFILLED WITH CDF AS DIRECTED BY THE TOWN OF LA CONNER.



TYPICAL UTILITY TRENCH DETAIL E / 3.2
NOT TO SCALE

TRENCH NOTES:

1. GRAVEL BORROW PLACED IN LOOSE LIFTS NOT EXCEEDING 8" IN DEPTH AND COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY AT LOCATIONS WHERE REQUIRED AND/OR SPECIFIED. THE PERCENTAGE PASSING THE #200 SIEVE SHALL NOT EXCEED 5%. NATIVE MATERIAL, WHERE ACCEPTABLE, MAY BE USED. PLACE NATIVE MATERIAL IN LIFTS NOT EXCEEDING 12 INCHES AND COMPACT TO A MINIMUM OF 90%.
2. 6" OF SAND, BUCKSHOT OR GRAVEL 100% PASSING THE 1/4" SCREEN TO BE HAND-COMPACTED ABOVE THE CROWN OF THE PIPE.
3. HAND-COMPACTED SAND, BUCKSHOT OR GRAVEL 100% PASSING THE 1/4" SCREEN TO BE TAMPED AROUND AND UNDER THE PIPE. THICKNESS EQUALS OUTSIDE DIAMETER OF PIPE.
4. 6" OF SAND, BUCKSHOT OR GRAVEL 100% PASSING 1/4" SCREEN, HAND-COMPACTED.
5. TRENCH LINE.
6. UNDISTURBED EARTH.
7. SURFACE RESTORATION SHALL BE IN ACCORDANCE WITH GENERAL NOTE-1.
8. IN TRENCHES WITH SOFT, YIELDING MATERIAL, AS DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL OVER-EXCAVATE TO 2' BELOW PIPE INVERT AND BACKFILL WITH 2 1/2" MINUS BALLAST AGGREGATE THE BOTTOM OF PIPE BEDDING.
9. PLACE DETECTABLE TRACER TAPE (MIN. 1' ABOVE PIPE) PER SECTION 9-15.18 OF THE 2023 STANDARD SPECIFICATIONS.

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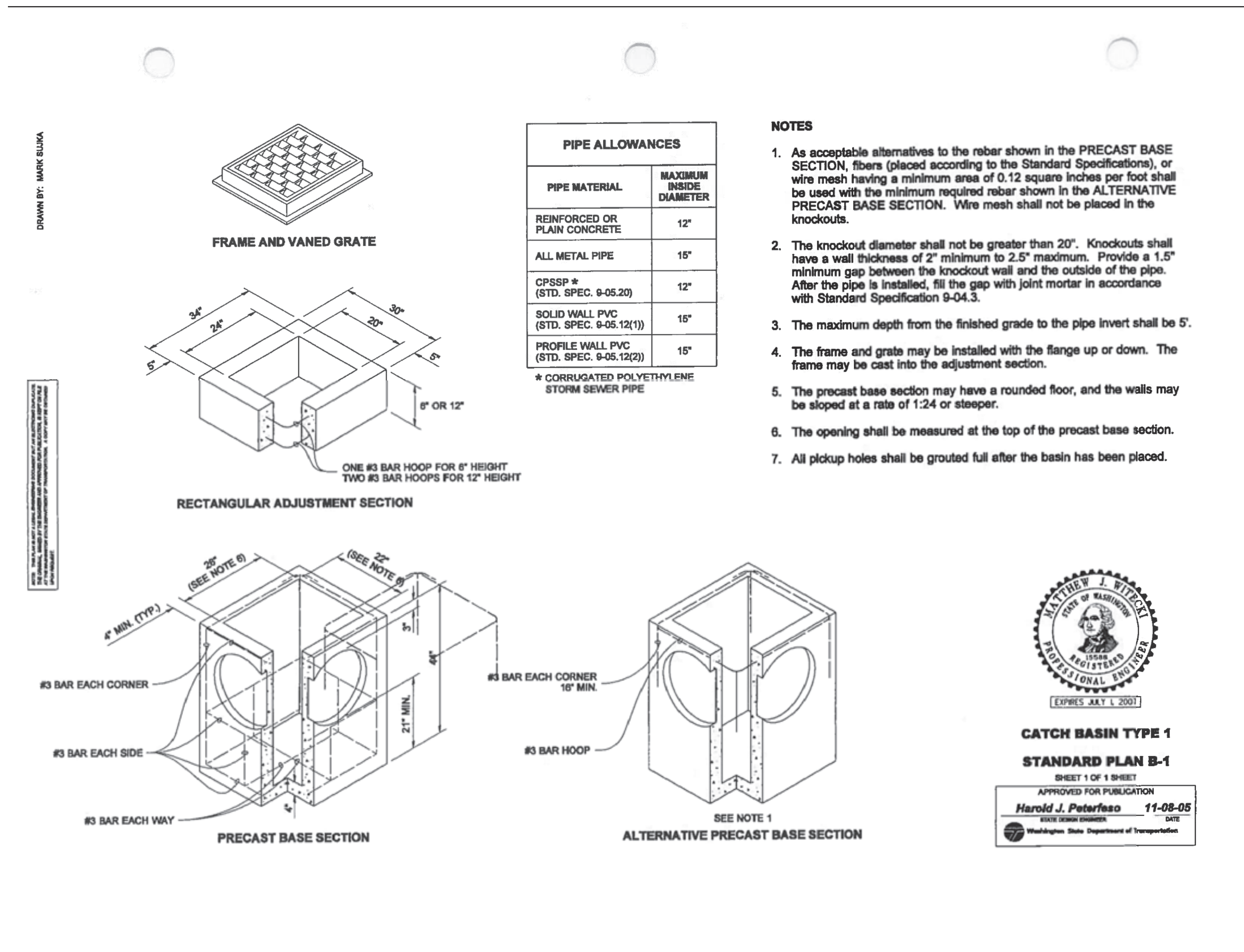
PROJECT:
CENTER STREET MIXED-USE
FOR
KSA INVESTMENTS, LLC

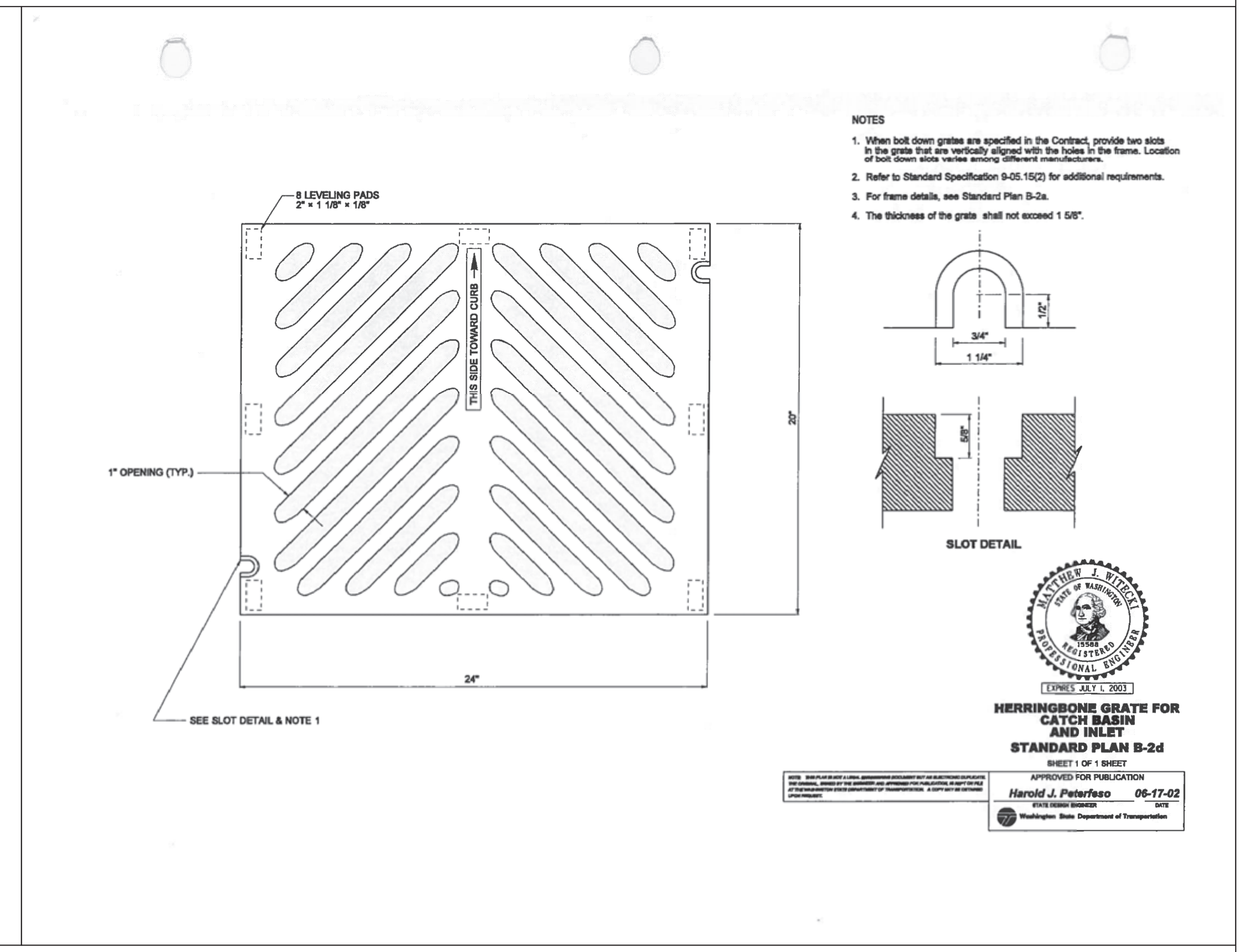
SHEET DESCRIPTION:
WATER, WSDOT STANDARD PLANS & CONSTRUCTION DETAILS

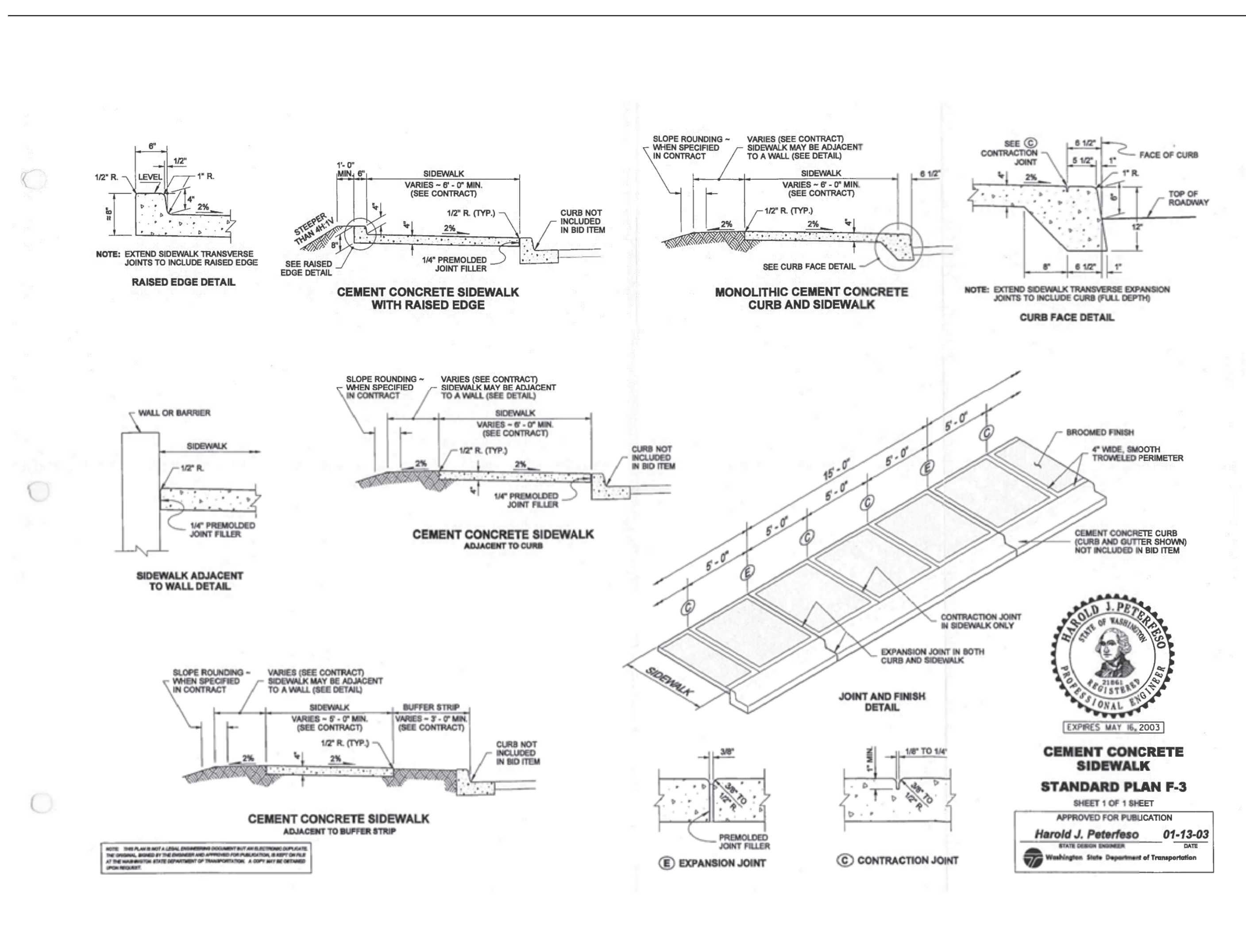
PATRICK L. SEVERIN
STATE OF WASHINGTON
32226
REGISTERED PROFESSIONAL ENGINEER

SCALE:	AS NOTED
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG
SHEET NUMBER:	C3.2

IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M.   LA CONNER, WASHINGTON







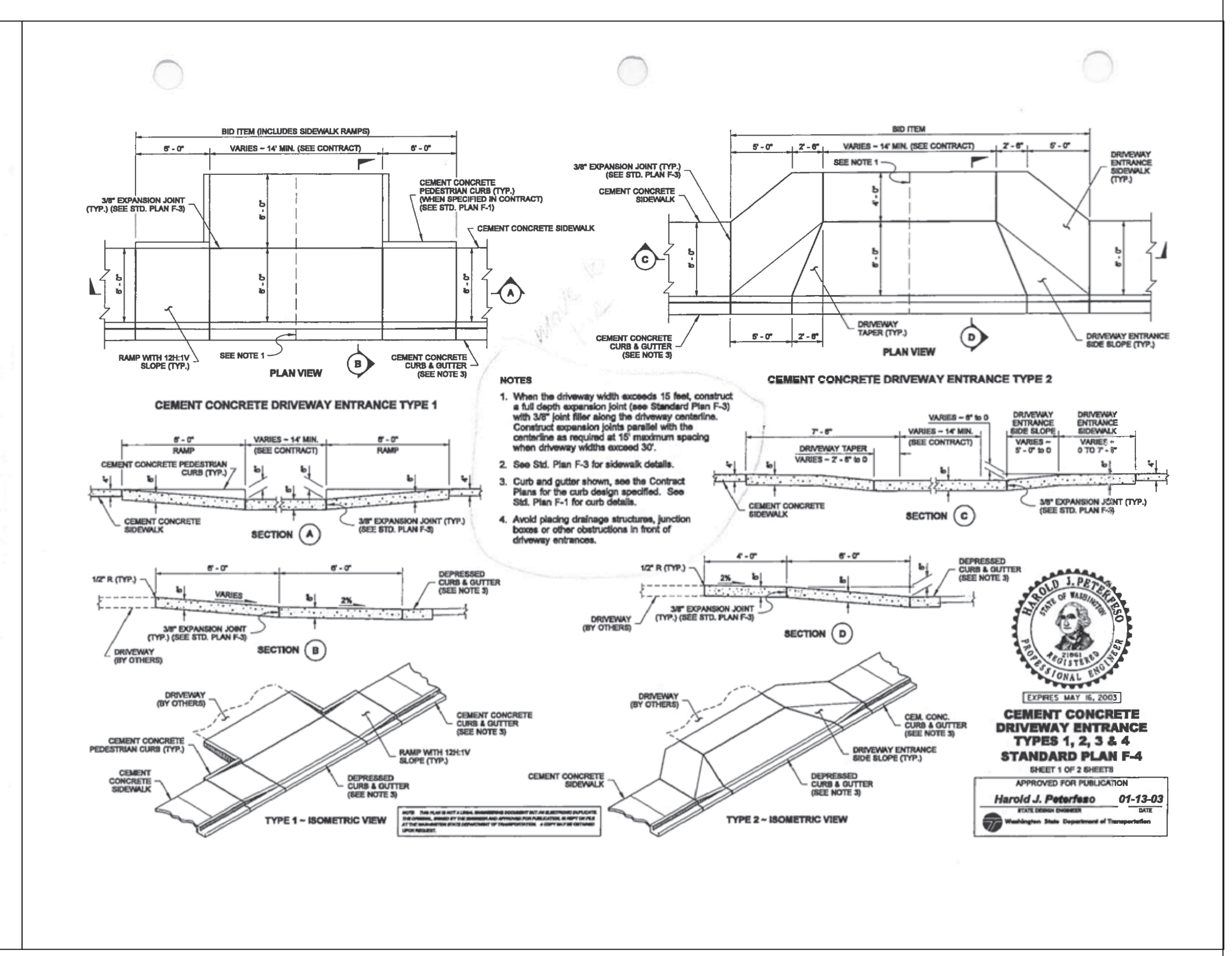


Figure II-3.26: Cross Section of Sediment Trap

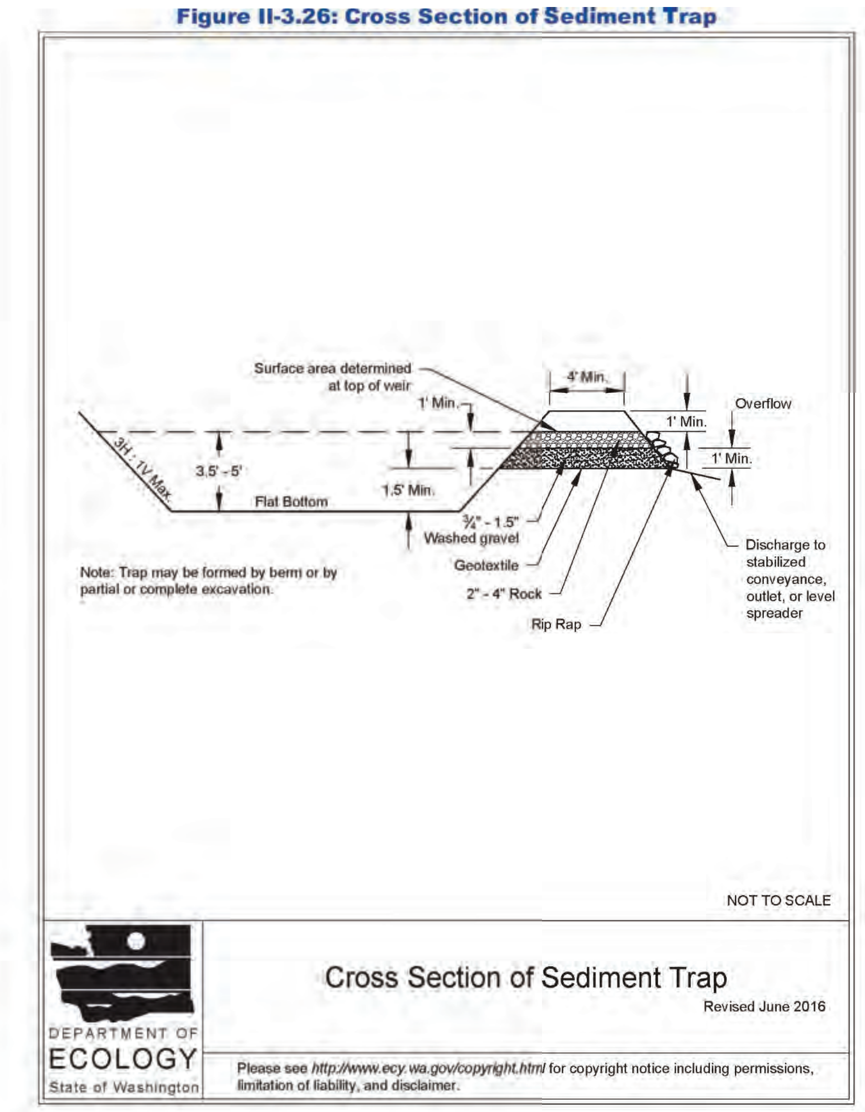


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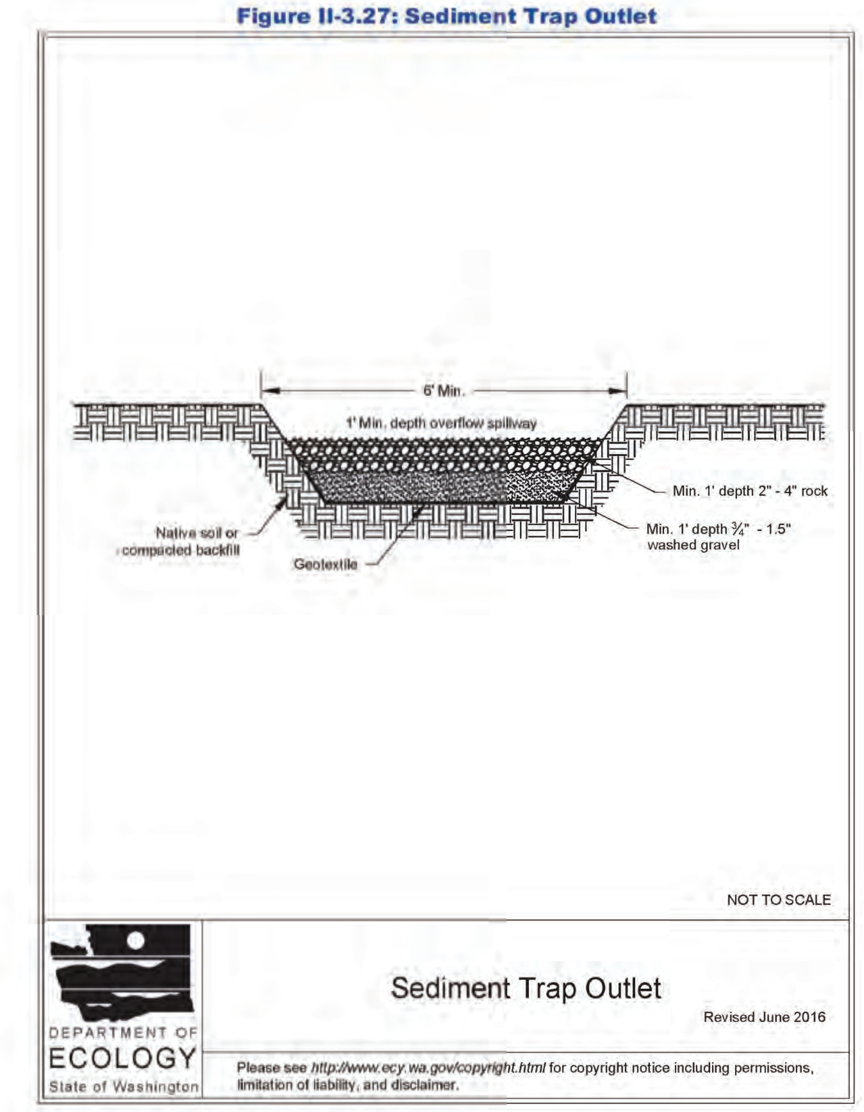
Figure II-3.27: Sediment Trap Outlet



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PROJECT:

CENTER STREET MIXED-USE
FOR
KSA INVESTMENTS, LLC

SHEET DESCRIPTION:

WSDOT STANDARD PLANS & SEDIMENT TRAP DETAIL

SCALE:	AS NOTED
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG

SHEET NUMBER:

C3.3

GENERAL CONSTRUCTION NOTES

1. ALL CONSTRUCTION AND MATERIALS SHALL CONFORM TO THE 2023 STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION AS PREPARED BY WASHINGTON STATE DEPARTMENT OF TRANSPORTATION AND THE AMERICAN PUBLIC WORKS ASSOCIATION (WSDOT/APWA), HEREIN REFERRED TO AS THE "STANDARD SPECIFICATIONS". REFERENCES WILL BE MADE TO THE STANDARD SPECIFICATIONS MANUAL AND THE STANDARD PLANS BOOK.

2. EXISTING UTILITIES HAVE BEEN TAKEN FROM AVAILABLE FIELD AND OFFICE RECORDS. THE CONTRACTOR IS RESPONSIBLE FOR UTILIZING THE ONE-CALL UTILITY LOCATE SERVICE, 1-800-424-5555, A MINIMUM OF TWO WORKING DAYS PRIOR TO ANY CONSTRUCTION. DAMAGES TO THE EXISTING UTILITIES RESULTING FROM THIS CONSTRUCTION SHALL BE REPAIRED BY AND AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL VERIFY ANY POTENTIAL UTILITY CONFLICTS PRIOR TO CONSTRUCTION.

3. THE CONTRACTOR SHALL MAKE DAILY EFFORTS TO KEEP THE SITE IN A NEAT AND ORDERLY CONDITION TO THE SATISFACTION OF THE OWNER, ENGINEER, AND TOWN OF LA CONNER ENGINEERING DEPARTMENT. IF CONSTRUCTION OCCURS DURING RAINY WEATHER CONDITIONS, THEREBY CAUSING DEBRIS TO BE TRACKED ONTO THE EXISTING ASPHALT, THE CONTRACTOR SHALL CONSTRUCT A QUARRY SPALL ROADWAY 20-FEET WIDE BY 100-FEET LONG MINIMUM. THE CONTRACTOR IS RESPONSIBLE FOR DAMAGES TO EXISTING IMPROVEMENTS RESULTING FROM THIS CONSTRUCTION.

4. DURING ALL PHASES OF CONSTRUCTION, THE CONTRACTOR SHALL SWEEP AND REMOVE ALL DEBRIS TRACKED ONTO THE EXISTING ROADS. FAILURE TO KEEP ROAD FREE FROM DEBRIS OFF EXISTING ROADWAY MAY CAUSE WORK STOPPAGE. THE CONTRACTOR SHALL ALSO WATER THE SITE (IF REQUIRED) TO REDUCE CONSTRUCTION DUST.

5. AT ALL TIMES, TRAFFIC LANES SHALL BE MAINTAINED ON EXISTING ROADS. TEMPORARY AND PARTIAL ROAD CLOSURE SHALL BE APPROVED BY TOWN OF LA CONNER ENGINEERING, FIRE, AND POLICE DEPARTMENTS PRIOR TO CONSTRUCTION. DURING CONSTRUCTION WITHIN THE RIGHT-OF-WAYS, THE CONTRACTOR SHALL PROVIDE ALL LABOR, MATERIALS, AND EQUIPMENT FOR TRAFFIC CONTROL AND CONSTRUCTION WARNING/CONTROL SIGNS.

6. THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL REGULATIONS CONCERNING DISPOSAL OF MATERIALS. ALL ASPHALT, CONCRETE, BRICK, AND STRUCTURES REMOVED FROM THIS SITE SHALL BE DISPOSED OF IN AN APPROVED SITE OBTAINED BY THE CONTRACTOR.

7. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING AND APPLYING FOR ALL PERMITS ASSOCIATED WITH THIS CONSTRUCTION NOT OBTAINED BY THE OWNER AND/OR ENGINEER.

8. THE ENGINEER SHALL BE IMMEDIATELY NOTIFIED PRIOR TO CONSTRUCTION IF ANY DISCREPANCY IN PLANS AND EXISTING CONDITIONS IS DISCOVERED.

9. THE CONTRACTOR SHALL STOCKPILE CLEAN, NATIVE TOPSOIL MATERIALS, FREE OF SOD AND DEBRIS LARGER THAN TWO INCHES, TO BE USED AS FILL IN THE PROPOSED LANDSCAPE AREAS. THE CONTRACTOR SHALL STOCKPILE EXCESS NATIVE MATERIAL ON THE SITE AS DIRECTED BY THE OWNER. EXCESS AND UNSUITABLE NATIVE MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT AN APPROVED DUMPSITE RETAINED BY THE CONTRACTOR. DEBRIS AND STRUCTURES SHALL BE REMOVED FROM SITE AND DISPOSED AT AN APPROVED DISPOSAL SITE RETAINED BY THE CONTRACTOR.

10. ALL PORTIONS OF THE SITE UNDER THE PROPOSED ASPHALT SHALL BE EXCAVATED TO EXPOSE A NON-ORGANIC MATERIAL SUITABLE FOR CONSTRUCTION. THE SUBGRADE SHALL BE PREPARED CONFORMING TO SECTION 2-06.3(1), AND COMPACTED TO A MINIMUM OF 105% MAXIMUM DENSITY WITH A MINIMUM TEN-TON SELF-PROPELLED VIBRATORY ROLLER. ANY AREAS THAT INDICATE PUMPING, UNSTABLE, OR YIELDING SOIL CONDITIONS SHALL BE OVER EXCAVATED AND REPLACED WITH TWO INCH TO FOUR- INCH QUARRY SPALLS. STOCKPILED MATERIAL SHALL BE PROTECTED FROM OVER- SATURATION BY RAINFALL OR PONDED WATER. FINAL GRADED CONDITIONS SHALL BE RAKED TO REMOVE ALL DEBRIS LARGER THAN ONE-INCH FROM THE SURFACE.

11. ORGANIC MATERIAL AND NON SUITABLE NATIVE MATERIAL DISCOVERED DURING SUBGRADE EXCAVATION AND SITE PREPARATION SHALL BE ENTIRELY REMOVED AND DISPOSED OF BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.

12. DURING PERIODS OF RAINFALL, THE CONTRACTOR SHALL PREVENT WATER FROM STANDING ON THE SUBGRADE OR ON THE PREPARED GRAVEL SUBGRADE. THE CONTRACTOR IS RESPONSIBLE FOR SUBGRADE PROTECTION, REPAIR AND REPLACEMENT OF SUBGRADE MATERIALS SHALL BE PAID FOR BY AND AT THE CONTRACTOR'S EXPENSE. STORM RUNOFF SHALL BE DISCHARGED TO THE STORM SYSTEM OR ON SITE LOCATION THAT WILL NOT IMPACT THE NEIGHBORING PROPERTIES, THIS PROJECT, DOWNSTREAM CONVEYANCE SYSTEM. THE CONTRACTOR IS REQUIRED TO PROVIDE TEMPORARY DITCHING AND PUMPS TO REMOVE ALL STANDING WATER FROM THE WORK AREA.

13. STRUCTURAL FILL TO FILL IN THE SWALE IS TO BE GLACIAL TILL, OR AS APPROVED BY THE PROJECT GEOTECHNICAL ENGINEER. STRUCTURAL FILL FOR DRY WEATHER CONSTRUCTION MAY CONTAIN UP TO 10 PERCENT FINES (THAT PORTION PASSING THE U.S. NO. 200 SIEVE) BASED ON THE PORTION PASSING THE U.S. NO. 4 SIEVE. IMPORTED FILL HAVING MORE THAN 10 PERCENT FINES IS TO BE REVIEWED BY THE DESIGN TEAM PRIOR TO THE START OF CONSTRUCTION. STRUCTURAL FILL FOR WET WEATHER CONSTRUCTION IS TO CONTAIN LESS THAN FIVE PERCENT FINES. THE OWNER SHALL PROVIDE INITIAL GRADATION AND TEST RESULTS TO THE ENGINEER FOR APPROVAL. GRADATION AND PROCTOR TEST RESULTS SHALL BE SUPPLIED BY THE OWNER PER 2000 TONS OF IMPORTED MATERIAL. CRITERIA FOR COMPACTED TILL LINERS IS GIVEN IN SECTION V-1.3.3 OF THE STORMWATER MANAGEMENT MANUAL FOR WESTERN WASHINGTON. REFER TO THE GEOTECHNICAL REPORT FOR ADDITIONAL INFORMATION ON STRUCTURAL FILL SPECIFICATIONS

ALL AREAS THAT DO NOT MEET THE REQUIRED SPECIFICATIONS SHALL BE RE-COMPACTED AND RETESTED AT NO COST TO THE OWNER.

14. GRAVEL BORROW (IMPORTED STRUCTURAL FILL) SHALL COMPLY WITH SECTION 9-03.14(1) OF THE 2023 STANDARD SPECIFICATIONS OR THE IMPORTED STRUCTURAL FILL REQUIREMENTS OUTLINED IN THE GEOTECHNICAL REPORT, WHICHEVER IS MORE STRINGENT. GRAVEL BASE SHALL CONSIST OF WELL GRADED SAND AND GRAVEL CONFORMING TO THE REQUIRED SPECIFICATIONS. THE PERCENT PASSING THE US NO. 200 SIEVE SHALL NOT EXCEED 7%. ALL GRAVEL BASE IMPORTED TO THE SITE SHALL HAVE A CONSISTENT GRADATION. PRIOR TO IMPORTING ANY GRAVEL BASE MATERIAL, THE CONTRACTOR SHALL PROVIDE GRADATION AND TEST RESULTS TO THE ENGINEER FOR APPROVAL. GRADATION AND PROCTOR TEST RESULTS SHALL BE SUPPLIED BY THE CONTRACTOR PER 2000 TONS OF IMPORTED MATERIAL. THE CONTRACTOR SHALL RETAIN LICENSED PERSONNEL TO PERFORM COMPACTION TESTS FOR THE FOLLOWING:

A. TOP OF PREPARED GRAVEL BORROW WITHIN THE PARKING LOT AND ROAD SECTION ON A 50-FOOT GRID/INTERVAL FOR GRAVEL FILLS GREATER THAN TWO FEET.

B. ONE TEST ADJACENT TO ALL STRUCTURES WITHIN THE ASPHALT.

C. TRENCHES WITH THREE FEET OR LESS OF GRAVEL TRENCH BACKFILL: TOP CENTER OF UTILITY TRENCH AT 50-FOOT INTERVALS.

D. TRENCHES WITH MORE THAN THREE FEET OF GRAVEL TRENCH BACKFILL: TOP CENTER OF UTILITY TRENCH AND MID-DEPTH OF TRENCH, BOTH AT 50-FOOT INTERVALS. ALL TEST RESULTS SHALL MEET OR EXCEED THE SPECIFICATIONS.

ALL AREAS THAT DO NOT MEET THE REQUIRED SPECIFICATIONS SHALL BE RE-COMPACTED AND RETESTED AT NO ADDITIONAL COST TO THE OWNER.

15. CRUSHED SURFACING TOP COURSE SHALL CONFORM TO SECTION 9-03.9(3) OF THE 2023 STANDARD SPECIFICATIONS. EACH LIFT SHALL BE MECHANICALLY COMPACTED TO A MINIMUM OF 105% MAXIMUM DENSITY AS DETERMINED BY ASTM D- 1557 TESTING PROCEDURE. PLACEMENT AND GRADING OF COMPACTED CRUSHED TOP COURSE MATERIAL WITHIN THE ASPHALT AREAS SHALL HAVE A TOLERANCE OF PLUS OR MINUS ONE-HALF INCH FROM THE DESIGNATED TOP OF CRUSHED SURFACING TOP COURSE, THE OWNER SHALL PROVIDE GRADATION AND DEGRADATION TEST RESULTS TO THE ENGINEER FOR APPROVAL OF SITE MATERIAL.

16. ASPHALT CONCRETE PAVEMENT SHALL CONFORM TO SECTION 5-04 OF THE 2023 STANDARD SPECIFICATIONS. THE FINAL GRADING OF CRUSHED SURFACING TOP COURSE WILL BE INSPECTED AND APPROVED BY THE ENGINEER PRIOR TO ASPHALT PAVING. ALL ABUTTING EDGES OF EXISTING ASPHALT SHALL BE SAW CUT FULL DEPTH TO PROVIDE A NEAT STABLE EDGE FOR THE NEW ASPHALT. ALL SAW CUT FACES SHALL BE TACK COATED AS WELL AS ALL STRUCTURES THAT ABUT ASPHALT. THE SURFACE JOINT BETWEEN EXISTING AND NEW ASPHALT MUST BE SEALED WITH HEAT-APPLIED CSS- 1 AND SAND COAT. ASPHALT SURFACE THAT HAS LOOSE MATERIAL OR POROUS CONDITIONS AS DETERMINED BY THE ENGINEER SHALL BE SEALED ACCORDING TO SECTION 5-04.3(5)C CRACK SEALING, AT NO ADDITIONAL COST TO THE OWNER, WITHIN 24 HOURS PRIOR TO PAVING. SOIL RESIDUAL HERBICIDE SHALL BE APPLIED TO ALL CRUSHED TOP COURSE SURFACES WITHIN THE PARKING LOTS AND ROADS.

17. HOT MIX ASPHALT SHALL BE PLACED AT THE LOCATIONS AND DEPTHS INDICATED ON THE PLANS, HOT MIX ASPHALT SHALL BE MECHANICALLY COMPACTED TO A MINIMUM OF 101% OF THE RICE DENSITY. COMPACTION SHALL OCCUR BETWEEN THE TEMPERATURES OF 180 DEGREES FAHRENHEIT AND 300 DEGREES FAHRENHEIT. DURING COLD WEATHER CONDITIONS, AS DETERMINED BY THE ENGINEER, ALL TRUCKLOADS OF ASPHALT SHALL BE COVERED SO AS TO RETAIN HEAT. THE OWNER SHALL RETAIN LICENSED MATERIALS-TESTING PERSONNEL TO PROVIDE COMPACTION TESTS AT 50- FOOT GRID/INTERVAL THROUGHOUT THE PARKING LOT AND ROAD SECTION. IF COMPACTION TEST RESULTS OF HOT MIX ASPHALT INDICATE LESS THAN 91% THE OWNER MAY, AT HIS EXPENSE, HAVE CORE SAMPLES TAKEN AND ANALYZED TO SUBSTANTIATE DENSITY. HOT MIX ASPHALT THAT DOES NOT MEET THE REQUIRED COMPACTION, SHALL EITHER BE REMOVED BY THE CONTRACTOR AT THEIR EXPENSE OR SHALL HAVE EQUAL THE INTENDED DESIGN. ADDITIONAL TESTING AND ASPHALT TO COMPENSATE FOR UNACCEPTABLE COMPACTION TEST RESULTS SHALL BE THE EXPENSE OF THE CONTRACTOR. NO ASPHALT PAVING OR ROLLING COMPACTION OF ASPHALT IS ALLOWED AFTER DARK. ALL ROLLING SHALL BE COMPLETED BY SUNSET TIME.

18. CONCRETE SIDEWALKS SHALL BE INSTALLED AS INDICATED ON THE CIVIL PLANS. SIDEWALKS SHALL BE SIX INCHES THICK SUPPORTED BY A MINIMUM OF SIX INCHES OF GRAVEL BORROW, COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY AS DETERMINED BY COMPACTION TESTING, UNLESS OTHERWISE SPECIFIED. SIDEWALKS ADJACENT TO PROPOSED ASPHALT SHALL HAVE THICKENED EDGE. SIDEWALKS SHALL HAVE FULL DEPTH EXPANSION JOINTS INSTALLED AT 25-FOOT INTERVALS WITH ONE-INCH SCORED STRESS JOINTS INSTALL AT FIVE FEET ON CENTER. EXTRUDED CURB SHALL BE PLACED ON FINISHED CONCRETE SURFACE AND BONDED WITH CONCRETE EPOXY OR CONCRETE SLURRY. CURBS SHALL BE PLACED IN STRAIGHT LINES AND ACCORDING TO RADII SHOWN ON THE PLANS. PAVEMENT THAT EXTENDS MORE THAN THREE INCHES BEYOND THE BACK OF EXTRUDED CONCRETE CURB SHALL BE SAW CUT FULL DEPTH AND REMOVED. ALL CONCRETE PAVEMENT, UNLESS OTHERWISE SPECIFIED, IS TO HAVE 1.5-INCH CRACK JOINTS SPACED AT 12 FEET MAXIMUM INTERVALS IN BOTH DIRECTIONS AND ARE TO BE SEALED TO RESTRICT WATER INFILTRATION INTO THE JOINTS.

STORM SEWER

1. THE FOLLOWING MATERIALS ARE ACCEPTABLE FOR THE STORM SEWERS IDENTIFIED ON THE PLANS:

A. PVC PIPE (POLYVINYL CHLORIDE) OVER 8" IN DIAMETER SHALL CONFORM TO SECTION 9-05.12(2) MEETING THE REQUIREMENTS OF ASTM D3034,SDR35. PVC PIPE 8" IN DIAMETER AND UNDER SHALL CONFORM TO SECTION 9-05.1(5) OF THE STANDARD SPECIFICATIONS MEETING THE REQUIREMENTS OF AASHTO M 294 TYPE S.

B. CORRUGATED POLYETHYLENE PIPE (CPP) SHALL HAVE A SMOOTH BARREL INTERIOR, CORRUGATED EXTERIOR, CONFORMING TO SECTION 9-05.1(7) MEETING THE REQUIREMENTS OF AASHTO M294.

C. PROFILE WALL PVC STORM PIPE 15" AND UNDER SHALL CONFORM TO SECTION 9-05.12(2) OF THE STANDARD SPECIFICATIONS, MEETING THE REQUIREMENTS OF AASHTO M304 SDR35. ALL FITTINGS SHALL CONFORM TO ASTM F 794. ALL PIPES SHALL HAVE GASKETED JOINTS.

2. STORM CATCH BASINS AS INDICATED ON THE PLANS SHALL CONFORM TO TOWN OF LA CONNER ENGINEERING DEPARTMENT AND:

BURLINGTON CATCH BASIN PER CUZ CONCRETE CB DETAIL
TYPE 1 CATCH BASIN PER WSDOT STD PLAN B-5.20-01
TYPE 1L CATCH BASIN PER WSDOT STD. PLAN B-5.40-01
TYPE 2 CATCH BASIN PER WSDOT STD. PLAN B-10.20-01
CATCH BASIN STD. GRATE PER WSDOT STD. PLAN B-30.50-01
CATCH BASIN THRU CURB GRATE PER WSDOT STD. PLAN B-25.20-01
ALL CATCH BASINS SHALL HAVE A MINIMUM 2-FOOT SUMP UNLESS OTHERWISE INDICATED.

IF SUBGRADE CONDITIONS ARE SOFT BELOW PROPOSED STRUCTURES, THE FOUNDATION SHALL BE OVER-EXCAVATED TWO-FEET BELOW THE STRUCTURE, AND THREE FEET FROM THE SIDES OF THE STRUCTURE, AND BACK-FILLED WITH MECHANICALLY COMPACTED 2-4" QUARRY SPALLS. ALL STRUCTURE JOINTS MUST BE GASKETED.

3. ALL PVC PIPE CONNECTIONS SHALL BE MADE TO STRUCTURES USING PVC SAND COLLARS.

4. UPON INSTALLATION OF ALL PIPES TO STORM STRUCTURES, THE KNOCKOUT AREA SHALL BE NEATLY MUDDED INSIDE AND OUT OF THE CATCH BASIN USING A NON-SHRINK CONCRETE GROUT.

5. UPON COMPLETION OF ALL CRUSHES TOP COARSE GRADING AND PREPARATION FOR ASPHALT PAVING, ALL CATCH BASIN STRUCTURES SHALL BE CORRECTLY ADJUSTED SO AS TO BE FLUSH WITH THE PROPOSED FINISH GRADE.

6. UPON PROJECT COMPLETION, THE CONTRACTOR SHALL FLUSH ALL STORM PIPES TO REMOVE ANY DEBRIS. DEBRIS SHALL NOT BE DISPOSED OF INTO THE DOWNSTREAM DRAINAGE SYSTEM, BUT DISPOSED OF IN AN APPROPRIATE MANNER.

7. BLOCK LETTERING SHALL BE EMBOSSED ON THE TOP SURFACES OF GRATES AND COVERS AS FOLLOWS:

A. "DRAIN" - 3-INCH LETTERS ON ALL SOLID COVERS.
B. "OUTFALL TO STREAM DUMP NO POLLUTANTS" - ½-INCH LETTERS ON ALL GRATES

8. ALL SOLID COVERS AND GRATES SHALL BE SECURED WITH 5/8-INCH STAINLESS STEEL SOCKET HEAD CAP SCREWS.

A LIGHT COATING OF ANTI-SEIZE THREAD COMPOUND SHALL BE APPLIED TO THE SCREWS AT THE TIME OF INSTALLATION. THE ANTI-SEIZE COMPOUND USED SHALL BE LOCTITE 767 OR APPROVED EQUAL. ANTI-SEIZE COMPOUND SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.

OTHER LOCKING DEVICES ARE ACCEPTABLE SUBJECT TO APPROVAL BY TOWN OF LA CONNER ENGINEERING DIRECTOR.

SANITARY SEWER

2.THE LOCATION OF THE SANITARY SEWER SERVICE IS INDICATED ON THE PLANS. SEWER SERVICE SHALL BE INSTALLED AT THE SLOPE AND LOCATION AS INDICATED ON THE CIVIL PLANS.

3.SEWER PIPE SHALL BE PVC, CONFORMING TO ASTM D 3034, SDR 35 AND SECTION 9-05.12 OF THE 2023 STANDARD SPECIFICATIONS. JOINTS SHALL BE BELL AND SPIGOT WITH A RUBBER GASKET. MATERIALS AND CONSTRUCTION SHALL CONFORM TO SECTION 7-08 AND SECTION 7-17 WSDOT/APWA, AND THE TOWN OF LA CONNER PUBLIC WORKS DEPARTMENT.

4.CLEANOUTS SHALL BE INSTALLED AT LOCATIONS AND ELEVATIONS AS SHOWN ON THE CONSTRUCTION PLANS. CLEANOUTS SHALL CONFORM TO THE CONSTRUCTION DETAIL, AND THE TOWN OF LA CONNER REQUIREMENTS. CLEANOUTS SHALL BE INSTALLED TO MATCH FINISH GRADE WITHIN CONCRETE AND ASPHALT AREAS.

5.THE CONTRACTOR IS TO INSTALL ALL COMPONENTS OF THE SANITARY SEWER SYSTEM A TO PREVENT ANY INTRUSION ON EXISTING GROUNDWATER.

6.AIR PRESSURE TESTING, IF REQUIRED BY THE TOWN OF LA CONNER PUBLIC WORKS, SHALL BE PERFORMED BY THE CONTRACTOR ON ALL PROPOSED SEWER LINES. THE PRESSURE TEST SHALL BE PERFORMED AT FIVE PSI FOR TEN MINUTES WITH NO SIGNIFICANT GAUGE DROP. ADEQUATE TIME (TWO MINUTES MINIMUM) SHALL ELAPSE BEFORE THE PRESSURE TEST IS PERFORMED TO ALLOW THE PRESSURE TO STABILIZE. THE CONTRACTOR IS RESPONSIBLE FOR RETAINING ALL STRUCTURES, CLEANOUTS AND PIPE ENDS DURING THIS TEST.

UTILITY TRENCHES

1.ALL TRENCHES SHALL BE EXCAVATED TO PROVIDE A MINIMUM WIDTH OF EIGHT INCHES ON EITHER SIDE OF THE PROPOSED UTILITY AS INDICATED ON THE UTILITY TRENCH DETAIL WITHIN THE CIVIL DRAWINGS. ALL STORM AND SEWER PIPES SHALL BE BEDDED WITH PEA GRAVEL OR BUCKSHOT WITH 100% OF THE MATERIAL PASSING THE 1/4-INCH SCREEN. BEDDING MATERIAL SHALL ALSO BE USED TO COVER THE PIPE TO A MINIMUM OF FOUR INCHES ABOVE THE TOP OF THE PIPE. THE CONTRACTOR SHALL CAREFULLY TAMP AND HAND COMPACT BEDDING AND COVER MATERIAL TO ASSURE ADEQUATE SUPPORT UNDER THE BARREL OF THE PIPE.

2.TRENCH BACKFILL MATERIAL SHALL CONSIST OF COMPACTED GRAVEL BORROW PLACED IN LOOSE LIFTS NOT EXCEEDING EIGHT INCHES AND COMPACTED TO A MINIMUM OF 105% MAXIMUM DENSITY AS DETERMINED AS ASTM D 1557 TESTING PROCEDURE. THE INITIAL LIFT OF GRAVEL TRENCH BACKFILL OVER THE PIPE SHALL NOT EXCEED 18 INCHES IN ORDER TO PROTECT THE PIPE. STRUCTURAL FILL SHALL BE USED AS TRENCH BACKFILL IN ALL TRENCHES UNDER PROPOSED ASPHALT, CONCRETE, CONSTRUCTION TRAFFIC AREAS, AND WITHIN FIVE FEET BEYOND IMPERVIOUS SURFACES. NATIVE MATERIAL SHALL NOT BE USED AS TRENCH BACKFILL.

3.NATIVE SOIL MATERIALS MAY BE USED AS TRENCH BACKFILL ONLY AT LOCATIONS BEYOND THOSE REQUIRING GRAVEL STRUCTURAL FILL. THE USE OF NATIVE SOIL MATERIALS AS TRENCH BACKFILL, WITHIN THOSE AREAS REQUIRING GRAVEL, MUST BE APPROVED BY THE GEOTECHNICAL ENGINEER, TOWN OF LA CONNER AND OWNER PRIOR TO THE WORK BEING PERFORMED. THE USE OF NATIVE SOILS WITHIN TRENCHES REQUIRING GASKET WILL ONLY BE ACCEPTED WITH THE APPROVAL OF THE GEOTECHNICAL ENGINEER, AND THE NATIVE SOILS MUST BE COMPACTED TO A MINIMUM OF 105 PERCENT MAXIMUM DRY DENSITY. THE USE OF NATIVE SOILS AS TRENCH BACKFILL SHALL NOT PRECLUDE THE MINIMUM ASPHALT SECTION REQUIREMENTS AS IDENTIFIED ON THE CIVIL PLANS.

4.AT LOCATIONS WHERE NATIVE SOILS ARE USED, RESULTING IN A PUMPING, UNSTABLE TRENCH CONDITIONS, OR THE SOILS USED CONTAIN UNSUITABLE PRODUCTS, THE CONTRACTOR SHALL REMOVE ALL UNSUITABLE MATERIALS AND REPLACE WITH APPROPRIATE NATIVE MATERIAL OR IMPORTED GRAVEL STRUCTURAL FILL MATERIAL, AT THE CONTRACTOR'S EXPENSE.

5.THE BOTTOM OF UTILITY TRENCHES SHALL HAVE A STABLE, NON-YIELDING, SOIL CONDITION SUITABLE FOR SUPPORTING THE DESIGN LOADS. WHERE THE BOTTOM OF TRENCH CONDITIONS EXHIBIT PUMPY, YIELDING CONDITIONS, THE BOTTOM OF TRENCH SHALL BE OVER EXCAVATED TO EXPOSE FIRM, STABLE MATERIAL, AND BACKFILL WITH TWO INCH TO FOUR INCH SHOT ROCK MATERIAL. WHERE OVER EXCAVATING EXPOSES SIMILAR UNSTABLE CONDITIONS, TRENCH OVER EXCAVATION SHALL BE DONE TO TWO FEET BELOW THE PIPE BEDDING MATERIAL AND BACKFILLED WITH TWO INCH TO FOUR INCH SHOT ROCK MATERIAL.

6.THE CONTRACTOR SHALL PROVIDE AND COORDINATE WITH PUGET SOUND ENERGY, VERIZON NORTHWEST, COMCAST CABLE SERVICES AND CASCADE NATURAL GAS FOR THE UNDERGROUND INSTALLATION OF POWER, TELEPHONE, UTILITIES AND VAULT TRENCHING AND BACKFILLING AS REQUIRED AND DELINEATED ON SAID UTILITY PLANS TO PROVIDE SERVICE TO LOTS SHOWN HEREIN, UNLESS SPECIFICALLY IDENTIFIED OTHERWISE ON THE PLANS. ALL NONMETALLIC UTILITY CONDUITS SHALL HAVE DETECTABLE MARKING TAPE CONFORMING TO WSDOT SECTION 9-15.18 INSTALLED DURING CONDUIT PLACEMENT.

7.PEA GRAVEL SHALL NOT BE USED WITHIN ANY PORTION OF THE WATER SYSTEM.

8.THE OWNER SHALL RETAIN LICENSED AND QUALIFIED PERSONNEL TO PROVIDE COMPACTION TESTING FOR THE FOLLOWING:

A. TRENCHES WITH THREE FEET OR LESS OF GRAVEL TRENCH BACKFILL: TOP CENTER OF UTILITY TRENCH AT 50-FEET INTERVALS.

B. TRENCHES WITH MORE THAN THREE FEET OF GRAVEL TRENCH BACKFILL: TOP CENTER OF UTILITY TRENCH AND MID-DEPTH OF TRENCH, BOTH AT 50-FEET INTERVALS. ALL TEST RESULTS SHALL MEET OR EXCEED THE SPECIFICATIONS. ALL AREAS THAT DO NO MEET THE REQUIRED SPECIFICATIONS SHALL BE RECOMPACTED AND RETESTED AT NO ADDITIONAL COST TO THE OWNER.

C. ANY AREAS THAT YIELD, DEFLECT OR PUMP UNDERNEATH NORMAL CONSTRUCTION TRAFFIC AS DIRECTED BY THE ENGINEER SHALL BE RECOMPACTED AND RETESTED AT NO ADDITIONAL COST TO THE OWNER.

9. AS COMPACTION TESTS ARE PERFORMED, THE OWNER IS RESPONSIBLE FOR PROVIDING ALL COMPACTION TESTING RECORDS TO THE CONTRACTOR, ENGINEER AND TOWN OF LA CONNER ENGINEERING DEPARTMENT.

10. ALL TRENCH EXCAVATION OVER A DEPTH OF 4 FEET SHALL BE SHORED AND CRIBBED IN ACCORDANCE WITH THE REQUIREMENTS OF THE WASHINGTON INDUSTRIAL SAFETY AND HEALTH ACT, CHAPTER 49.17 RCW, AND IN ACCORDANCE WITH SECTION 2.09 OF THE STANDARD SPECIFICATIONS.

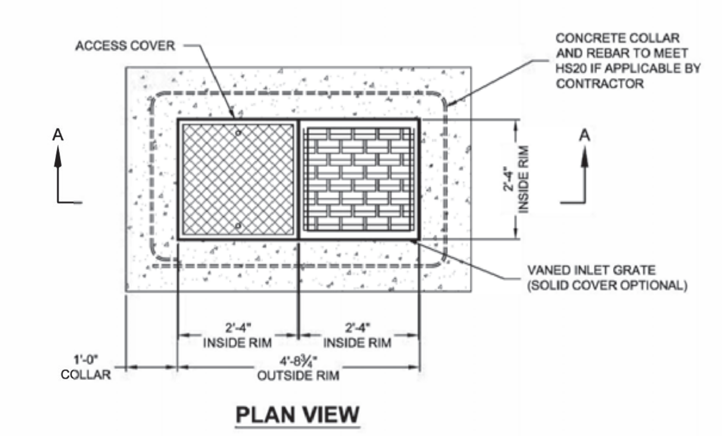
11. IF GROUND WATER IS ENCOUNTERED, TRENCH DRAINS SHALL BE INSTALLED IN TRENCHES WITH GROUND WATER CONDITIONS. SAID DRAINS SHALL GRAVITY DRAIN TO DAYLIGHT OR THE NEAREST CATCH BASIN. PERFORATED PIPE OPENINGS SHALL BE SIZED TO NOT ALLOW BEDDING MATERIAL INTO SAID DRAIN PIPE.

CONFLICT NOTE

ALL CONSTRUCTION WITH IN TOWN OF LA CONNER RIGHT OF WAY SHALL BE IN COMPLIANCE WITH THE TOWN OF LA CONNER CONSTRUCTION STANDARDS. IN THE EVENT THAT THERE IS A CONFLICT BETWEEN THE PROJECT PLANS AND SPECIFICATIONS, THE TOWN OF LA CONNER CONSTRUCTION STANDARDS/NOTES SHALL BE USED.

TOWN OF LA CONNER NOTE

PLEASE NOTE THAT ALL CONSTRUCTION ACTIVITIES, MATERIALS, PRACTICES AND OTHER REQUIRED ASPECTS OF THE PROJECT MUST BE IN COMPLIANCE WITH THE TOWN OF LA CONNER SPECIFICATIONS, PLANS AND STANDARD DETAILS, WHICH ARE FOUND IN SECTION E OF THE TOWN OF LA CONNER INFRASTRUCTURE IMPROVEMENTS PROJECT MANUAL. THE CONTRACTOR IS REQUIRED TO HAVE A COPY OF THE AFOREMENTIONED MANUAL FOR REFERENCE ON-SITE WITH CIVIL PLAN SETS.



PLAN VIEW

STORMFILTER CATCHBASIN DESIGN NOTES

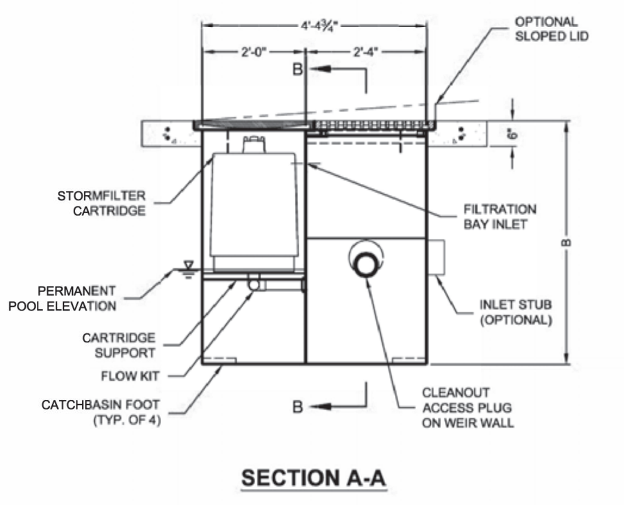
STORMFILTER TREATMENT CAPACITY IS A FUNCTION OF THE CARTRIDGE SELECTION AND THE NUMBER OF CARTRIDGES. 1 CARTRIDGE CATCHBASIN HAS A MAXIMUM OF ONE CARTRIDGE. SYSTEM IS SHOWN WITH A 27" CARTRIDGE, AND IS ALSO AVAILABLE WITH AN 18" CARTRIDGE. STORMFILTER CATCHBASIN CONFIGURATIONS ARE AVAILABLE WITH A DRY INLET BAY FOR VECTOR CONTROL.
PEAK HYDRAULIC CAPACITY PER TABLE BELOW. IF THE SITE CONDITIONS EXCEED PEAK HYDRAULIC CAPACITY, AN UPSTREAM BYPASS STRUCTURE IS REQUIRED.

CARTRIDGE SELECTION

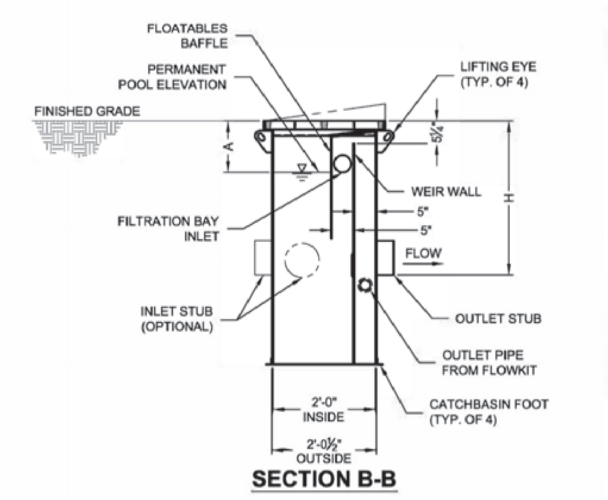
CARTRIDGE HEIGHT	27"		18"		18" DEEP	
MINIMUM HYDRAULIC DROP (H)	3.05'		2.3'		3.3'	
SPECIFIC FLOW RATE (gpm/sf)	2 gpm/ft²	1 gpm/ft²	2 gpm/ft²	1 gpm/ft²	2 gpm/ft²	1 gpm/ft²
CARTRIDGE FLOW RATE (gpm)	22.5	11.25	15	7.5	15	7.5
PEAK HYDRAULIC CAPACITY	1.0		1.0		1.8	
INLET PERMANENT POOL LEVEL (A)	1'-0"		1'-0"		2'-0"	
OVERALL STRUCTURE HEIGHT (B)	4'-9"		3'-9"		4'-9"	

GENERAL NOTES
1. CONTECH TO PROVIDE ALL MATERIALS UNLESS NOTED OTHERWISE.
2. FOR SITE SPECIFIC DRAWINGS WITH DETAILED STORMFILTER CATCHBASIN STRUCTURE DIMENSIONS AND WEIGHTS, PLEASE CONTACT YOUR CONTECH ENGINEERED SOLUTIONS LLC REPRESENTATIVE. www.ContechES.com
3. STORMFILTER CATCHBASIN WATER QUALITY STRUCTURE SHALL BE IN ACCORDANCE WITH ALL DESIGN DATA AND INFORMATION CONTAINED IN THIS DRAWING.
4. INLET SHOULD NOT BE LOWER THAN OUTLET. INLET (IF APPLICABLE) AND OUTLET PIPING TO BE SPECIFIED BY ENGINEER AND PROVIDED BY CONTRACTOR.
5. STORMFILTER CATCHBASIN EQUIPPED WITH 4 INCH (APPROXIMATE) LONG STUBS FOR INLET (IF APPLICABLE) AND OUTLET PIPING. STANDARD OUTLET STUB IS 8 INCHES IN DIAMETER. MAXIMUM OUTLET STUB IS 15 INCHES IN DIAMETER. CONNECTION TO COLLECTION PIPING CAN BE MADE USING FLEXIBLE COUPLING BY CONTRACTOR.
6. STEEL STRUCTURE TO BE MANUFACTURED OF 1/4 INCH STEEL PLATE. CASTINGS SHALL MEET AASHTO M306 LOAD RATING. TO MEET HS20 LOAD RATING ON STRUCTURE, A CONCRETE COLLAR IS REQUIRED. WHEN REQUIRED, CONCRETE COLLAR WITH QUANTITY (2) #4 REINFORCING BARS TO BE PROVIDED BY CONTRACTOR.
7. FILTER CARTRIDGES SHALL BE MEDIA-FILLED, PASSIVE, SIPHON ACTUATED, RADIAL FLOW, AND SELF CLEANING. RADIAL MEDIA DEPTH SHALL BE 7-INCHES. FILTER MEDIA CONTACT TIME SHALL BE AT LEAST 37 SECONDS.
8. SPECIFIC FLOW RATE IS EQUAL TO THE FILTER TREATMENT CAPACITY (gpm) DIVIDED BY THE FILTER CONTACT SURFACE AREA (sq ft).

INSTALLATION NOTES
A. ANY SUB-BASE, BACKFILL DEPTH, AND/OR ANTI-FLOTATION PROVISIONS ARE SITE-SPECIFIC DESIGN CONSIDERATIONS AND SHALL BE SPECIFIED BY ENGINEER OF RECORD.
B. CONTRACTOR TO PROVIDE EQUIPMENT WITH SUFFICIENT LIFTING AND REACH CAPACITY TO LIFT AND SET THE CATCHBASIN (LIFTING CLUTCHES PROVIDED).
C. CONTRACTOR TO TAKE APPROPRIATE MEASURES TO PROTECT CARTRIDGES FROM CONSTRUCTION-RELATED EROSION RUNOFF.



SECTION A-A



SECTION B-B

1-CARTRIDGE CATCHBASIN STORMFILTER DATA

STRUCTURE ID	CB #6
WATER QUALITY FLOW RATE (cfs)	0.004
PEAK FLOW RATE (<1 cfs)	0.046
RETURN PERIOD OF PEAK FLOW (yrs)	100
CARTRIDGE FLOW RATE (gpm)	7.5
MEDIA TYPE (CSF, PERLITE, ZPG, GAC, PHS)	ZPG
RIM ELEVATION	8.57

PIPE DATA:	I.E.	DIAMETER
INLET STUB	5.76	6"
OUTLET STUB	5.76	6"

CONFIGURATION: OUTLET / INLET / INLET — OUTLET / INLET / INLET

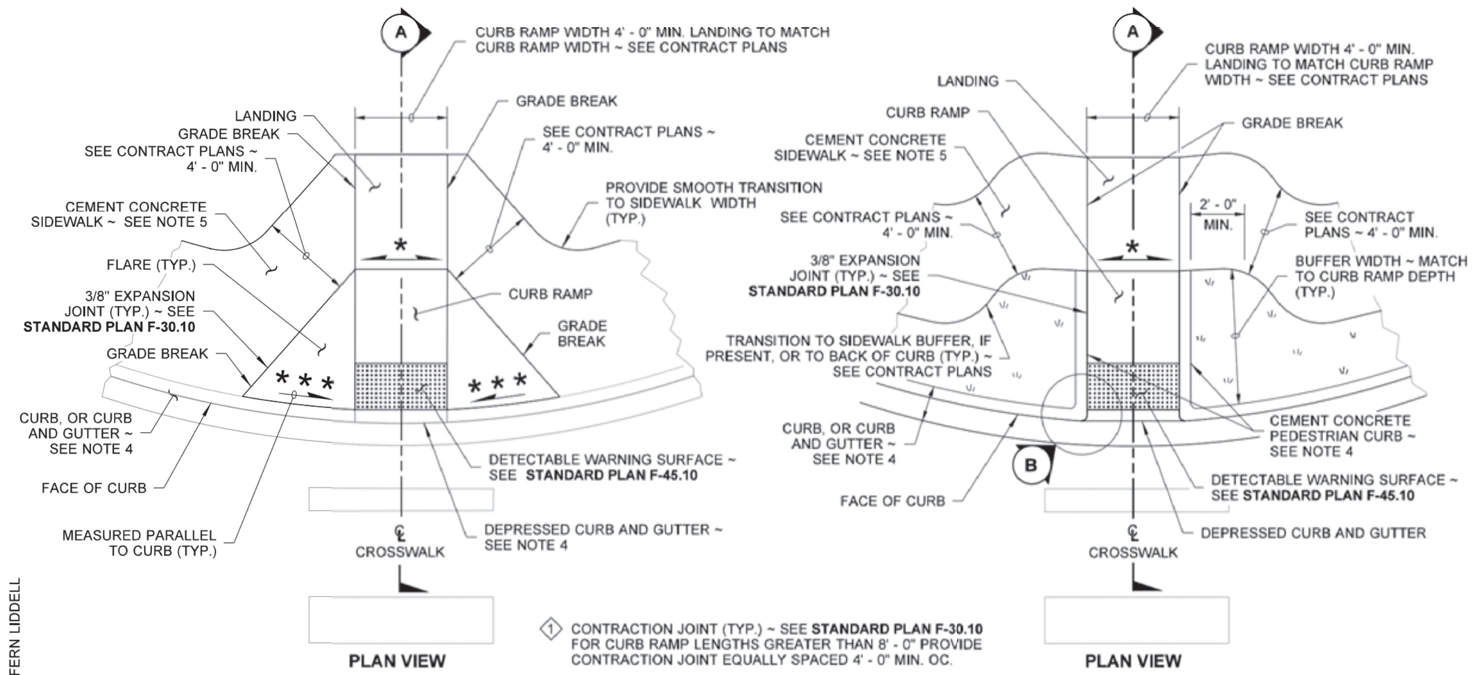
SLOPED LID	YES\NO
SOLID COVER	YES\NO
NOTES/SPECIAL REQUIREMENTS:	

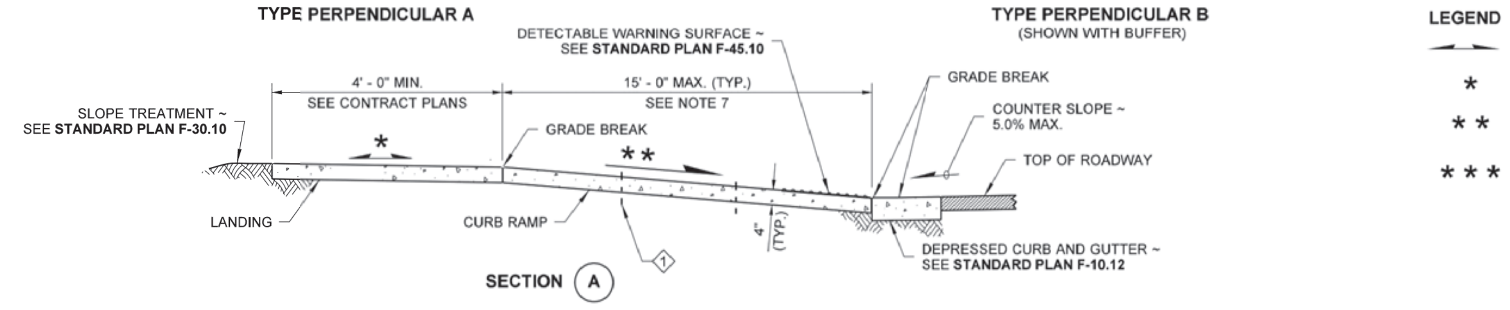
The Stormwater Management StormFilter®

CONTECH® ENGINEERED SOLUTIONS LLC
www.ContechES.com
9025 Centre Pointe Dr., Suite 400, West Chester, OH 45069
800-338-1122 513-645-7000 513-645-7993 FAX

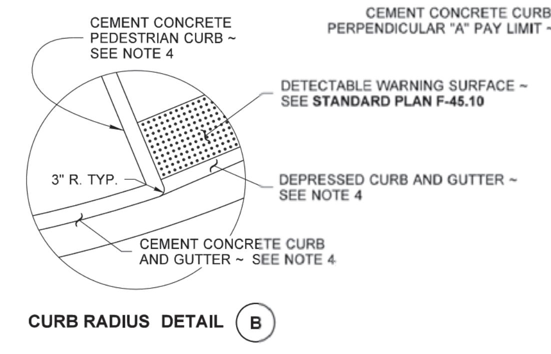
1 CARTRIDGE CATCHBASIN STORMFILTER STANDARD DETAIL

I:\STORMWATER\COMMOPS\10 STORMFILTER\40 STANDARD DRAWINGS\CATCH BASIN SF\SFCB-S\DWG\SFCB1-DTL.DWG 10/21/2014 8:39 AM

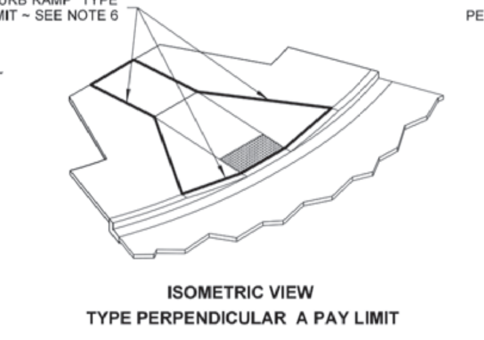




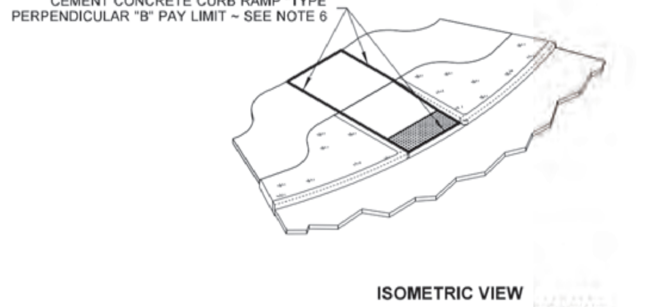
SECTION A



CURB RADIUS DETAIL B



ISOMETRIC VIEW
TYPE PERPENDICULAR A PAY LIMIT



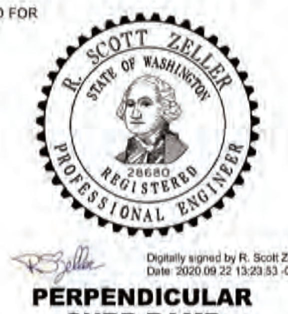
ISOMETRIC VIEW
TYPE PERPENDICULAR B PAY LIMIT

NOTES

1. At marked crosswalks, the connection between the curb ramp and the roadway must be contained within the width of the crosswalk markings.
2. Where "GRADE BREAK" is called out, the entire length of the grade break between the two adjacent surface planes shall be flush.
3. Do not place Gratings, Junction Boxes, Access Covers, or other appurtenances on any part of the Curb Ramp or Landing, or in front of the Curb Ramp where it connects to the roadway.
4. See Contract Plans for the curb design specified. See **Standard Plan F-10.12** for Curb, Curb and Gutter, Depressed Curb and Gutter, and Pedestrian Curb details.
5. See **Standard Plan F-30.10** for Cement Concrete Sidewalk Details. See Contract Plans for width and placement of sidewalk.
6. The Bid Item "Cement Concrete Curb Ramp Type __" does not include the adjacent Curb, Curb and Gutter, Depressed Curb and Gutter, Pedestrian Curb, or Sidewalks.
7. The Curb Ramp length is not required to exceed 15 feet (unless shown otherwise in the Contract Plans). When applying the 15-foot max. length, the running slope of the Curb Ramp is allowed to exceed 8.3%. Use a single constant slope from bottom of ramp to top of ramp to match into the landing over a horizontal distance of 15 feet. Do not include the abutting landing in the 15-foot max. measurement.
8. Curb Ramps and Landings shall receive a broom finish. See **Standard Specifications 8-14.**
9. Pedestrian Curb may be omitted if the ground surface at the back of the Curb Ramp and/or Landing will be at the same elevation as the Curb Ramp or Landing and there will not be material to retain.

LEGEND

◄—►	SLOPE IN EITHER DIRECTION
*	1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)
* *	7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.)
* * *	9.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (10% MAX.)



Digitally signed by R. Scott Zeller Date: 2020.09.22 13:23:53 -07'00'

PERPENDICULAR CURB RAMP

STANDARD PLAN F-40.15-04

SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION
Date: 2020.09.25 14:44:37 -07'00'
STATE DESIGN ENGINEER
Washington State Department of Transportation

DRAWN BY: FERN LIDDELL

Sound Development Group
ENGINEERING, SURVEYING & LAND DEVELOPMENT SERVICES
P.O. Box 1705 • 1111 Cleveland Avenue, Suite 202
Mount Vernon, WA 98273 Tel: 360-404-2010

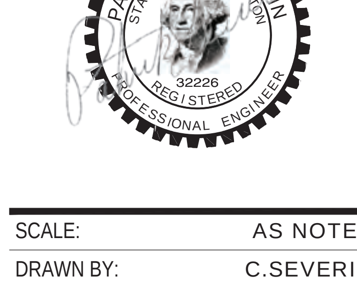
SHEET REVISIONS:

NO.	DATE	DESCRIPTION	APPROVED
1	9.22.23	ARCHITECT REVISION	P.L.S.

CALL 48 HOURS BEFORE YOU DIG 1.800.424.5555

PROJECT:
CENTER STREET MIXED-USE
FOR
KSA INVESTMENTS, LLC

SHEET DESCRIPTION:
STANDARD SPECIFICATIONS & STORMFILTER DETAIL



SCALE:	AS NOTED
DRAWN BY:	C.SEVERIN
DESIGNED BY:	P.SEVERIN
DATE:	08.30.2023
JOB NUMBER:	21098
DWG NAME:	21098PLN.DWG
SHEET NUMBER:	C4.0