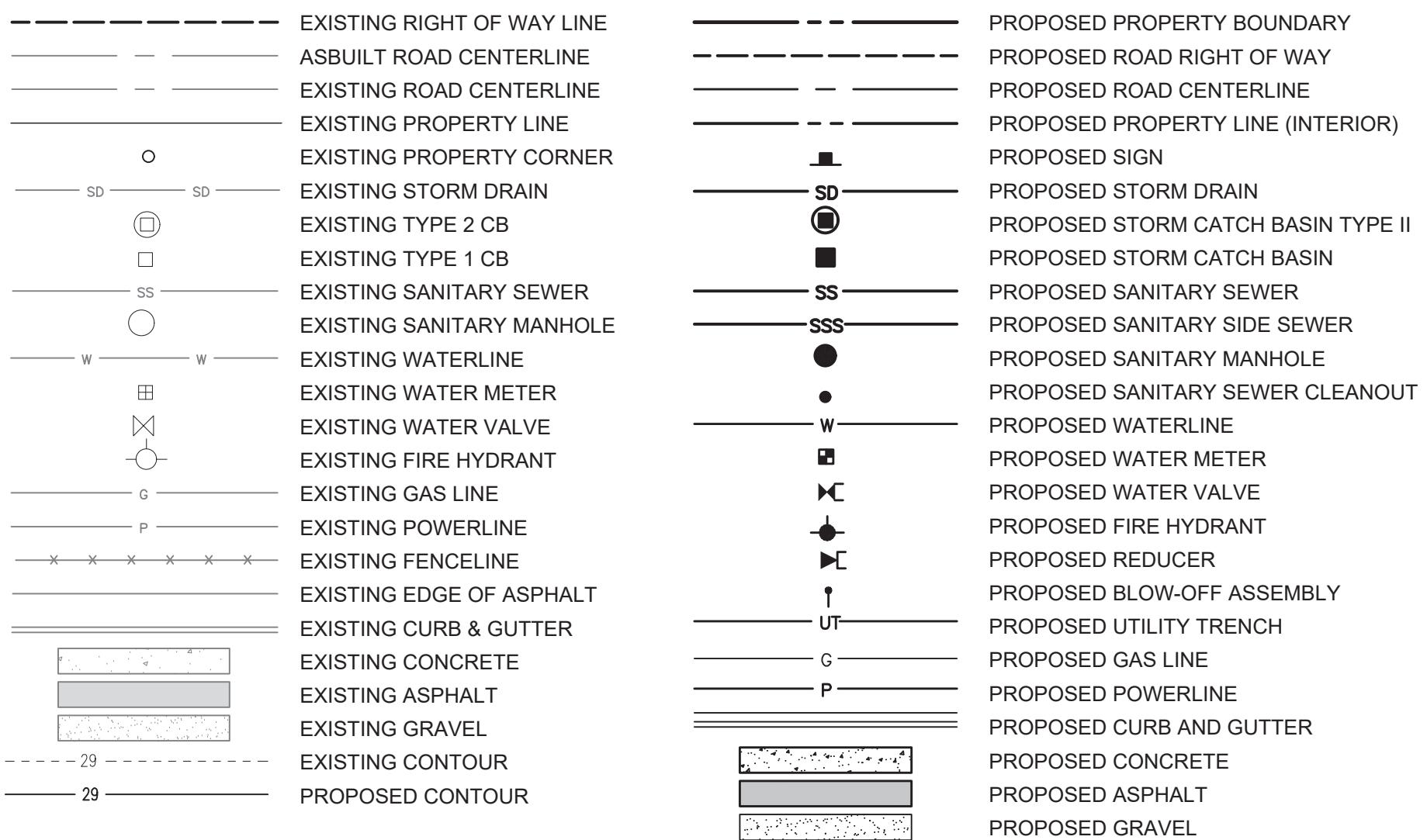


**SHEET INDEX**

SHEET NUMBER	SHEET TITLE
C1.0	COVER SHEET
C1.1	EXISTING CONDITIONS, DEMOLITION & TESC PLAN
C1.2	TESC PLAN NOTES & DETAILS
C2.0	GRADING AND DIMENSIONAL PLAN
C2.1	UTILITY PLAN
C2.2	CENTER STREET ROADWAY IMPROVEMENTS - PLAN & PROFILE
C2.3	FOURTH STREET ROADWAY IMPROVEMENTS - PLAN & PROFILE
C3.0	SITE & SEWER DETAILS
C3.1	SEWER & WATER DETAILS
C3.2	WATER, WSDOT STANDARD PLANS & CONSTRUCTION DETAILS
C3.3	WSDOT STANDARD PLANS
C3.4	WSDOT STANDARD PLANS & SEDIMENT TRAP DETAIL
C4.0	STANDARD SPECIFICATIONS & STORMFILTER DETAIL

**LEGEND****LEGAL DESCRIPTION**

THE EAST 3 FEET OF LOT 2 AND ALL OF LOTS 3, 6 AND 7, BLOCK 9, "CALHOUN ADDITION TO THE TOWN OF LA CONNER," AS PER PLAT RECORDED IN VOLUME 1 OF PLATS, PAGE 14, RECORDS OF SKAGIT COUNTY, WASHINGTON.

SITUATE IN THE COUNTY OF SKAGIT, STATE OF WASHINGTON.

NOTE: LEGAL DESCRIPTION FROM EXHIBIT "A" OF STATUTORY WARRANTY DEED RECORDED 08/26/2021 UNDER SKAGIT COUNTY AUDITOR'S FILE NUMBER 202108260088.

**SITE INFORMATION**

SITE PARCEL NUMBER: P74143

SITE ADDRESS: 306 CENTER STREET, LA CONNER, WA 98257

BUILDING UNITS: 6 AIRBNB LODGING UNITS, 14 APARTMENT DWELLING UNITS

**UNDERGROUND UTILITY NOTE**

UNDERGROUND UTILITY LOCATIONS SHOWN HEREON ARE BASED ON SURFACE INDICATORS, UTILITY MAPS PROVIDED BY THE CLIENT, SKAGIT COUNTY, AND APS UTILITY SERVICE WAS ALSO USED FOR THEIR LOCATION.

**SURVEY/DATUM INFORMATION****BASIS OF BEARING**

N 88° 25' 54" W BETWEEN THE FOUND NAILS IN THE INTERSECTIONS OF 3RD AND 4TH ST WITH CENTER ST.

**FIELD EQUIPMENT**

THIS SURVEY WAS ACCOMPLISHED BY FIELD TRAVERSE WITH A "TRIMBLE S5" AND A TRIMBLE R10-2 GPS RECEIVER, STANDARD ERROR DISTANCE +/- 2CM (+1 PPM), AND MEETS OR EXCEEDS STANDARDS AS SET FORTH IN W.A.C. CH. 332-130.

**HORIZONTAL DATUM**

WASHINGTON STATE PLANE NORTH ZONE 4601 (NAD 83/2012) USING W.S.R.N.

**VERTICAL DATUM**

THE VERTICAL DATUM FOR THIS PROJECT IS NAVD 88 BASED ON W.S.R.N. GPS TIE. THE ELEVATION OF THE SOUTHWEST TOP HOLD DOWN BOLT ON THE FIRE HYDRANT AT THE INTERSECTION OF 4TH ST. AND CENTER ST. HAS AN ELEVATION OF 9.47' NAVD AS SHOWN HEREON.

BASE FLOOD ELEVATION = 8.0' NGVD 29 + 3.8' = 11.8' NAVD 88

**UNDERGROUND UTILITY LOCATIONS**

UNDERGROUND UTILITY LOCATIONS SHOWN HEREON ARE BASED ON SURFACE INDICATORS.

**RESEARCH**

1) R.O.S. AFN 9407190146

2) R.O.S. AFN 200904210003

PLAT OF CALHOUN ADDITION TO THE TOWN OF LA CONNER VOL. 1, PAGE 14

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**CONTRACTOR:**

FABER CONSTRUCTION  
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BURLINGTON, WA 98233  
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**ENGINEER/SURVEYOR:**

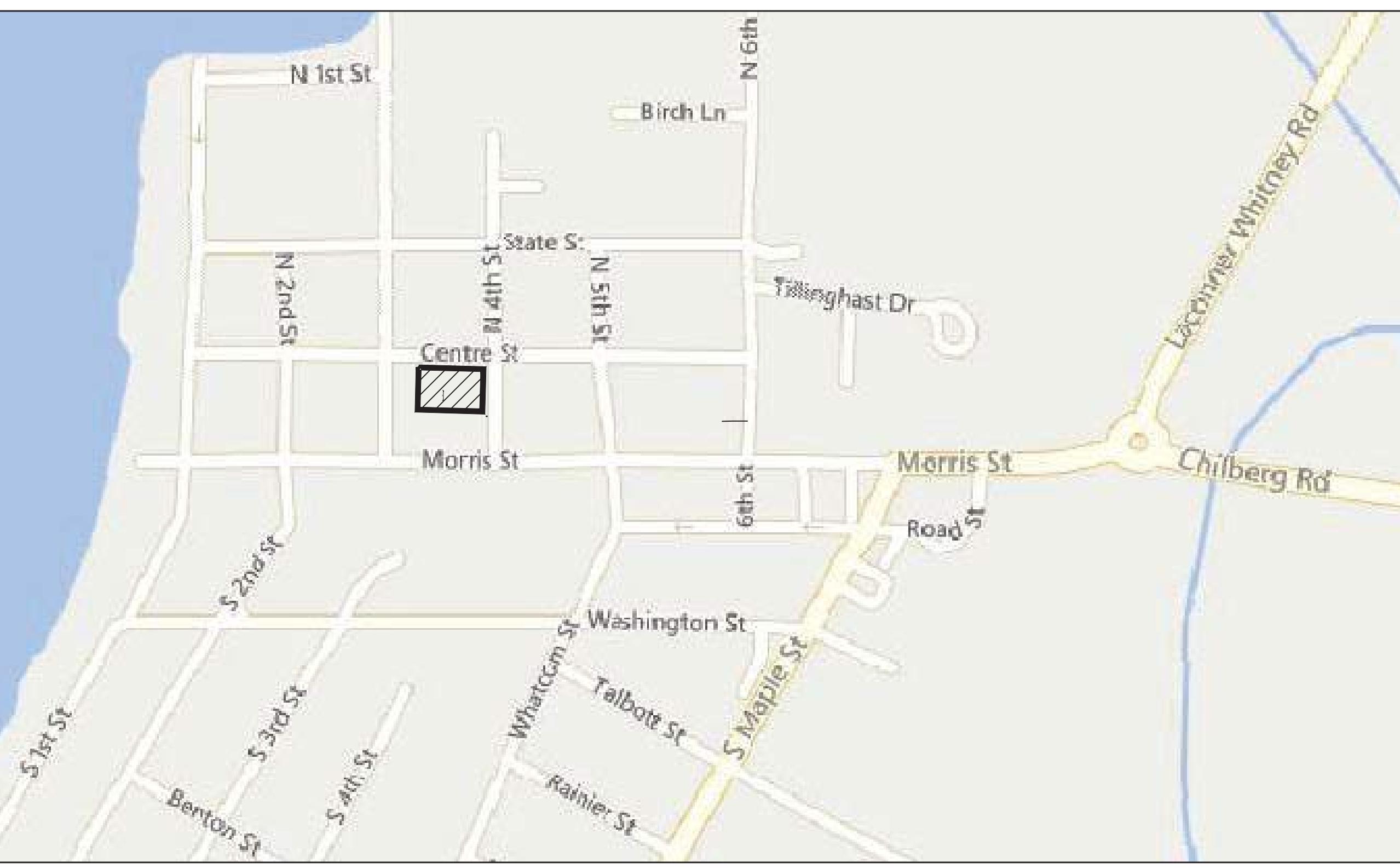
SOUND DEVELOPMENT GROUP, LLC  
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VICINITY MAP

NOT TO SCALE

**TOWN OF LA CONNER GENERAL NOTES**

- ALL CONSTRUCTION SHALL CONFORM TO THE REQUIREMENTS OF THE TOWN OF LA CONNER THAT ARE CONTAINED IN A BOUND VOLUME ENTITLED "INFRASTRUCTURE IMPROVEMENTS PROJECT MANUAL."
- THE LOCATION OF THE EXISTING UTILITIES SHOWN ON THE DRAWING IS APPROXIMATE. LOCATION AND PROTECTION OF UNDERGROUND UTILITIES SHALL BE IN ACCORDANCE WITH CHAPTER 19.122 RCW. CALL 800-424-5555 AT LEAST TWO BUSINESS DAYS BEFORE ANY EXCAVATION.
- REPLACE ANY DAMAGED OR DESTROYED MONUMENTS.
- THIS PLAN AND PROFILE INFORMATION HAS BEEN FURNISHED BY THE DEVELOPER OR HIS ENGINEER.
- MINIMUM SEPARATION BETWEEN SANITARY SEWERS AND POTABLE WATER LINES SHALL BE 10 FEET HORIZONTALLY (MEASURED SIDE TO SIDE) AND 18 INCHES VERTICALLY FROM BOTTOM OF WATERLINE TO CROWN OF SEWER. MINIMUM VERTICAL SEPARATION FOR PERPENDICULAR OR OBLIQUE CROSSINGS SHALL BE 3 FEET FROM OUTSIDE EDGES. SITUATIONS THAT REQUIRE LESS THAN MINIMUM SEPARATION SHALL BE CONSTRUCTED IN ACCORDANCE WITH DOE'S "CRITERIA FOR SEWAGE WORKS DESIGN", LATEST EDITION.
- THE DEVELOPER SHALL BE RESPONSIBLE FOR ACQUIRING ALL NECESSARY EASEMENTS AND AGREEMENTS PRIOR TO CONSTRUCTION.
- A PLUG SHALL BE PLACED IN THE OUTLET PIPE OF THE EXISTING MANHOLE TO WHICH NEW PIPE IS TO BE CONNECTED OR IN THE OUTLET OF THE FIRST NEW MANHOLE. THIS PLUG SHALL REMAIN IN PLACE AND MAY NOT BE REMOVED WITHOUT THE PERMISSION OF THE TOWN OF LA CONNER. REMOVAL WILL RESULT IN FORFEITURE OF THE SYSTEM ISOLATION DEPOSIT.

**CAUTION - EXTREME HAZARD** - OVERHEAD ELECTRICAL SERVICE LINES ARE GENERALLY NOT SHOWN ON THE DRAWINGS. ELECTRICAL LINES SHOWN ON THE DRAWINGS ARE LOCATED BY POINT-TO-POINT, POWER-POLE-TO-POWER-POLE CONNECTION. THE CONTRACTOR IS RESPONSIBLE FOR DETERMINING THE EXTENT OF ANY HAZARD CREATED BY OVERHEAD ELECTRICAL POWER IN ALL AREAS AND SHALL FOLLOW PROCEDURES DURING CONSTRUCTION AS REQUIRED BY LAW AND REGULATION. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL MEET WITH UTILITY OWNERS AND DETERMINE THE EXTENT OF HAZARD AND REMEDIAL MEASURES, AND SHALL TAKE WHATEVER PRECAUTIONS MAY BE REQUIRED.

**UNDERGROUND FIRE LINE REQUIREMENTS:**

- THE ENTIRE UNDERGROUND FIRE LINE WILL REQUIRE A FULL HYDRO-TEST AT 200 PSI FOR TWO (2) HOURS. CONTACT THE FIRE MARSHAL BEFORE PERFORMING THIS TEST.
- EACH FIRE HYDRANT AND FDC RISER ON THE FIRE SYSTEM WILL REQUIRE A ONE (1) MINUTE FLUSH FROM THE PUMPER PORT OR MANIFOLD WITH A DIFFUSER OR UNTIL THERE ARE NO PARTICULATES IN THE SYSTEM.
- THE PUMPER PORT ON THE HYDRANTS SHALL HAVE A FIVE (5) INCH STORTZ HARD FACED COUPLER INSTALLED PRIOR TO FINAL APPROVAL.
- A COPY OF NFPA U FORM FILLED OUT MUST BE PROVIDED TO THE FIRE MARSHAL PRIOR TO FINAL.
- ALL FITTINGS AND PIPING FROM THE VAULT TO THE BUILDING WILL NEED TO BE INSPECTED BY THE FIRE MARSHAL PRIOR TO COVER.
- THREE (3) SETS OF PLANS, MATERIAL CUT SHEETS AND FIRE FLOW CALCULATIONS WILL NEED TO BE APPROVED PRIOR TO ANY WORK BEING DONE.
- ALL WORK BEING DONE FROM THE DOUBLE CHECK VALVE ASSEMBLY TO THE RISER WILL NEED TO BE INSTALLED BY A U LICENSED CONTRACTOR.
- THE FIRE LINE MUST BE BACKFILLED WITH EITHER SAND OR PEA GRAVEL. NATIVE MATERIALS CANNOT BE USED AS BACKFILL MATERIALS.

**TOWN OF LA CONNER**  
APPROVED FOR CONSTRUCTION - 06/18/24

REVIEW- ALT  
BACK CHECK- ALT  
FINAL - BL  
TRAFFIC - SEPARATE SHEET

PROJECT:

**CENTER STREET**  
**MIXED-USE**  
FOR  
**KSA INVESTMENTS, LLC**

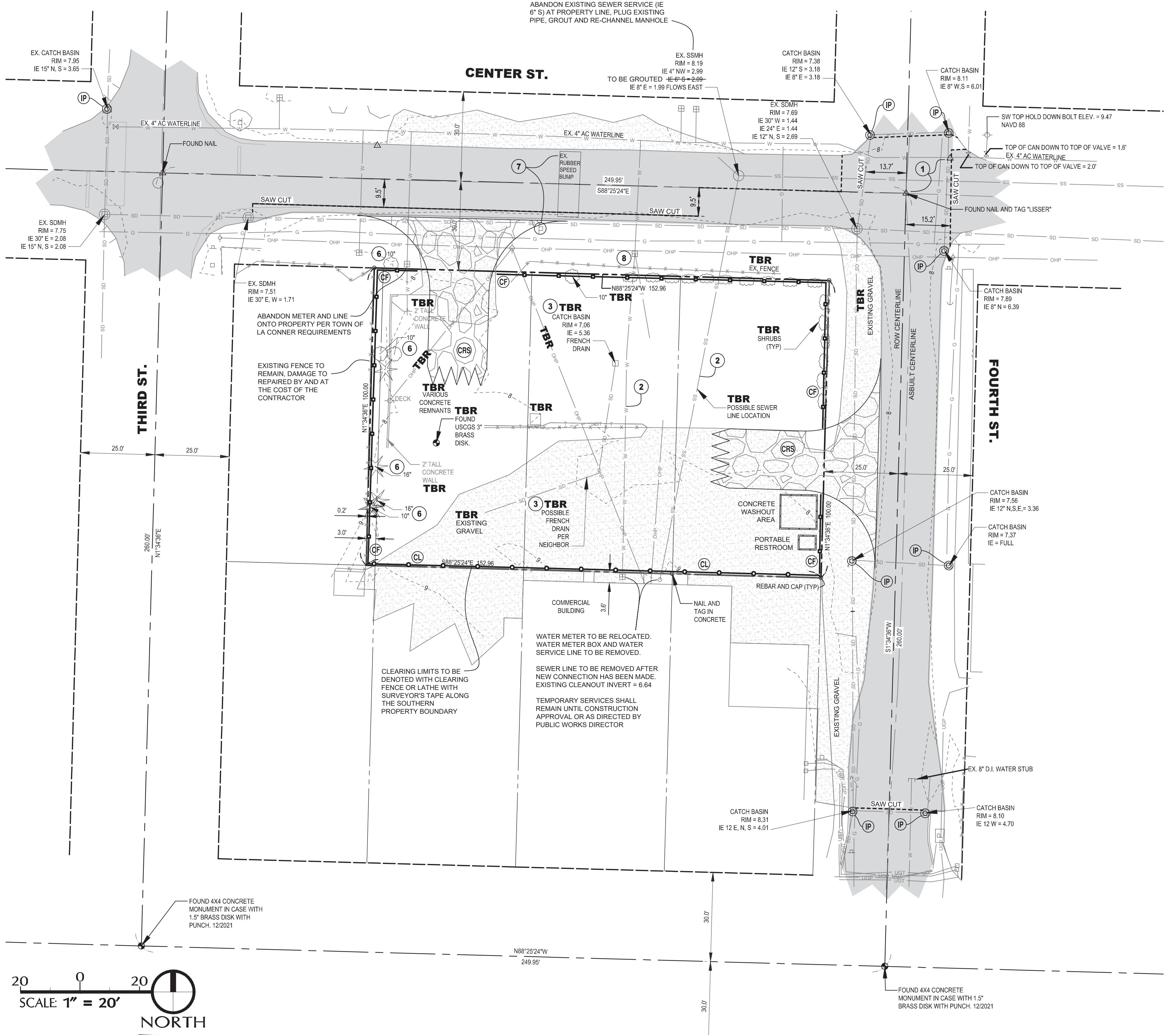
**COVER SHEET**

SHEET DESCRIPTION:



SCALE: **NO SCALE**  
DRAWN BY: **C.SEVERIN**  
DESIGNED BY: **P.SEVERIN**  
DATE: **02.22.2024**  
JOB NUMBER: **21098**  
DWG NAME: **21098PLN.DWG**  
SHEET NUMBER: **C1.0**

**IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA CONNER, WASHINGTON**



## **TESC PLAN LEGEND**

<b>CODE</b>	<b>SYMBOL</b>	<b>DESCRIPTION</b>
CRS		CONSTRUCTION ROAD STABILIZATION, PER DETAIL A/1.2, IF EXISTING GRAVEL IS NOT SUFFICIENT FOR SEDIMENT REMOVAL INSTALL RUMBLE STRIP PLATES AT ENTRANCE AND EXIT, AS REQUIRED BY THE FILL, GRADE AND/OR CLEARING PERMIT APPROVED 8/15/23 BY THE TOWN OF LA CONNER
CF		ORANGE CONSTRUCTION FENCE (BMP C103) OR EQUIVALENT HIGH VISIBILITY FENCE AT CESCL DISCRETION DURING DRY SEASON SILT FENCE DURING WET SEASON
CL		CLEARING LIMITS - CLEARING / ORANGE CONSTRUCTION FENCE (BMP C103) OR LATHE AND SURVEYOR'S TAPE
VEG		RETAIN EXISTING VEGETATION
IP		INLET PROTECTION PER DETAIL B/1.2. TO BE INSTALLED ON ALL PROPOSED AND EXISTING CBs WITH OPEN GRATES
SP		SILT PROTECTION - STRAW WADDLES
PS		PERMANENT SEEDING AND PLANING, BMP C120 AND BMP T5.13
DC		DUST CONTROL
<b>TBR</b>		TO BE REMOVED

**TESC NOTES:**

1. SILT FENCE SHALL BE INSTALLED IF WARRANTED BY SITE CONDITIONS AS DETERMINED BY THE PROJECT CESCL, ENGINEER OR CITY INSPECTOR. THE SILT FENCE SHALL BE PLACED AS REQUIRED TO PREVENT SILT LADEN RUNOFF FROM LEAVING THE SITE. SEE DETAIL C/2.1 FOR SILT FENCE INSTALLATION.
2. TRENCH SPOILS ARE TO BE PLACED ON THE UPHILL SIDE OF THE TRENCH WHERE FEASIBLE.
3. WHEN FEASIBLE, NO MORE THAN 500 LF OF TRENCH SHALL BE OPENED AT ONE TIME. TRENCH SPOILS ARE TO BE PLACED ON THE UPHILL SIDE OF THE TRENCH; ALL DEWATERING, ALTHOUGH NOT EXPECTED, WILL BE DISCHARGED TO THE SEDIMENT TRAP. SEDIMENT TRAP, IF NEEDED, IS TO BE CONSTRUCTED PER DETAILS ON SHEET C3.4 WITH A SEDIMENT TRAP SURFACE AREA OF 35 SF.
4. CONSTRUCTION ACCESS WILL BE PROVIDED THROUGH THE TWO EXISTING GRAVELED ACCESS POINTS, ONE AS AN ENTRANCE AND THE OTHER AS AN EXIT. SHOULD THE EXISTING GRAVEL AREAS NOT RETAIN SEDIMENTS FROM VEHICLE WHEELS, A CONSTRUCTION ACCESS IS TO BE INSTALLED PER DETAIL A/1.2. REFER TO THE TESC PLAN NOTES ON SHEET C1.2 FOR INFORMATION ON STREET SWEEPING AND WASHING.
5. ALL BMP'S ARE TO BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION.
6. NO CONCRETE TRUCKS WILL BE ALLOWED TO WASHOUT ON-SITE. THE CONCRETE WASHOUT AREA HAS BEEN DENOTED FOR CONCRETE POURING AND FINISHING TOOLS ONLY.
7. NO VEHICLE MAINTENANCE IS ALLOWED ON-SITE.
8. ALL BMP'S SHALL BE INSPECTED AND MAINTAINED ON A REGULAR BASIS, AS DETERMINED BY THE TIME OF YEAR AND RAINFALL EVENTS.
9. INLET PROTECTION IS TO BE INSTALLED ON ALL STORM STRUCTURES WITH OPEN GRATES WITHIN 300' OF THE SITE, WHICH MAY NOT BE SHOWN ON THE PLANS.

## **EXISTING CONDITIONS NOTES**

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- 1 CONTRACTOR TO MINIMIZE IMPACT TO ALL FOUND MONUMENTS AND TO SCHEDULE RESTORATION OF MONUMENT BY A LICENSED PROFESSIONAL LAND SURVEYOR AT THE COMPLETION OF CONSTRUCTION, AS NECESSARY AND APPROPRIATE. EXISTING USCGS 3" BRASS DISK IS TO BE REMOVED AND WILL NOT BE REPLACED, AS PRE-AUTHORIZED BY USCGS.
- 2 CONTRACTOR TO POTHOLE AND CONFIRM UTILITY LINES DO NOT SERVE OTHER PROPERTIES. IF LINES DO NOT SERVE OTHER PROPERTIES, CONTRACTOR TO CALL FOR DISCONNECTION, CAPPING AND ABANDONMENT/REMOVAL OF UTILITIES ENTERING THE SITE FROM THE NORTH AND SOUTH PROPERTY LINES. UNDERGROUND AND ABOVE-GROUND UTILITIES ARE TO BE DISCONNECTED AT THE DIRECTION OF THE ASSOCIATED UTILITY PURVEYOR. ALL NECESSARY WORK (TRENCHING, BACKFILL, COMPACTION, ETC) IS THE RESPONSIBILITY OF THE CONTRACTOR, UNLESS UTILITY PURVEYOR SPECIFIES OTHERWISE.
- 3 WATER AND SANITARY SEWER LINES ARE TO REMAIN LIVE UNTIL UTILITY IMPROVEMENTS ARE MADE ON FOURTH STREET, PER UTILITY PLAN, AND SERVICE IS RESTORED TO THE EXISTING LINES AND METER.
- 4 CONTRACTOR TO POTHOLE STORM LINE AND CONFIRM IF THE STORM LINE/TRENCH DRAIN IS LIMITED TO THE SUBJECT PROPERTY. SHOULD THE FEATURE BE LIMITED TO THIS PROPERTY, THE STORM LINE AND TRENCH DRAIN IS TO BE REMOVED. SHOULD THE FEATURE NOT BE LIMITED TO THE SUBJECT PROPERTY, THE ENGINEER IS TO BE CONTACTED IMMEDIATELY FOR RE-ROUTING PLANS.
- 5 CONTRACTOR TO LOCATE ALL NEXT INLINE VALVES PRIOR TO CONSTRUCTION AND/OR WORK ON WATER SYSTEM.
- 6 CONTRACTOR TO COORDINATE ALL UTILITY AND ROAD SHUT-DOWN PERIODS WITH THE TOWN OF LA CONNER, EMERGENCY DEPARTMENTS, UTILITY PURVEYORS AND AFFECTED PROPERTIES (COMMERCIAL AND RESIDENTIAL). CONTRACTOR TO PROVIDE TRAFFIC CONTROL PLAN, APPROVED BY THE TOWN OF LA CONNER, PRIOR TO BEGINNING CONSTRUCTION.
- 7 LANDSCAPING AND EXISTING SHRUBS ALONG THE WESTERN PROPERTY BOUNDARY ARE TO BE REMOVED. CONTRACTOR TO COORDINATE WITH NEIGHBORS SHOULD EXISTING FEATURES, THAT ARE TO REMAIN (I.E. FENCE LINES), ARE AFFECTED DURING THE REMOVAL PROCESS.
- 8 CONTRACTOR TO REMOVE ALL EXISTING SIGNS AFFECTED BY CONSTRUCTION, INCLUDING BUT NOT LIMITED TO THE "SHARE THE ROAD" AND BIKE-PEDESTRIAN DIAMOND-SHAPED SIGN ON CENTER STREET AND THE STOP SIGN IN THE SOUTHEAST CORNER OF THE CENTER AND FOURTH STREET INTERSECTION; TOWN OF LA CONNER TO REINSTALL AFFECTED SIGNS PER MUTCD. CONTRACTOR TO RESTORE EXISTING RUBBER SPEED BUMP AFFECTED BY CONSTRUCTION, OR REMOVE AND REPLACE AS DIRECTED BY THE TOWN OF LA CONNER
- 9 EXISTING WATER METER AND METER BOX TO BE REMOVED. NEW METER TO BE PROVIDED BY TOWN OF LA CONNER FOR USE AS A LANDSCAPE METER, SEE SHEETS C2.0-C2.3 FOR PLACEMENT AND ADDITIONAL INFORMATION. WATER LINE CROSSING CENTER STREET TO BE ABANDONED IN PLACE BY DIRECTION OF LA CONNER PUBLIC WORKS.
- 10 ALL ASBESTOS CONCRETE (AC) PIPING REMOVED IS TO BE TREATED AS HAZARDOUS WASTE AND IS REQUIRED TO BE DISPOSED OF PROPERLY. CONTRACTOR TO OBTAIN APPROVAL OF DISPOSAL PLAN PRIOR TO REMOVING ANY AC MATERIALS FROM THEIR LOCATION

**TOWN OF LA CONNER**  
APPROVED FOR CONSTRUCTION - 06/18/24

REVIEW- ALT  
BACK CHECK- ALT  
FINAL - BL  
TRAFFIC - SEPARATE SHEET



SCALE: **1" = 20'**  
DRAWN BY: **C.SEVERIN**  
DESIGNED BY: **P.SEVERIN**  
DATE: **02.22.2024**  
JOB NUMBER: **21098**  
DWG NAME: **21098PLN.DWG**  
SHEET NUMBER:

## C1.1

PROJECT.

# **XISTING CONDITIONS, DEMOLITION AND TESC PLAN**

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# IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA CONNER, WASHINGTON

## ESC-1 STANDARD ESC NOTES

APPROVAL OF THE EROSION AND SEDIMENT CONTROL (ESC) PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G., SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).

1. AS DIRECTED BY PUBLIC WORKS DIRECTOR PRIOR TO COMMENCING CONSTRUCTION, ALL CRITICAL AREAS, INCLUDING WETLAND BUFFERS, STREAM CORRIDOR, LANDFILL AREAS, AND STEEP SLOPES SHALL CONTINUOUSLY DEMARCATED IN THE FIELD USING FLAGGING TAPE OR FENCING.
2. EROSION CONTROL METHODS AND MATERIALS SHALL MEET REQUIREMENTS OF SECTION 8-01 OF THE APWA/WASHINGTON STATE DEPARTMENT OF TRANSPORTATION 2022 STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION, AND REQUIREMENTS SET FORTH IN VOLUME I OF THE "STORMWATER MANAGEMENT MANUAL FOR THE PUGET SOUND BASIN (THE TECHNICAL MANUAL)" BY THE WASHINGTON STATE DEPARTMENT OF ECOLOGY. CURRENT EDITION. THE CONTRACTOR SHALL FOLLOW RECOMMENDATIONS MADE BY SUPPLIERS AND MANUFACTURERS OF MATERIALS AND EQUIPMENT USED.
3. THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED IN CONJUNCTION WITH ALL CLEARING AND GRADING ACTIVITIES IN SUCH A MANNER AS TO ENSURE THAT SEDIMENT-LADEN WATER DOES NOT ENTER THE DRAINAGE SYSTEM OR VIOLATE APPLICABLE WATER STANDARDS, AND MUST BE INSTALLED AND IN OPERATION PRIOR TO ANY GRADING OR LAND CLEARING. WHEREVER POSSIBLE, MAINTAIN NATURAL VEGETATION FOR SILT CONTROL.
4. THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED (E.G., ADDITIONAL SUMPS, LOCATION OF DITCHES AND SILT FENCES, ETC.) AS NEEDED. THEREFORE, DURING THE COURSE OF CONSTRUCTION, IT SHALL BE THE OBLIGATION AND RESPONSIBILITY OF THE CONTRACTOR TO ADDRESS ANY NEW CONDITIONS THAT MAY BE CREATED BY HIS ACTIVITIES AND TO PROVIDE ADDITIONAL FACILITIES OVER AND ABOVE THE MINIMUM REQUIREMENTS AS MAY BE NEEDED.
5. ANY PERMANENT RETENTION/DETENTION FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE PERMANENT FACILITY IS TO FUNCTION ULTIMATELY AS AN INFILTRATION OR DISPERSION SYSTEM, THE FACILITY SHALL NOT BE USED AS A TEMPORARY SETTLING BASIN. NO TEMPORARY DETENTION TANK, DETENTION VAULT, OR SYSTEM, WHICH BACKS UNDER OR INTO A POND, SHALL BE USED AS A TEMPORARY SETTLING BASIN.
6. WHERE SEEDING FOR TEMPORARY EROSION CONTROL IS REQUIRED, FAST GERMINATING GRASSES SHALL BE APPLIED AT AN APPROPRIATE RATE (EXAMPLE: ANNUAL OR PERENNIAL) AND APPLIED AT APPROXIMATELY 60 POUNDS PER ACRE.
7. WHERE STRAW MULCH IS REQUIRED FOR TEMPORARY EROSION CONTROL, IT SHALL BE APPLIED AT A MINIMUM THICKNESS OF TWO (2) INCHES.
8. ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH THE TOWN OF LA CONNER STANDARDS AND SPECIFICATIONS.
9. THE ESC FACILITIES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DETAILS ON THE APPROVED PLANS. LOCATIONS MAY BE MOVED TO SUIT FIELD CONDITIONS, SUBJECT TO APPROVAL BY THE TOWN OF LA CONNER PUBLIC WORKS DIRECTOR AND TOWN OF LA CONNER INSPECTOR.
10. A COPY OF THE APPROVED EROSION CONTROL PLANS MUST BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
11. ALL LOTS ADJOINING OR HAVING ANY NATIVE GROWTH PROTECTION EASEMENTS (NGPE) SHALL HAVE A FOUR (4) FOOT HIGH TEMPORARY CONSTRUCTION FENCE (CYCLONE OR PLASTIC MESH) SEPARATING THE LOT (OR BUILDABLE PORTIONS OF THE LOT) FROM THE AREA RESTRICTED BY THE NGPE AND SHALL BE INSTALLED PRIOR TO ANY GRAVING OR CLEARING AND REMAIN IN PLACE UNTIL A DWELLING IS CONSTRUCTED AND OWNERSHIP TRANSFERRED TO THE FIRST OWNER/OCCUPANT.
12. A CLEARING CONTROL FENCE SHALL BE INSTALLED AT THE DRIP LINE OF TREES TO BE SAVED WHEREVER THE TREE CANOPIES EXTEND INTO THE AREA TO BE CLEARED. ON-SITE SUPERVISION SHALL BE PROVIDED WHENEVER EQUIPMENT MUST OPERATE UNDER TREE CANOPIES, AND FENCING SHALL BE REPLACED WHEN NECESSARY. THE CLEARING CONTROL FENCE SHALL CONSIST OF A SINGLE STRAND OF WIRE ATTACHED ABOUT THREE (3) FEET HIGH TO WOOD LATH AT A TWELVE (12) FOOT MAXIMUM SPACING AND FLAGGED WITH SURVEY RIBBON AT FOUR (4) FOOT INTERVALS.
13. OFF-SITE STREETS MUST BE KEPT CLEAN AT ALL TIMES. IF DIRT IS DEPOSITED ON THE PUBLIC STREET SYSTEM, THE STREET SHALL BE IMMEDIATELY CLEANED WITH A POWER SWEeper OR OTHER EQUIPMENT. ALL VEHICLES SHALL LEAVE THE SITE BY WAY OF THE CONSTRUCTION ENTRANCE AND SHALL BE CLEANED OF ALL DIRT THAT WOULD BE DEPOSITED ON THE PUBLIC STREETS.
14. ANY CATCH BASINS COLLECTING RUNOFF FROM THE SITE, WHETHER THEY ARE ON OR OFF THE SITE, SHALL HAVE THEIR GRATES COVERED WITH FILTER FABRIC DURING CONSTRUCTION.
15. THE WASHED GRAVEL BACKFILL ADJACENT TO THE FILTER FABRIC FENCE SHALL BE REPLACED AND THE FILTER FABRIC CLEANED. IF IT IS NONFUNCTIONAL, BY EXCESSIVE SILT ACCUMULATION AS DETERMINED BY THE TOWN OF LA CONNER. ALSO, ALL INTERCEPTOR SWALES SHALL BE CLEANED IF SILT ACCUMULATION EXCEEDS ONE-QUARTER DEPTH.
16. ROCK FOR EROSION PROTECTION OF ROADWAY DITCHES, WHERE REQUIRED, MUST BE OF SOUND QUARRY ROCK, PLACED TO DEPTH OF ONE (1) FOOT AND MUST MEET THE FOLLOWING SPECIFICATIONS: 4"-8" ROCK/40%-70% PASSING; 2"-4" ROCK/30%-40% PASSING; AND 1"-2" ROCK/10%-20% PASSING.
17. IF ANY PART(S) OF THE CLEARING LIMIT, BOUNDARY OR TEMPORARY EROSION/SEDIMENTATION CONTROL PLAN IS DAMAGED, IT SHALL BE REPAVED IMMEDIATELY.
18. ALL PROPERTIES ADJACENT TO THE PROJECT SITE SHALL BE PROTECTED FROM SEDIMENT DEPOSITION AND RUNOFF. DO NOT FLUSH CONCRETE BY-PRODUCTS OR TRUCKS NEAR OR INTO THE STORM DRAINAGE SYSTEM. IF EXPOSED AGGREGATE IS FLUSHED INTO THE STORM SYSTEM, IT COULD MEAN RELEASING THE ENTIRE DOWNSTREAM STORM SYSTEM, OR POSSIBLY RELATING THE STORM LINE.
19. THE IMPLEMENTATION OF THESE ESC PLANS AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT AND UPGRADING OF THESE FACILITIES IS THE RESPONSIBILITY OF THE APPLICANT/ESC SUPERVISOR UNTIL ALL CONSTRUCTION IS APPROVED.
20. THE ESC SUPERVISOR NAME IS: \_\_\_\_\_  
24 HR. CONTACT NUMBER IS: \_\_\_\_\_
21. THE APPLICANT'S NAME IS: \_\_\_\_\_  
24 HR. CONTACT NUMBER IS: \_\_\_\_\_
22. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED BY A CONTINUOUS LENGTH OF SURVEY TAPE (OR FENCING, IF REQUIRED) PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE CLEARING LIMITS SHALL BE PERMITTED. THE CLEARING LIMITS SHALL BE MAINTAINED BY THE APPLICANT/ESC SUPERVISOR FOR THE DURATION OF CONSTRUCTION.
23. THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/ESC SUPERVISOR AND MAINTAINED TO ENSURE CONTINUED PROPER FUNCTIONING. WRITTEN RECORDS SHALL BE KEPT OF WEEKLY REVIEWS OF THE ESC FACILITIES DURING THE WET SEASON (OCT. 1 TO APRIL 30) AND OF THE MONTHLY REVIEWS DURING THE DRY SEASON (MAY 1 TO SEPT. 30).
24. ANY AREAS OF EXPOSED SOILS, INCLUDING ROADWAY EMBANKMENTS, THAT WILL NOT BE DISTURBED FOR TWO DAYS DURING THE WET SEASON (OR SEVEN DAYS DURING THE DRY SEASON) SHALL BE IMMEDIATELY STABILIZED WITH THE APPROVED ESC METHODS (E.G., SEEDING, MULCHING, PLASTIC COVERING, ETC.)
25. ANY AREA NEEDING ESC MEASURES NOT REQUIRING IMMEDIATE ATTENTION SHALL BE ADDRESSED WITHIN FIFTEEN (15) DAYS.
26. THE ESC FACILITIES ON ACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN FORTY-EIGHT (48) HOURS FOLLOWING A TEMP STORM.
27. AT NO TIME SHALL MORE THAN ONE (1) FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAYING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO THE DOWNTOWNSYSTEM.
28. OFF-SITE STORMWATER AND/OR GROUNDWATER SHALL BE DIVERGED AWAY FROM SLOPES & DISTURBED AREAS WITH INTERCEPTOR DIKES, PIPES, OR SWALES. OFF-SITE STORMWATER SHALL BE MANAGED DIFFERENTLY FROM STORMWATER GENERATED ON-SITE.
29. EXCAVATED MATERIAL TO BE PLACED ON UPHILL SIDE OF TRENCH.
30. STABILIZED CONSTRUCTION ENTRANCES AND ROADS SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES, SUCH AS WASH PADS, MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
31. ANY PERMANENT FLOW CONTROL FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE FACILITY IS TO FUNCTION AS ULTIMATELY AS AN INFILTRATION SYSTEM, THE TEMPORARY FACILITY MUST BE GRADED SO THAT THE BOTTOM AND SIDES ARE AT LEAST THREE FEET ABOVE THE FINAL GRADE OF THE PERMANENT FACILITY.
32. PRIOR TO THE BEGINNING OF THE WET SEASON (OCT. 1), ALL DISTURBED AREAS SHALL BE REVIEWED TO IDENTIFY WHICH ONES CAN BE SEADED IN PREPARATION FOR THE WINTER RAINS. DISTURBED AREAS SHALL BE SEADED WITHIN ONE WEEK OF THE BEGINNING OF THE WET SEASON. A SKETCH MAP OF THOSE AREAS TO BE SEADED AND THOSE AREAS TO REMAIN UNCOVERED SHALL BE SUBMITTED TO THE TOWN OF LA CONNER INSPECTOR. THE TOWN OF LA CONNER INSPECTOR CAN REQUIRE SEEDING OF ADDITIONAL AREAS IN ORDER TO PROTECT SURFACE WATERS, ADJACENT PROPERTIES, OR DRAINAGE FACILITIES.

## ESC-2 WET SEASON SPECIAL PROVISIONS (OCTOBER 1 - APRIL 30)

1. THE ALLOWED TIME THAT A DISTURBED AREA CAN REMAIN UNWORKED WITHOUT COVER MEASURES IS REDUCED TO TWO DAYS, RATHER THAN SEVEN.
2. STOCKPILES AND STEEP CUT AND FILL SLOPES ARE TO BE PROTECTED IF UNWORKED FOR MORE THAN 12 HOURS.
3. COVER MATERIALS SUFFICIENT TO COVER ALL DISTURBED AREAS SHALL BE STOCKPILED ON SITE.
4. ALL AREAS THAT ARE TO BE UNWORKED DURING THE WET SEASON SHALL BE SEADED WITHIN ONE WEEK OF THE BEGINNING OF THE WET SEASON.
5. MULCH IS REQUIRED TO PROTECT ALL SEADED AREAS.
6. FIFTY LINEAR FEET OF SILT FENCE (AND THE NECESSARY STAKES) PER ACRE OF DISTURBANCE MUST BE STOCKPILED ON SITE.
7. CONSTRUCTION ROAD AND PARKING LOT STABILIZED ARE REQUIRED FOR ALL SITES UNLESS THE SITE IS UNDERLAIN BY COARSE-GRAINED SOIL.
8. SEDIMENT RETENTION IS REQUIRED UNLESS NO OFFSITE DISCHARGE IS ANTICIPATED FOR THE SPECIFIC DESIGN FLOW.
9. SURFACE WATER CONTROLS ARE REQUIRED UNLESS NO OFFSITE DISCHARGE IS ANTICIPATED FOR THE SPECIFIED DESIGN FLOW.
10. PHASING AND MORE CONSERVATIVE BMP'S MUST BE EVALUATED FOR CONSTRUCTION ACTIVITY NEAR SURFACE WATERS.
11. ANY RUNOFF GENERATED BY DEWATERING SHALL BE TREATED THROUGH CONSTRUCTION OF A SEDIMENT TRAP WHEN THERE IS SUFFICIENT SPACE OR BY RELEASING THE WATER TO A WELL-VEGETATED, GENTLY SLOPING AREA. SINCE PUMPS ARE USED FOR WATERING, IT MAY BE POSSIBLE TO PUMP THE SEDIMENT-LADEN WATER WELL AWAY FROM THE SURFACE WATER SO THAT VEGETATION CAN BE MORE EFFECTIVELY UTILIZED FOR TREATMENT. A STRAW BALE FILTER SHALL BE PLACED AROUND THE DISCHARGE FROM THE DEWATERING PUMP. IF THERE IS NOT SPACE FOR A SEDIMENT TRAP OR 25 FEET OF SUITABLE VEGETATION, OTHER FILTRATION METHODS SHALL BE REQUIRED.
12. THE FREQUENCY OF MAINTENANCE REVIEW INCREASES FROM MONTHLY TO WEEKLY.
13. SOILS SHALL BE STABILIZED AT THE END OF THE SHIFT, BEFORE A HOLIDAY OR WEEKEND, IF NEEDED, BASED ON THE WEATHER FORECAST.

## ESC-3 SENSITIVE AREAS SPECIAL PROVISIONS

1. WHENEVER POSSIBLE, PHASE ALL OR PART OF THE PROJECT SO THAT IT OCCURS DURING THE DRY SEASON. IF THIS IS IMPOSSIBLE, NOVEMBER THROUGH FEBRUARY SHALL BE AVOIDED SINCE THIS IS THE MOST LIKELY PERIOD FOR LARGE, HIGH-INTENSITY STORMS.
2. ALL PROJECTS SHALL BE COMPLETED AND STABILIZED AS QUICKLY AS POSSIBLE. LIMITING THE SIZE AND DURATION OF A PROJECT IS PROBABLY THE MOST EFFECTIVE FORM OF EROSION CONTROL.
3. WHERE APPROPRIATE, SANDBARS OR AN EQUIVALENT BARRIER SHALL BE CONSTRUCTED BETWEEN THE PROJECT AREA AND THE SURFACE WATER IN ORDER TO ISOLATE THE CONSTRUCTION AREA FROM HIGH WATER THAT MIGHT RESULT DUE TO PRECIPITATION.
4. ADDITIONAL PERIMETER PROTECTION SHALL BE CONSIDERED TO REDUCE THE LIKELIHOOD OF SEDIMENT ENTERING THE SURFACE WATERS. SUCH PROTECTION MIGHT INCLUDE MULTIPLE SILT FENCES, SILT FENCES WITH A HIGHER AOS, CONSTRUCTION OF A BERM, OR A THICK LAYER OF ORGANIC MULCH UPSLOPE OF A SILT FENCE.
5. IF WORK IS TO OCCUR WITHIN THE ORDINARY HIGH WATER MARK OF A STREAM, MOST PROJECTS MUST ISOLATE THE WORK AREA FROM THE STREAM BY DIVERTING THE STREAM OR CONSTRUCTING A COFFERDAM. CERTAIN SMALL PROJECTS THAT PROPOSE ONLY A SMALL AMOUNT OF GRADING MAY NOT REQUIRE ISOLATION SINCE DIVERSIONS TYPICALLY RESULT IN DISTURBANCE AND THE RELEASE OF SOME SEDIMENT TO THE STREAM. FOR SUCH SMALL PROJECTS, THE POTENTIAL IMPACTS FROM CONSTRUCTION WITH AND WITHOUT A DIVERSION MUST BE WEIGHED.
6. IF A STREAM MUST BE CROSSED, A TEMPORARY BRIDGE SHALL BE CONSIDERED RATHER THAN ALLOWING EQUIPMENT TO UTILIZE THE STREAMBED FOR A CROSSING.
7. ANY RUNOFF GENERATED BY DEWATERING SHALL BE TREATED THROUGH CONSTRUCTION OF A SEDIMENT TRAP WHEN THERE IS SUFFICIENT SPACE OR BY RELEASING THE WATER TO A WELL-VEGETATED, GENTLY SLOPING AREA. SINCE PUMPS ARE USED FOR DEWATERING, IT MAY BE POSSIBLE TO PUMP THE SEDIMENT-LADEN WATER WELL AWAY FROM THE SURFACE WATER SO THAT VEGETATION CAN BE MORE EFFECTIVELY UTILIZED FOR TREATMENT. A STRAW BALE FILTER SHALL BE PLACED AROUND THE DISCHARGE FROM THE DEWATERING PUMP. IF THERE IS NOT SPACE FOR A SEDIMENT TRAP OR 25 FEET OF SUITABLE VEGETATION, OTHER FILTRATION METHODS SHALL BE REQUIRED.

## ESC-4 FINAL SITE STABILIZATION NOTES

1. ALL DISTURBED AREAS OF THE SITE SHALL BE VEGETATED OR OTHERWISE PERMANENTLY STABILIZED AT A MINIMUM DISTURBED AREAS SHALL BE SEADED AND MULCHED WITH A HIGH LINEAGE THAT SUFFICIENT COVERAGE WILL DEVELOP SHORTLY AFTER FINAL APPROVAL. MULCHING IS REQUIRED TO ALLOW FINAL APPROVAL OF THE SITE EXCEPT FOR THE AREA OF THE DISTURBED AREA RESERVED FOR LANDSCAPING. THE ONLY EXCEPTIONS TO THESE REQUIREMENTS ARELOTS WITHIN A PLAT THAT ARE TO BE DEVELOPED UNDER AN APPROVED RESIDENTIAL PERMIT IMMEDIATELY FOLLOWING PLAT APPROVAL. IN THESE CASES, MULCH AND/OR TEMPORARY SEEDING ARE ADEQUATE FOR COVER.

2. STRUCTURAL MEASURES SUCH AS, BUT NOT LIMITED TO, SILT FENCES, PIPE SLOPES, DRAINS, CONSTRUCTION ENTRANCES, STORM DRAIN INLET PROTECTION, AND SEDIMENT TRAPS AND PONDS SHALL BE REMOVED FROM THE SITE. MEASURES THAT WILL QUICKLY DECOMPOSE, SUCH AS BRUSH BARRIERS AND ORGANIC MULCHES, MAY BE LEFT IN PLACE. IN THE CASE OF SILT FENCES, IT MAY BE BEST TO REMOVE FENCES IN CONJUNCTION WITH THE SEDIMENT, SINCE IT MAY BE NECESSARY TO BRING MACHINERY BACK IN TO REMOVE THEM. THIS WILL RESULT IN DISTURBED SOILS THAT WILL AGAIN REQUIRE PROTECTION. THE INSPECTOR MUST APPROVE AN APPLICANT'S PROPOSAL TO REMOVE FENCING PRIOR TO THE ESTABLISHMENT OF VEGETATION. IN SOME CASES, SUCH AS RESIDENTIAL BUILDING FOLLOWING PLAT DEVELOPMENT, IT MAY BE APPROPRIATE TO LEAVE SOME OR ALL ESC MEASURES FOR USE DURING SUBSEQUENT DEVELOPMENT. THIS SHALL BE DETERMINED ON A SITE-SPECIFIC BASIS.
3. ALL PERMANENT SURFACE WATER FACILITIES, INCLUDING CATCH BASINS, MANHOLES, PIPES, DITCHES, CHANNELS, R/D FACILITIES AND WATER QUALITY FACILITIES, SHALL BE CLEANED. ANY OFFSITE CATCH BASIN THAT REQUIRED PROTECTION DURING CONSTRUCTION SHALL ALSO BE CLEANED.
4. IF ONLY THE INFRASTRUCTURE OF THE SITE HAS BEEN DEVELOPED (E.G., SUBDIVISIONS AND SHORT PLAT) WITH BUILDING CONSTRUCTION TO OCCUR UNDER A DIFFERENT PERMIT, THEN THE SENSITIVE AREA BUFFERS, SENSITIVE AREA TRACTS OR SENSITIVE AREA SETBACK AREA SHALL BE CLEARLY MARKED AS DESCRIBED IN SECTION D.4.1 IN ORDER TO ALERT FUTURE BUYERS AND BUILDERS.

## ESC-5 ROAD AND UTILITIES ESC NOTES

1. PHASING THE PROJECT SO THAT THE SITE IS WORKED PROGRESSIVELY FROM END TO END, RATHER THAN CLEARING AND GRUBBING THE ENTIRE LENGTH OF THE PROJECT, THIS RESULTS IN SMALLER EXPOSED AREAS FOR SHORTER DURATIONS, THUS REDUCING THE EROSION RISK.
2. MULCHING AND VEGETATING CUT AND FILL SLOPES AS SOON AS THEY ARE GRADED. FREQUENTLY, THIS IS DONE AT THE END OF CONSTRUCTION WHEN PAVING OR UTILITY INSTALLATION IS COMPLETE. VEGETATING THESE AREAS AT THE START OF THE PROJECT STABILIZED THOSE AREAS MOST SUSCEPTIBLE TO EROSION.
3. PROTECTING ALL CATCH BASIN INLETS WITH CATCH BASIN INSERTS WHEN THESE DO NOT DRAIN TO PONDS OR TRAPS. THIS WILL NOT PROVIDE THE SAME LEVEL OF PROTECTION AS A SEDIMENT POND OR TRAP, BUT CAN REMOVE MOST OF THE SAND-SIZED MATERIAL ENTRAINED IN THE RUNOFF.
4. PHASING THE PROJECT SO THAT ALL CLEARING AND GRADING IN SENSITIVE AREA BUFFERS OCCURS IN THE DRY SEASON. THIS SUBSTANTIALLY REDUCES THE CHANCE OF EROSION AND ALLOWS FOR RAPID REVEGETATION IN THE LATE SUMMER AND EARLY FALL.
5. USING FLOCULANT TO REDUCE THE TURBIDITY OF WATER RELEASED FROM SEDIMENT PONDS, WHEN APPROVED BY THE DEPARTMENT OF ECOLOGY.
6. HIRING A PRIVATE CONSULTANT WITH EXPERTISE IN ESC TO REVIEW AND MONITOR THE SITE.
7. THE ESC SUPERVISOR NAME IS: \_\_\_\_\_  
24 HR. CONTACT NUMBER IS: \_\_\_\_\_
8. THE APPLICANT'S NAME IS: \_\_\_\_\_  
24 HR. CONTACT NUMBER IS: \_\_\_\_\_
9. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED BY A CONTINUOUS LENGTH OF SURVEY TAPE (OR FENCING, IF REQUIRED) PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE CLEARING LIMITS SHALL BE PERMITTED. THE CLEARING LIMITS SHALL BE MAINTAINED BY THE APPLICANT/ESC SUPERVISOR FOR THE DURATION OF CONSTRUCTION.
10. THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/ESC SUPERVISOR AND MAINTAINED TO ENSURE CONTINUED PROPER FUNCTIONING. WRITTEN RECORDS SHALL BE KEPT OF WEEKLY REVIEWS OF THE ESC FACILITIES DURING THE WET SEASON (OCT. 1 TO APRIL 30) AND OF THE MONTHLY REVIEWS DURING THE DRY SEASON (MAY 1 TO SEPT. 30).
11. ANY AREAS OF EXPOSED SOILS, INCLUDING ROADWAY EMBANKMENTS, THAT WILL NOT BE DISTURBED FOR TWO DAYS DURING THE WET SEASON (OR SEVEN DAYS DURING THE DRY SEASON) SHALL BE IMMEDIATELY STABILIZED WITH THE APPROVED ESC METHODS (E.G., SEEDING, MULCHING, PLASTIC COVERING, ETC.)
12. ANY AREA NEEDING ESC MEASURES NOT REQUIRING IMMEDIATE ATTENTION SHALL BE ADDRESSED WITHIN FIFTEEN (15) DAYS.
13. THE ESC FACILITIES ON ACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN FORTY-EIGHT (48) HOURS FOLLOWING A TEMP STORM.
14. AT NO TIME SHALL MORE THAN ONE (1) FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAYING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO THE DOWNTOWNSYSTEM.
15. OFF-SITE STORMWATER AND/OR GROUNDWATER SHALL BE DIVERGED AWAY FROM SLOPES & DISTURBED AREAS WITH INTERCEPTOR DIKES, PIPES, OR SWALES. OFF-SITE STORMWATER SHALL BE MANAGED DIFFERENTLY FROM STORMWATER GENERATED ON-SITE.
16. EXCAVATED MATERIAL TO BE PLACED ON UPHILL SIDE OF TRENCH.
17. STABILIZED CONSTRUCTION ENTRANCES AND ROADS SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES, SUCH AS WASH PADS, MAY BE REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT.
18. ANY PERMANENT FLOW CONTROL FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE FACILITY IS TO FUNCTION AS ULTIMATELY AS AN INFILTRATION SYSTEM, THE TEMPORARY FACILITY MUST BE GRADED SO THAT THE BOTTOM AND SIDES ARE AT LEAST THREE FEET ABOVE THE FINAL GRADE OF THE PERMANENT FACILITY.
19. PRIOR TO THE BEGINNING OF THE WET SEASON (OCT. 1), ALL DISTURBED AREAS SHALL BE REVIEWED TO IDENTIFY WHICH ONES CAN BE SEADED IN PREPARATION FOR THE WINTER RAINS. DISTURBED AREAS SHALL BE SEADED WITHIN ONE WEEK OF THE BEGINNING OF THE WET SEASON. A SKETCH MAP OF THOSE AREAS TO BE SEADED AND THOSE AREAS TO REMAIN UNCOVERED SHALL BE SUBMITTED TO THE TOWN OF LA CONNER INSPECTOR. THE TOWN OF LA CONNER INSPECTOR CAN REQUIRE SEEDING OF ADDITIONAL AREAS IN ORDER TO PROTECT SURFACE WATERS, ADJACENT PROPERTIES, OR DRAINAGE FACILITIES.

## ESC-6 CONSTRUCTION SEQUENCE

1. ATTEND PRE-CONSTRUCTION MEETING.
2. FLAG OR FENCE CLEARING LIMITS.
3. POST SIGN WITH NAME AND PHONE NUMBER OF ESC SUPERVISOR.
4. INSTALL CATCH BASIN PROTECTION IF REQUIRED.
5. GRADE AND INSTALL CONSTRUCTION ENTRANCE(S).
6. INSTALL PERIMETER PROTECTION (SILT FENCE, BRUSH BARRIER, ETC.).
7. CONSTRUCT SEDIMENT PONDS AND TRAPS.
8. GRADE AND STABILIZE CONSTRUCTION ROADS.
9. CONSTRUCT SURFACE WATER CONTROLS (INTERCEPTOR DIKES, PIPE SLOPE DRAINS, ETC.) SIMULTANEOUSLY WITH CLEARING AND GRADING FOR PROJECT DEVELOPMENT.
10. MAINTAIN EROSION CONTROL MEASURES IN ACCORDANCE WITH THE TOWN OF LA CONNER STANDARDS AND MANUFACTURER'S RECOMMENDATIONS.
11. RELOCATE SURFACE WATER CONTROLS AND EROSION CONTROL MEASURES OR INSTALL NEW MEASURES SO THAT AS SITE CONDITIONS CHANGE THE EROSION AND SEDIMENT CONTROL IS ALWAYS IN ACCORDANCE WITH THE TOWN OF LA CONNER EROSION AND SEDIMENT CONTROL STANDARDS.
12. COVER ALL AREAS THAT WILL BE UNWORKED FOR MORE THAN SEVEN DAYS OR TWO DAYS DURING THE WET SEASON WITH STRAW, WOOD FIBER MULCH, COMPOST, PLASTIC SHEETING OR EQUIVALENT.
13. STABILIZE ALL AREAS THAT REACH FINAL GRADE WITHIN SEVEN DAYS.
14. SEED OR SOD ANY AREAS TO REMAIN UNWORKED FOR MORE THAN 30 DAYS.
15. UPON COMPLETION OF THE PROJECT, ALL DISTURBED AREAS MUST BE STABILIZED AND BMP'S REMOVED IF APPROPRIATE.

1. THE ALLOWED TIME THAT A DISTURBED AREA CAN REMAIN UNWORKED WITHOUT COVER MEASURES IS REDUCED TO TWO DAYS, RATHER THAN SEVEN.
2. STOCKPILES AND STEEP CUT AND FILL SLOPES ARE TO BE PROTECTED IF UNWORKED FOR MORE THAN 12 HOURS.
3. COVER MATERIALS SUFFICIENT TO COVER ALL DISTURBED AREAS SHALL BE STOCKPILED ON SITE.
4. ALL AREAS THAT ARE TO BE UNWORKED DURING THE WET SEASON SHALL BE SEADED WITHIN ONE WEEK OF THE BEGINNING OF THE WET SEASON.
5. MULCH IS REQUIRED TO PROTECT ALL SEADED AREAS.
6. FIFTY LINEAR FEET OF SILT FENCE (AND THE NECESSARY STAKES) PER ACRE OF DISTURBANCE MUST BE STOCKPILED ON SITE.
7. CONSTRUCTION ROAD AND PARKING LOT STABILIZED ARE REQUIRED FOR ALL SITES UNLESS THE SITE IS UNDERLAIN BY COARSE-GRAINED SOIL.
8. SEDIMENT RETENTION IS REQUIRED UNLESS NO OFFSITE DISCHARGE IS ANTICIPATED FOR THE SPECIFIC DESIGN FLOW.
9. SURFACE WATER CONTROLS ARE REQUIRED UNLESS NO OFFSITE DISCHARGE IS ANTICIPATED FOR THE SPECIFIED DESIGN FLOW.
10. PHASING AND MORE CONSERVATIVE BMP'S MUST BE EVALUATED FOR CONSTRUCTION ACTIVITY NEAR SURFACE WATERS.
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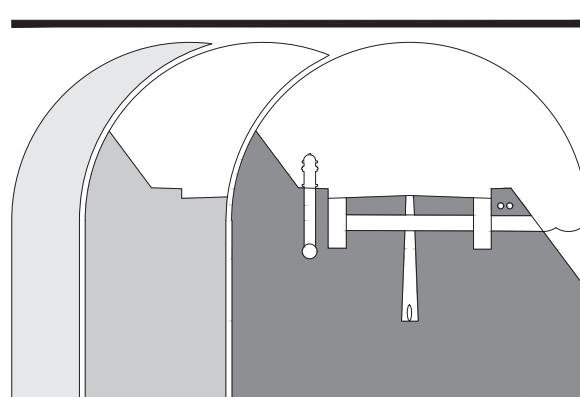
## SEEDING

IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M.

LA CONNER, WASHINGTON

SHEET NOTES

- CONTRACTOR TO PROTECT EXISTING TREES ALONG WESTERN PROPERTY LINE AND ASSOCIATED ROOT SYSTEM. THE PROJECT LANDSCAPE ARCHITECT AND/OR ARBORIST IS TO BE CONTACTED IMMEDIATELY SHOULD ANY CONCERN ARISE REGARDING THE TREES.
- THE 5.0' LANDSCAPE STRIP BORDERING THE NORTHERN SIDEWALK IS TO NOT ADVERSELY IMPACT SIDEWALK LIGHTING. FOR ADDITIONAL DETAILS, REFER TO THE LANDSCAPE PLAN.
- CONTRACTOR TO MINIMIZE IMPACT TO ALL FOUND MONUMENTS AND TO SCHEDULE RESTORATION OF MONUMENT(S) BY A LICENSED PROFESSIONAL LAND SURVEYOR AT THE COMPLETION OF CONSTRUCTION, AS NECESSARY AND APPROPRIATE.
- BUILDING DIMENSIONS: SEE ARCHITECTURE PLANS  
PROPOSED UNITS: 6 LODGING UNITS, 14 DWELLING UNITS  
GROUND LEVEL ELEV: 8.70' (NO OCCUPANCY AT THIS ELEVATION)  
FINISHED FLOOR (UNIT) ELEV: 12.8' (OCCUPANCY AT THIS ELEVATION)
- BUILDING FOOTPRINT AREA: 3,560 SF - 0.0857 AC  
BUILDING COVERAGE (INCL. COVERED PARKING AND OVERHANGS): 9,473 SF - 0.2175 AC
- PARKING STALLS PROVIDED: 10 STANDARD STALLS, 12 COMPACT STALLS, 2 ACCESSIBLE STALLS (24 TOTAL); SEE ARCHITECTURAL PLAN FOR STALL WIDTHS
- DUMPSTER AREA DIMENSIONS: 7.5' X 9' (CONSTRUCTED PER DETAIL D/2.0)
- FOURTH STREET IS TO HAVE A FULL STREET ASPHALT OVERLAY WITH GRADING AS SHOWN ON PLANS. CENTER STREET IS TO HAVE A QUARTER-HALF STREET OVERLAY. OVERLAY EXTENTS ARE OUTLINED WITH SAWCUT LINES AND/OR EDGE OF ASPHALT LINES. CONTRACTOR IS TO PAVE OVERLAY AND CONSTRUCT ADDITIONAL ROAD WIDTH PER TOWN OF LA CONNER STANDARD DETAIL G3/3.0. CONCRETE CURB AND GUTTER PER WSDOT STANDARD PLAN F-1a.
- CONCRETE PAVEMENT WITHIN THE PROPERTY BOUNDARY IS TO BE PER DETAIL B/2.0.
- ALL SIDEWALKS WITHIN THE RIGHT-OF-WAY ARE TO BE CONSTRUCTED PER WSDOT STANDARD PLAN F-3 WITH A TYPE 1 DRIVEWAY ENTRANCE OFF OF FOURTH STREET PER WSDOT STANDARD PLAN F-4. PERPENDICULAR CURB RAMP AT INTERSECTION OF CENTER AND FOURTH STREET PER WSDOT STD PLAN F-40-15-04 ON SHEET C4.0. CONTRACTOR TO INSTALL TACTILE WARNING STRIPS PER ADA STANDARDS ON ALL SIDEWALK RAMPS (ASPHALT/CONCRETE).
- CONTRACTOR TO COORDINATE ALL UTILITY AND ROAD SHUT-DOWN PERIODS WITH THE TOWN OF LA CONNER, EMERGENCY DEPARTMENTS, UTILITY PURVYORS AND AFFECTED PROPERTIES (RESIDENTIAL AND COMMERCIAL). CONTRACTOR TO PROVIDE TRAFFIC CONTROL PLAN, APPROVED BY THE TOWN OF LA CONNER, PRIOR TO BEGINNING CONSTRUCTION.
- CONTRACTOR TO REMOVE ALL EXISTING SIGNS AFFECTED BY CONSTRUCTION, INCLUDING BUT NOT LIMITED TO "SHARE THE ROAD" AND BIKE-PEDESTRIAN DIAMOND-SHAPED SIGN ON CENTER STREET AND THE STOP SIGN IN THE SOUTHEAST CORNER OF THE CENTER AND FOURTH STREET INTERSECTION; TOWN OF LA CONNER TO REINSTATE SIGN PER MUTCD.
- CONTRACTOR TO RESTORE EXISTING RUBBER SPEED BUMP AFFECTED BY CONSTRUCTION, OR REMOVE AND REPLACE AS DIRECTED BY THE TOWN OF LA CONNER, PER SHEET C4.0.
- CONTRACTOR TO INSTALL NEW SIGN AND POST SAYING "NO PARKING WEST OF THIS SIGN" IN THE NORTHWEST CORNER OF THE PROJECT, WHERE INDICATED ON THE PLANS. CONTRACTOR TO COORDINATE LOCATION WITH TOWN OF LA CONNER PUBLIC WORKS.
- CONTRACTOR TO REPAINT SKIP YELLOW CENTER STRIPE ON CENTER AND FOURTH STREET, AND TO PAINT/REPAINT AFFECTED STOP BARS AT THE CENTER AND FOURTH STREET INTERSECTION, PLACED PER MUTCD.
- CONTRACTOR TO FOLLOW TOWN OF LA CONNER'S TYPICAL TRENCH SECTION DETAIL FOR ALL TRENCH RESTORATION WORK WITHIN THE RIGHT-OF-WAY (DETAIL G1/3.0) AND SDG DETAIL E/3.2 FOR ALL ON-SITE TRENCHES.
- CONTRACTOR TO INSTALL TWO ELECTRIC VEHICLE CHARGING STATIONS AND 2" POWER CONDUIT FOR FUTURE CHARGING STATIONS PER THE ELECTRICAL PLAN



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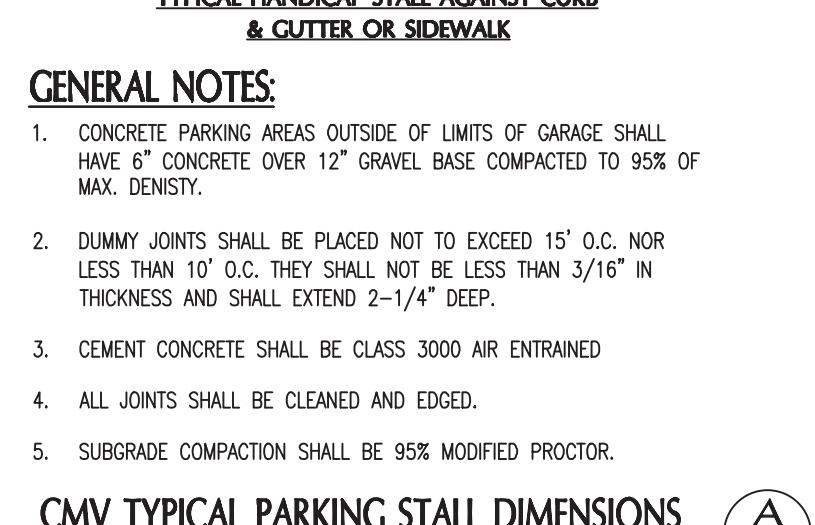
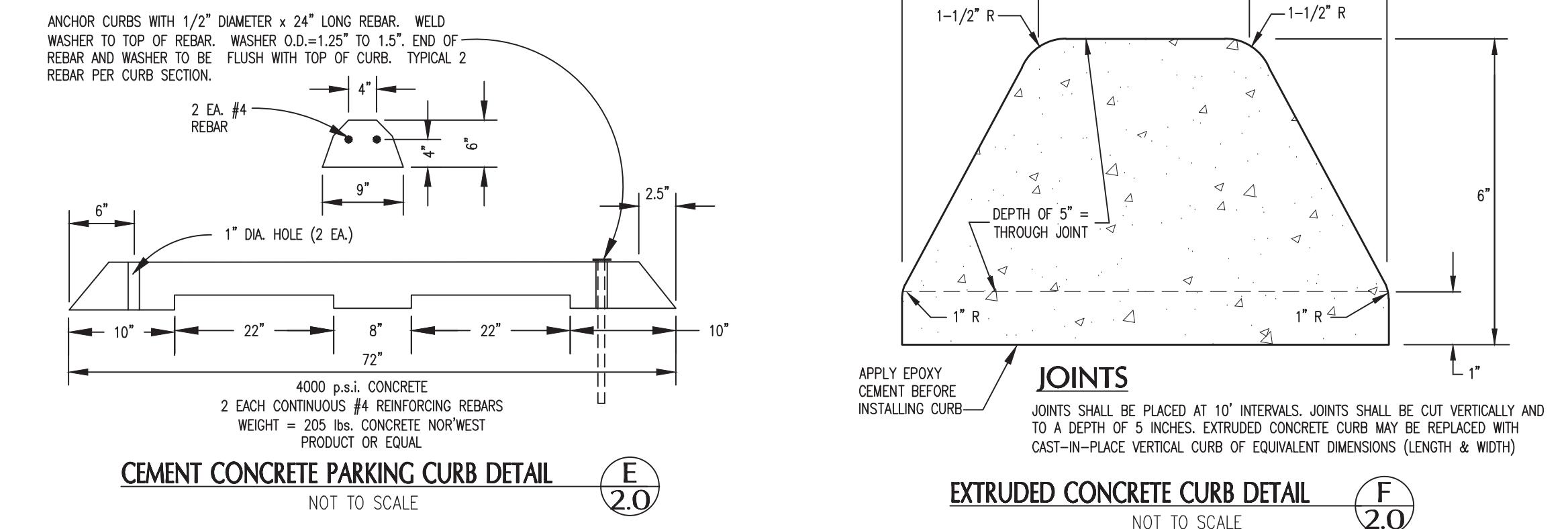
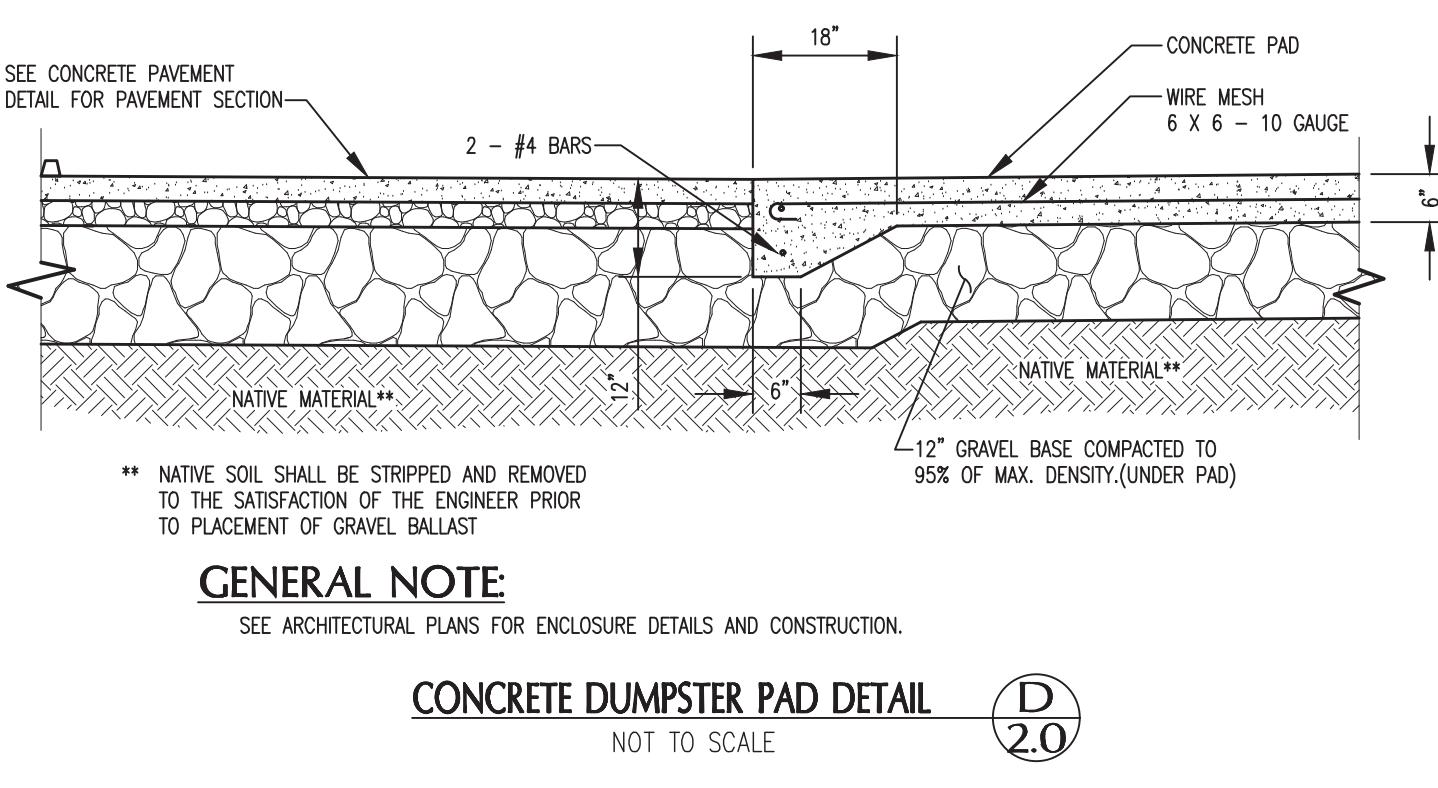
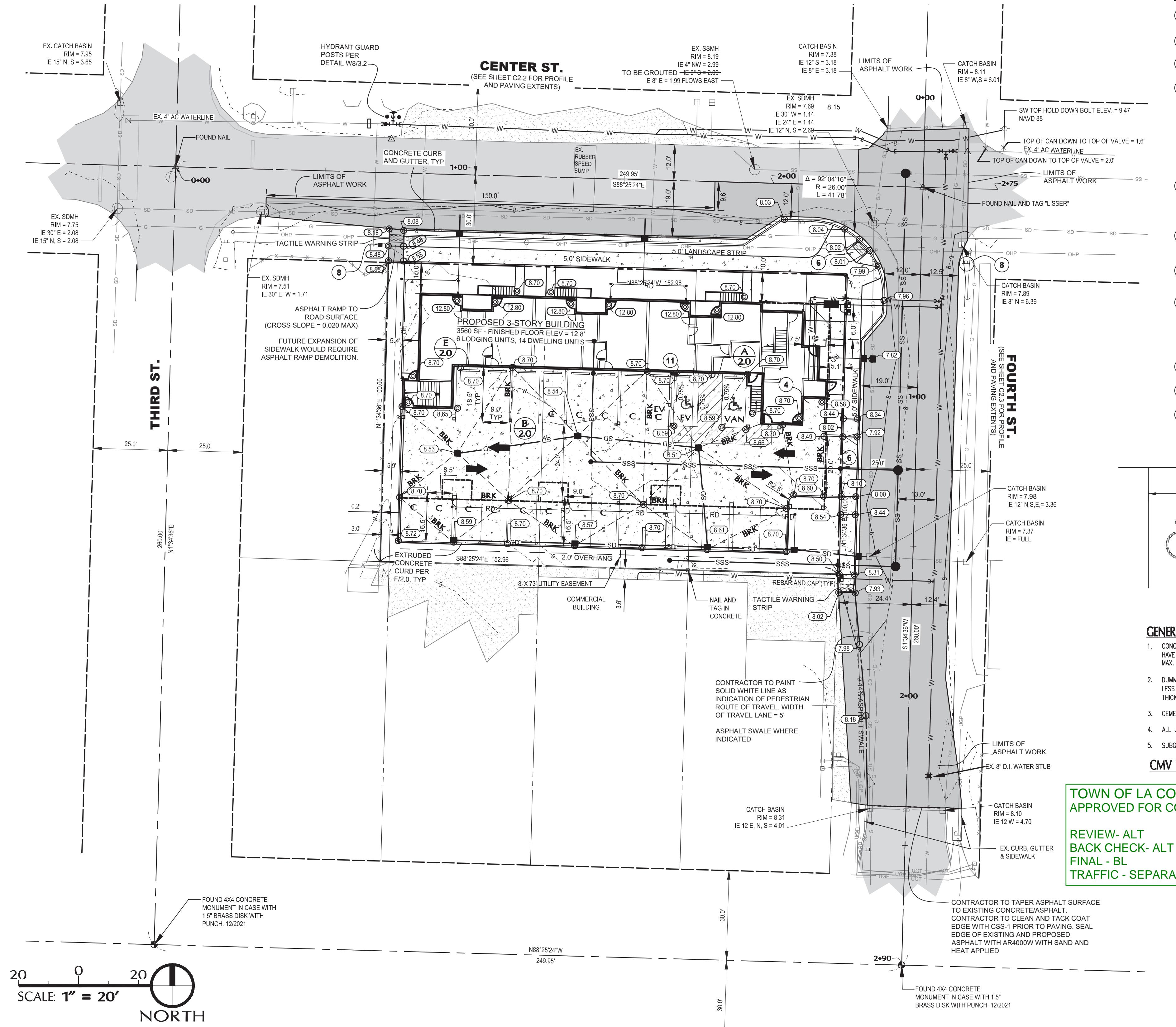
SHEET REVISIONS  
NO. DATE OF REVISION APPROVED  
1 02.23 ARCHITECT REVISION P.L.S.  
2 11.27.23 DEA REVISION P.L.S.  
3 02.29.24 ARCH REVISION P.L.S.  
4 05.13.24 DEA/CITY REVISION P.L.S.

CALL 48 HOURS  
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PROJECT: CENTER STREET FOR MIXED-USE  
KSA INVESTMENTS, LLC

GRADING AND DIMENSIONAL PLAN

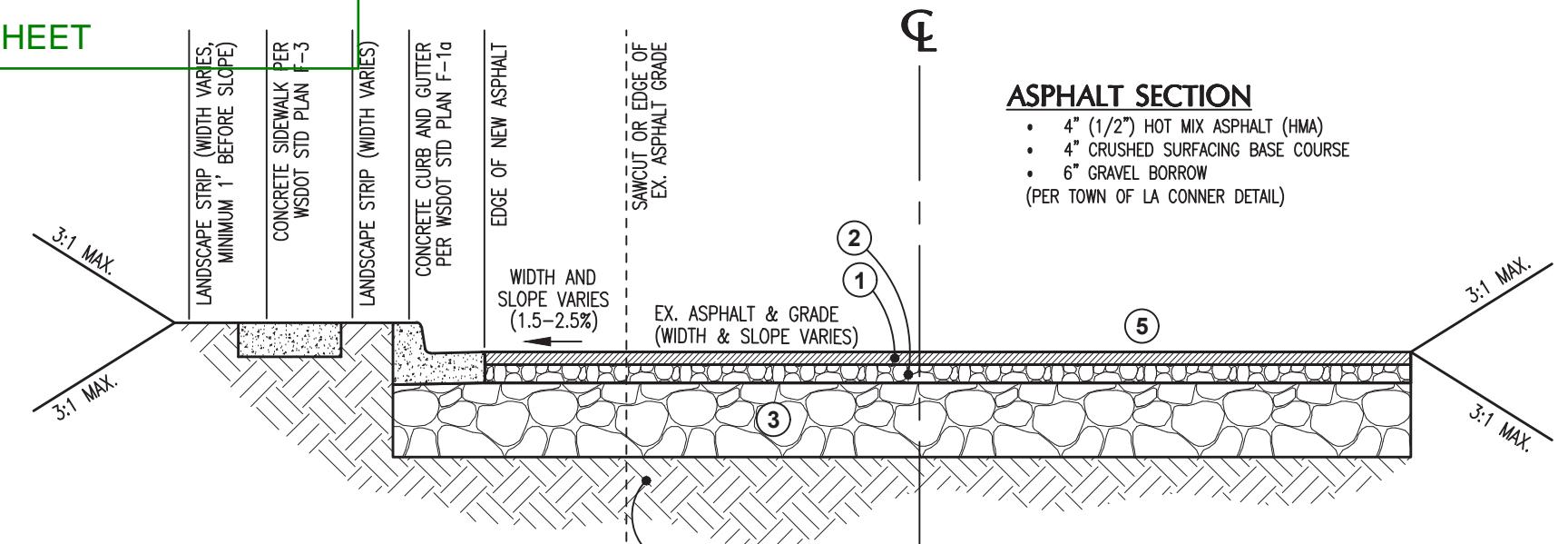
SHEET DESCRIPTION:



CMV TYPICAL PARKING STALL DIMENSIONS  
NOT TO SCALE  
A.2.0

TOWN OF LA CONNER  
APPROVED FOR CONSTRUCTION - 06/18/24

REVIEW- ALT  
BACK CHECK- ALT  
FINAL - BL  
TRAFFIC - SEPARATE SHEET



GENERAL NOTES:

- ALL CONCRETE SHALL BE PORTLAND CEMENT CONCRETE (PCC) W/ LIGHT BROOM FINISH. CRACK CONTROL JOINTS SHALL BE PLACED AT A MAXIMUM DISTANCE OF 12 FEET, PERPENDICULAR TO THE DIRECTION OF MAXIMUM STRESS. CRACK CONTROL JOINTS MAY BE CREATED BY PLACING AN INSERT OR GROOVE INTO THE FRESH CONCRETE SURFACE DURING FINISHING, OR BY SAWCUTTING THE CONCRETE AFTER IT HAS INITIALLY SET-UP. WE RECOMMEND THE DEPTH OF THE CRACK CONTROL JOINTS SHALL BE APPROXIMATELY 1/4 THE THICKNESS OF THE CONCRETE; OR ABOUT 1 1/2 INCHES DEEP FOR THE RECOMMENDED CONCRETE THICKNESS OF 6 INCHES. CONTROL JOINTS SHALL BE SEALED WITH AN APPROPRIATE SEALANT TO HELP RESTRICT WATER INFILTRATION INTO THE JOINTS.
- GRAVEL BORROW SHALL SUPPORT ALL PAVEMENT. GRAVEL BORROW WITH LESS THAN 5% PASSING THE 200 SIEVE, SHALL CONFORM TO SECTION 9-03.14 OF THE 2023 STANDARD SPECIFICATIONS COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY IN ACCORDANCE WITH ASTM D-1557 TESTING. EXISTING GRAVEL MATERIAL MAY BE UTILIZED AS GRAVEL BORROW AS DETERMINED BY THE GEOTECHNICAL ENGINEER.
- THE EXCAVATED SUBGRADE SHALL BE FREE OF TOPSOIL, ORGANICS, AND OTHER DELETERIOUS MATERIAL, COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY IN ACCORDANCE WITH ASTM D-1557 TESTING, PREPARED CONFORMING TO SECTION 2-06.3(1) OF THE 2023 STANDARD SPECIFICATIONS. AFTER SUBGRADE EXCAVATION, PRIOR TO THE PLACEMENT OF ANY STRUCTURAL FILL, CRUSHED ROCK, AND GRAVEL, THE CONTRACTOR SHALL SCHEDULE THEIR RETAINED GEOTECHNICAL ENGINEER TO INSPECT AND APPROVE ALL SURFACE CONDITIONS. THE CONTRACTOR SHALL PROVIDE A FULL-LOADED GRAVEL TRUCK FOR THIS INSPECTION PHASE.



GENERAL NOTES:

- ALL ASPHALT SHALL BE 1/2" HOT MIX ASPHALT CONFORMING TO SECTION 5-04 OF THE 2023 STANDARD SPECIFICATIONS, COMPAKTED TO A MINIMUM OF 91% RICE DENSITY. WHERE PROPOSED ASPHALT ABBUTS EXISTING ASPHALT, THE EXISTING ASPHALT SHALL BE SAW CUT FULL DEPTH AND TACK COATED IMMEDIATELY BEFORE PAVING. ALL SURFACE JOINTS SHALL BE SEALED WITH AR 4000 W AND SAND, APPLIED WITH HEAT.
- CRUSHED SURFACING BASE COURSE SHALL CONFORM TO SECTION 9-03.9(3) OF THE 2023 STANDARD SPECIFICATIONS, COMPAKTED TO A MINIMUM OF 95% MAXIMUM DENSITY IN ACCORDANCE WITH ASTM D-1557 TESTING. ALL CRUSHED SURFACING SHALL BE SPRAYED WITH SOIL RESIDUAL HERBICIDE A MAXIMUM OF 24 HOURS PRIOR TO PAVING, ACCORDING TO SECTION 5-04.3(5) OF THE 2023 STANDARD SPECIFICATIONS.
- GRAVEL BASE, A MINIMUM OF 6 INCH COMPACTED DEPTH SHALL SUPPORT ALL PAVEMENT. GRAVEL BORROW WITH LESS THAN 5% PASSING THE 200 SIEVE, SHALL CONFORM TO SECTION 9-03.14 OF THE 2023 STANDARD SPECIFICATIONS COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY IN ACCORDANCE WITH ASTM D-1557 TESTING, PREPARED CONFORMING TO SECTION 2-06.3(1) OF THE 2023 STANDARD SPECIFICATIONS.
- THE EXCAVATED SUBGRADE SHALL BE FREE OF TOPSOIL, ORGANICS, AND OTHER DELETERIOUS MATERIAL, COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY IN ACCORDANCE WITH ASTM D-1557 TESTING, PREPARED CONFORMING TO SECTION 2-06.3(1) OF THE 2023 STANDARD SPECIFICATIONS.
- IN ALL AREAS OUTSIDE OF THE INDICATED ROAD WIDENING ON FOURTH STREET, CONTRACTOR TO MATCH EXISTING SUBGRADE SECTIONS, BACKFILL TRENCHES PER DETAIL G1/3.0 AND REFRESH ASPHALT SURFACE. SEE TOWN OF LA CONNER DETAIL G3/3.0 FOR ADDITIONAL INFORMATION.

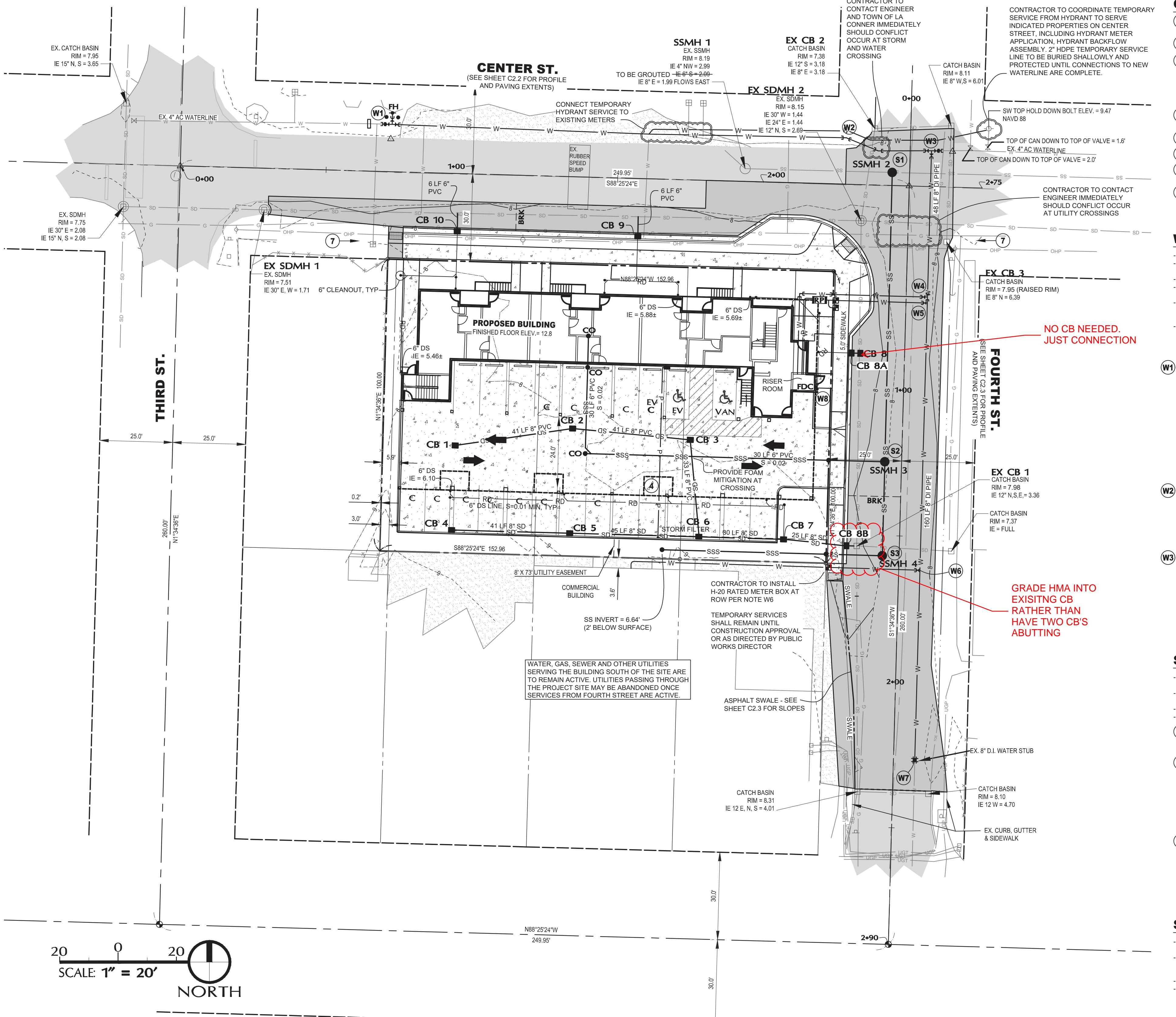


SCALE: 1" = 20'  
DRAWN BY: C.SEVERIN  
DESIGNED BY: P.SEVERIN  
DATE: 02.22.2024  
JOB NUMBER: 21098  
DWG NAME: 21098PLN.DWG  
SHEET NUMBER: C.2.0

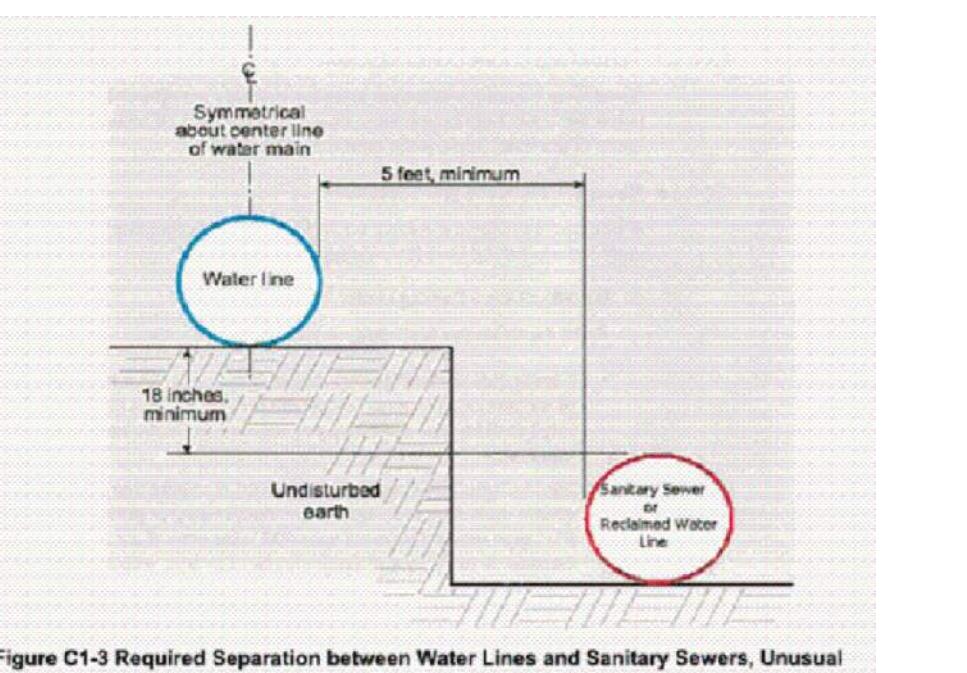
C.2.0

**IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA**

# CONNER, WASHINGTON



**TOWN OF LA CONNER  
APPROVED FOR CONSTRUCTION - 06/18/24**



# DOE MINIMUM SEPARATIONS

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NOT TO SCALE

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## GENERAL UTILITY NOTES

- CONTRACTOR TO LOCATE AND VERIFY ALL SIZES, LOCATIONS, INVERTS AND MATERIALS OF EXISTING UTILITIES. NOTIFY ENGINEER IMMEDIATELY SHOULD ANY DISCREPANCIES OCCUR. CONTRACTOR TO COORDINATE WITH ALL FRANCHISE UTILITIES.
- ALL UTILITY STATIONING AND OFFSETS ON THIS SHEET ARE BASED ON THE FOURTH STREET ALIGNMENT, UNLESS SPECIFICALLY STATED OTHERWISE.
- ALL UTILITIES ARE TO BE CONNECTED AND INSTALLED BY THE CONTRACTOR UNLESS OTHERWISE DIRECTED BY THE TOWN OF LA CONNER. CONTRACTOR IS RESPONSIBLE FOR COORDINATING UTILITY AND ROAD SHUT-DOWN PERIODS TO THE TOWN OF LA CONNER, UTILITY PURVEYORS, AND AFFECTED CUSTOMERS (COMMERCIAL/RESIDENTIAL). CONTRACTOR IS RESPONSIBLE FOR ALL SAWCUTS, EXCAVATION, REMOVAL OF MATERIALS, CONNECTIONS, BACKFILL, COMPACTION, RESTORATION, RESURFACING AND OTHER WORK AS NECESSARY FOR PROJECT COMPLETION. CONTRACTOR TO PROVIDE A TRAFFIC CONTROL PLAN, APPROVED BY THE TOWN OF LA CONNER. CONTRACTOR TO PROVIDE DOCUMENTATION FOR MATERIAL SOURCES AND THE DUMP SITE, SUBJECT TO THE APPROVAL OF THE TOWN OF LA CONNER.
- ELECTRIC VEHICLE (EV) CHARGING STATION AND 1-2" CONDUIT FOR FUTURE CHARGING STATION(S) TO BE INSTALLED BY THE CONTRACTOR WHERE INDICATED ON THESE PLANS, SEE ELECTRICAL PLANS FOR ADDITIONAL INFORMATION.
- CONTRACTOR TO PROVIDE POWER FROM BUILDING TO PROPOSED REDUCED-PRESSURE BACKFLOW ASSEMBLY (RPBA) IN WATERLINE NOTE W5 BELOW.
- CONTRACTOR TO ADJUST ALL RIMS (MANHOLE, CATCH BASIN, VALVE BOX, ETC) TO FINISHED GRADE
- CONTRACTOR TO COORDINATE WITH PUGET SOUND ENERGY, LA CONNER PUBLIC WORKS AND ALL NECESSARY UTILITY PURVEYORS AND AFFECTED CUSTOMERS REGARDING THE PROCESS OF MOVING POWER AND COMMUNICATION LINES UNDERGROUND ALONG THE CENTER STREET FRONTRAGE.
- CONTRACTOR TO COORDINATE LOCATION OF ELECTRICAL VEHICLE CHARGING STATION EMERGENCY SHUT-OFF SWITCH WITH ELECTRICIAN AND TOWN OF LA CONNER. LOCATION MUST BE WITHIN 50' OF CHARGING STATION BUT NOT WITHIN CLOSE PROXIMITY OF THE STATION.

## **WATERLINE NOTES**

ALL JOINTS TO BE RESTRAINED UNDER THE FOLLOWING WATERLINE NOTES: W1-4, W7. THRUST-BLOCKING PER DETAIL W4/3.1  
FIXTURES TO BE INSTALLED WITHIN THE BUILDING'S MECHANICAL/RISER ROOM INCLUDE A 6" DOUBLE-CHECK DETECTOR ASSEMBLY (FIRE LINE) PER TOWN OF LA CONNER  
STANDARDS. SEE MECHANICAL PLAN FOR ADDITIONAL INFORMATION.  
CONTRACTOR TO VERIFY FIRE LINE SIZE WITH SPRINKLER DESIGNER PRIOR TO INSTALLATION (W4).  
CONTRACTOR TO INSTALL ALL WATERLINE ELEMENTS WITH REASONABLE SEPARATION FROM OTHER UTILITIES, AND PER DETAIL A/3.1 FOR SEWER. WHERE VERTICAL  
SEPARATIONS CANNOT BE MET, CONTRACTOR TO CENTER PIPE LENGTHS OVER THE SEWER.  
CONTRACTOR TO INSTALL BLOW-OFF ASSEMBLIES, DECHLORINATE (AND OBTAIN APPROVAL OF DISCHARGE POINT FOR TEST WATER), PERFORM PRESSURE AND  
BACTERIOLOGICAL TESTING, AND OBTAIN THE TOWN OF LA CONNER'S APPROVAL PRIOR TO CONNECTING THE PROPOSED WATERLINE TO ANY PORTION OF THE EXISTING  
WATER DISTRIBUTION SYSTEM (SEE W1, W3, W7). CONTRACTOR TO PROVIDE PLAN FOR TESTING AND DISINFECTION AS A PART OF THE PROJECT SUBMITTALS.  
CONTRACTOR TO COORDINATE ALL WATERLINE SHUT-DOWNS WITH LA CONNER FIRE DEPARTMENT, LA CONNER PUBLIC WORKS AND AFFECTED CUSTOMERS (303, 307 AND 313  
CENTER STREET) WITH AT LEAST 4 DAYS NOTICE.

STA 0+72.20, 16.25 LT  
(CENTER STREET ALIGNMENT)  
- CONCRETE KEY BLOCK (W OF PROPOSED TEE)  
- 4" TRANSITION COUPLING, DI X AC  
- 4" D.I. PUP  
- 8" X 4" D.I. REDUCER, PE X MI

STA 0+67.89, 7.30 LT  
- 8" X 6" D.I. TEE, MJ X FL  
- 6" D.I. GATE VALVE, FL X MJ (W) PER DETAIL W6/3.2, TYP  
30 LF 6" D.I. PIPE TO VALVE (W)  
- 6" D.I. GATE VALVE, MJ X MJ  
1 LF 6" D.I. PIPE TO BEND

W6

STA 1+61.52, 7.30 LT  
1 - 1.5" SERVICE TAP INTO PROPOSED 8"  
WATERLINE PER DETAIL W2/3.1  
1 - 1.5" GATE VALVE PER DETAILS W2/3.1  
AND W6/3.2  
30 LF 1.5" WATER SERVICE LINE (W)

DETAIL SHOWS 2"  
SETTER WITH 1.5"

**CALL 48 HOURS  
BEFORE YOU DIG  
1.800.424.5555**

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**CENTER STREET  
MIXED-USE**

**FOR**

**KSA INVESTMENTS, LLC**

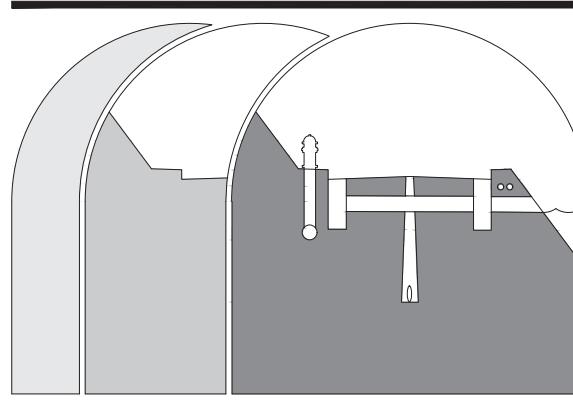
E: **1" = 20'**  
N BY: **C.SEVERIN**  
NED BY: **P.SEVERIN**  
**02.22.2024**  
NUMBER: **21098**  
NAME: **21098PLN.DWG**  
T NUMBER:

C2.1



**IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA CONNER, WASHINGTON**

A scale bar and a north arrow. The scale bar is a horizontal line with tick marks. The left end is labeled '10' and the right end is labeled '0'. The right end has a vertical line extending upwards, ending in a circle with a vertical line through it, representing a north arrow.



**Sound Development Group**  
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P.O. Box 1705 • 1111 Cleveland Avenue, Suite 200  
Mount Vernon, WA 98273      Tel: 360-404-2011  
CLIENT REVIEWS

SHEET REVISIONS:			
NO.	DATE	DESCRIPTION	APPROVE
1	9.22.23	ARCHITECT REVISION	P.L.S.
2	11.27.23	DEA REVISION	P.L.S.
3	02.29.24	ARCH REVISION	P.L.S.
4	05.13.24	DEA/CITY REVISION	P.L.S.

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**CALL 48 HOURS  
BEFORE YOU DIG  
1.800.424.5555**

## PROJECT:

INER  
6/18/24  
E SHEET

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## SHEET DESCRIPTION:

# CENTER STREET MIXED-USE

FOR

**TOWN OF LA CONNER  
APPROVED FOR  
CONSTRUCTION - 06/18/2018**

REVIEW- ALT  
BACK CHECK- ALT  
FINAL - BL  
TRAFFIC - SEPARATE



SCALE: **AS NOTED**

DRAWN BY: **C.SEVERIN**

DESIGNED BY: **P.SEVERIN**

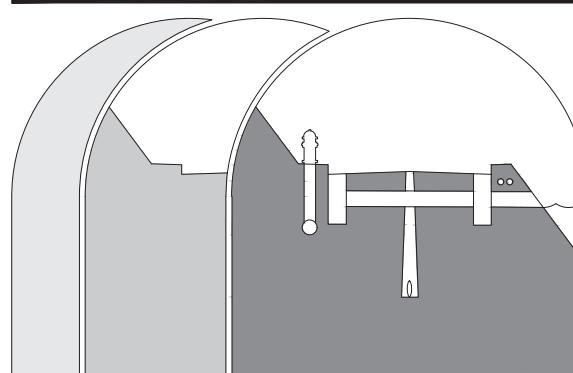
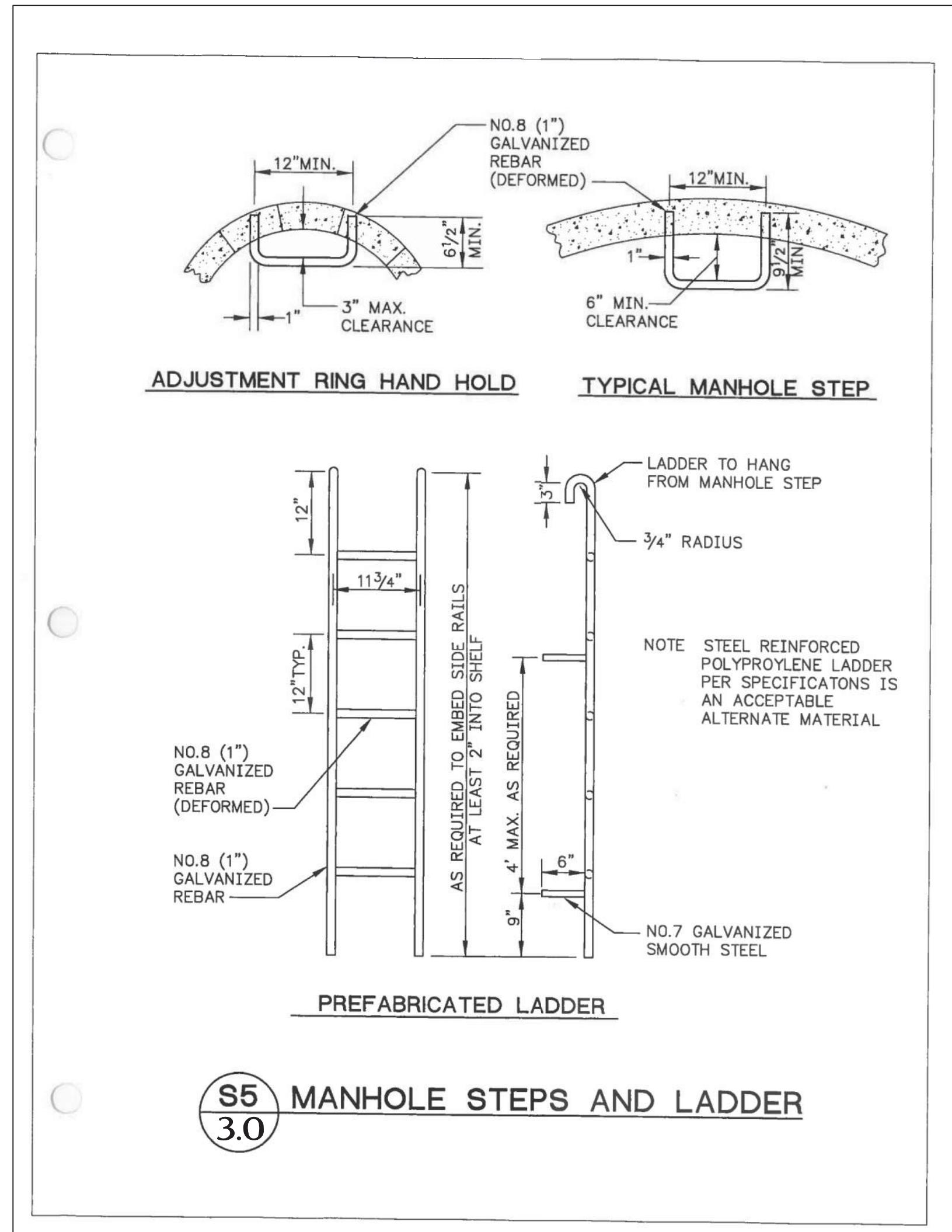
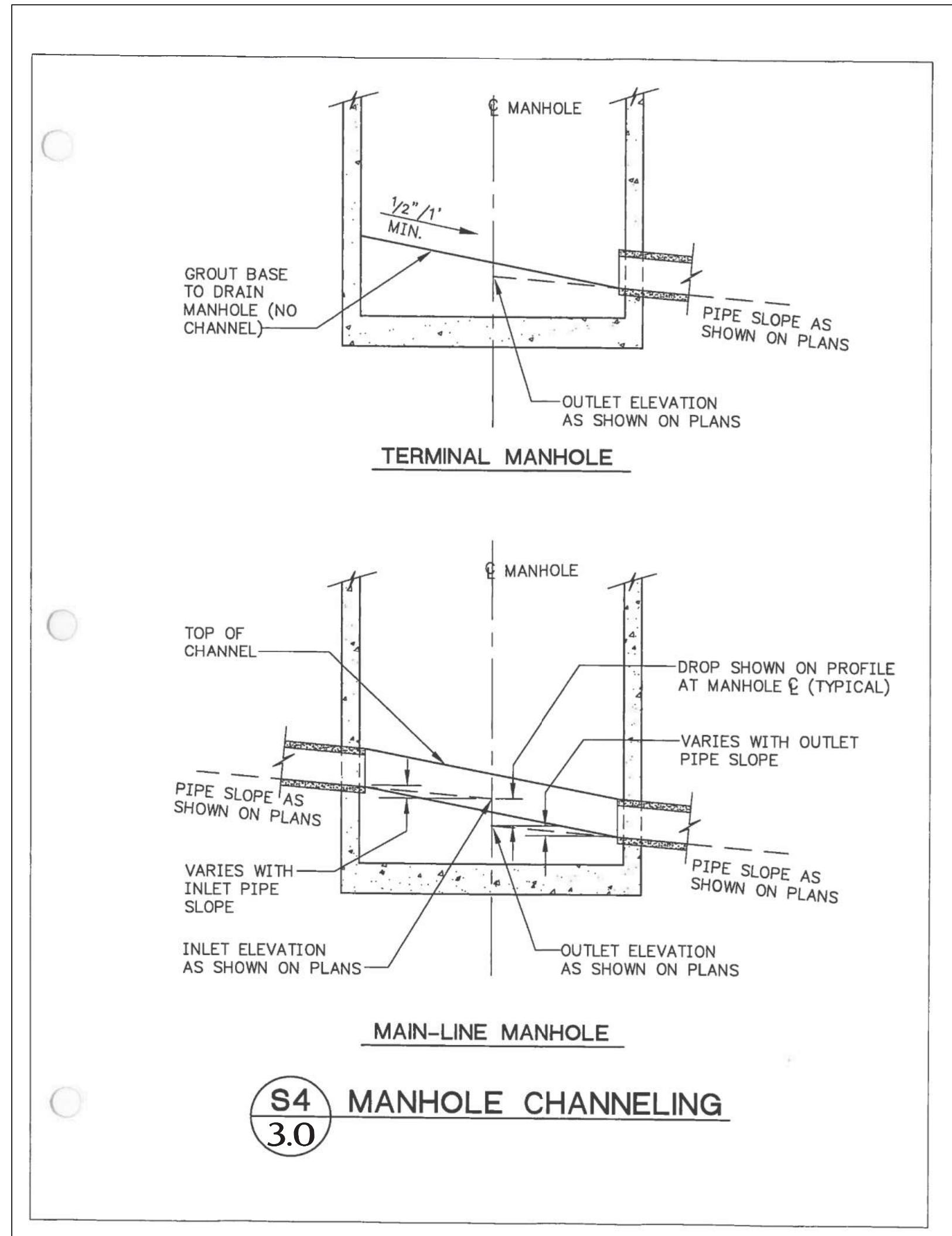
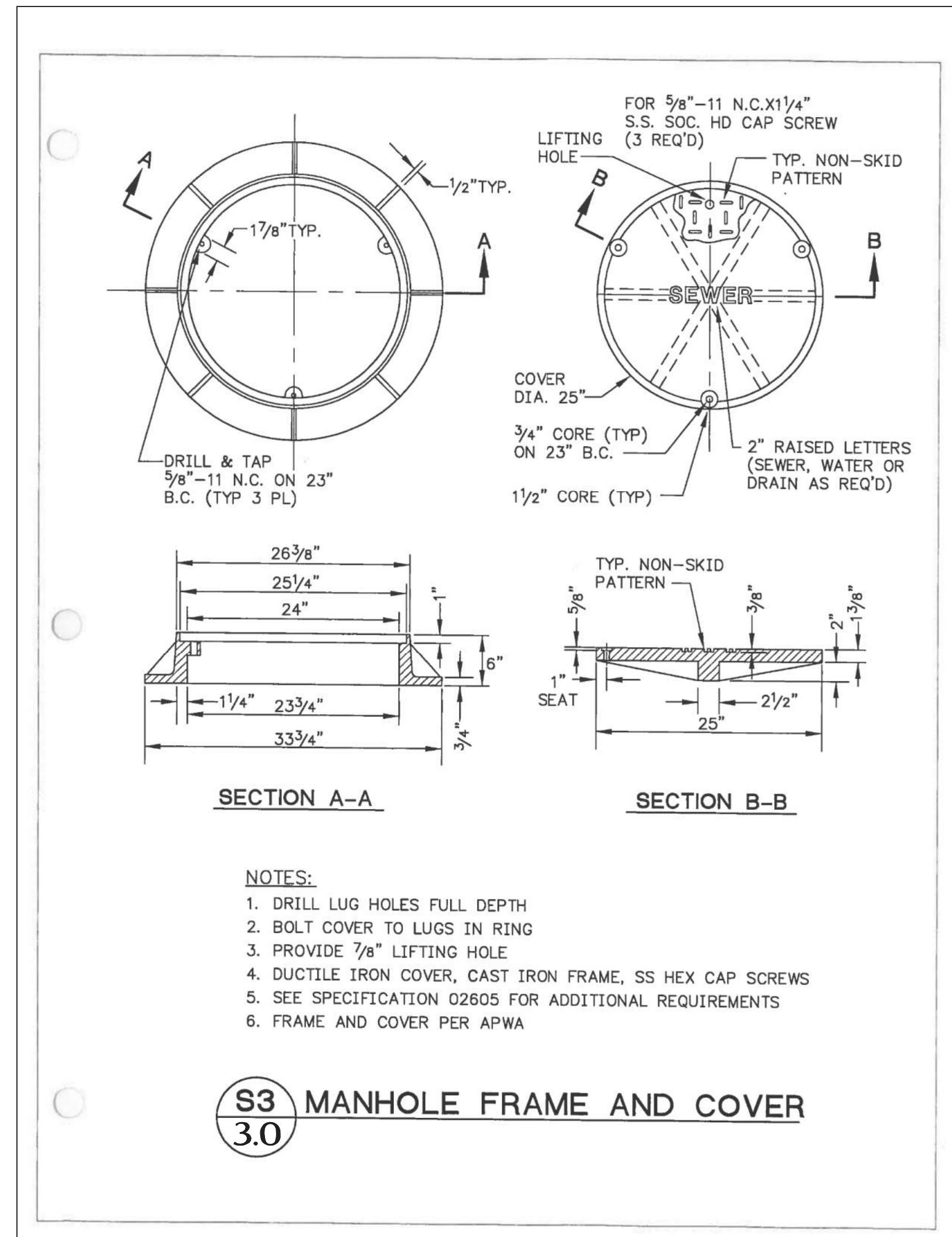
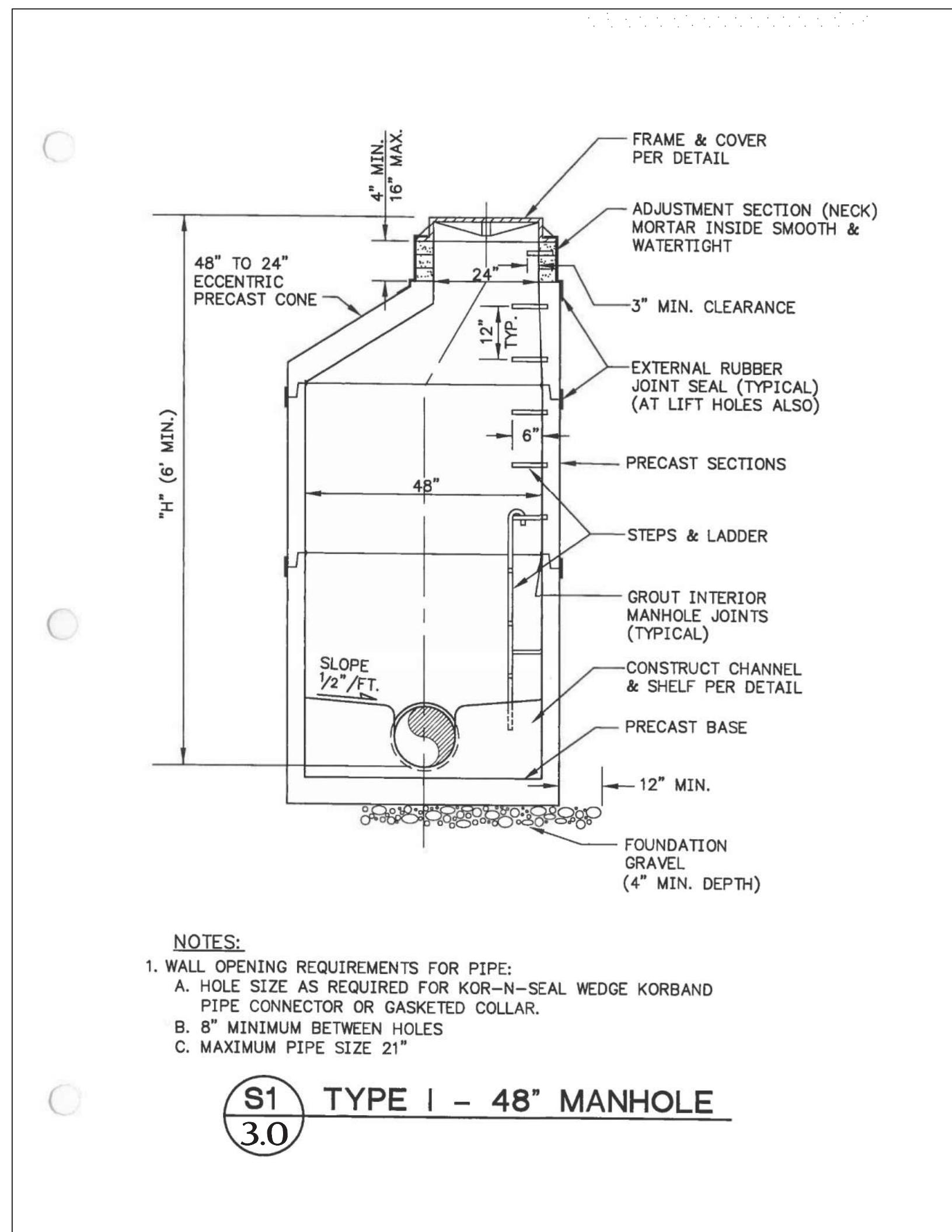
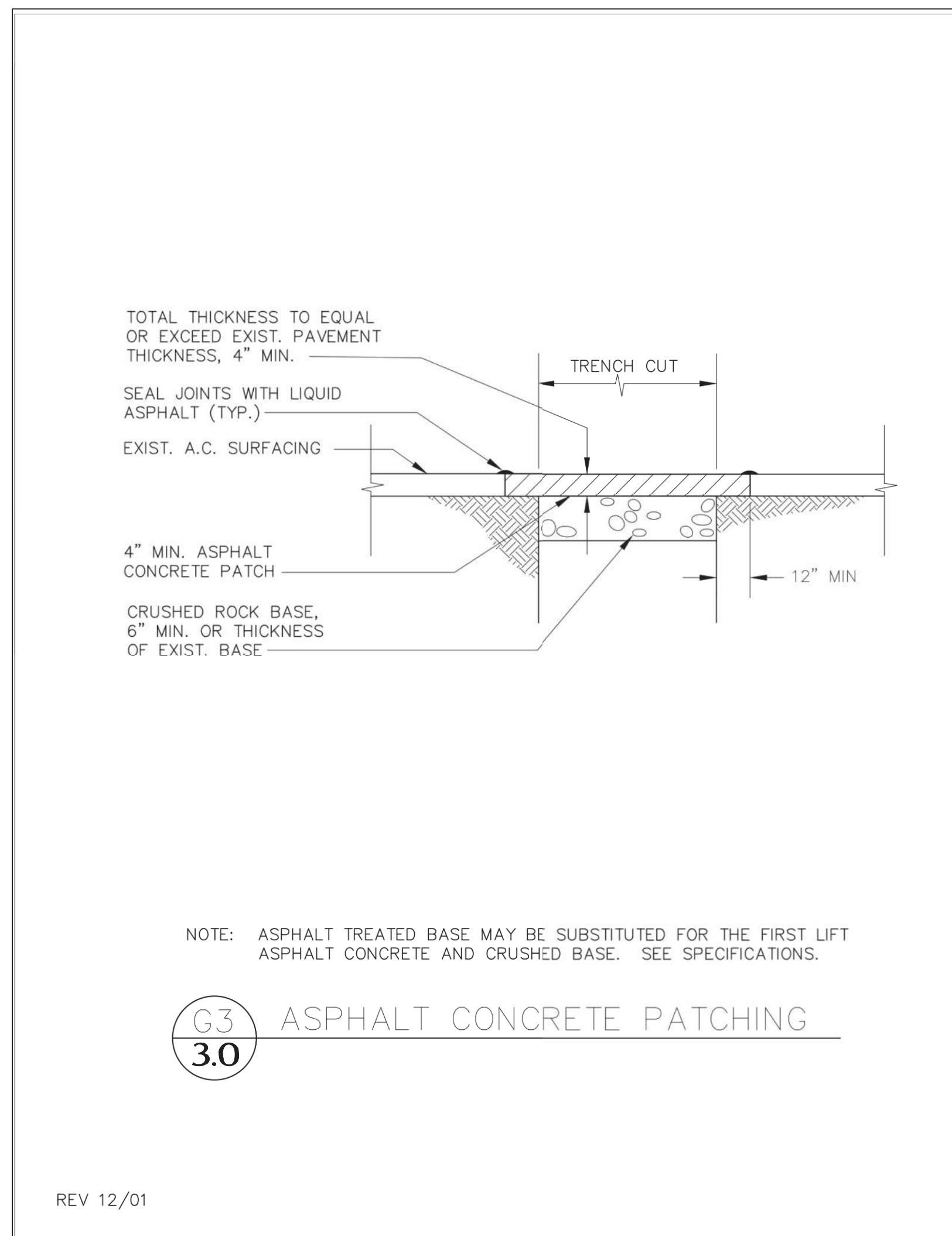
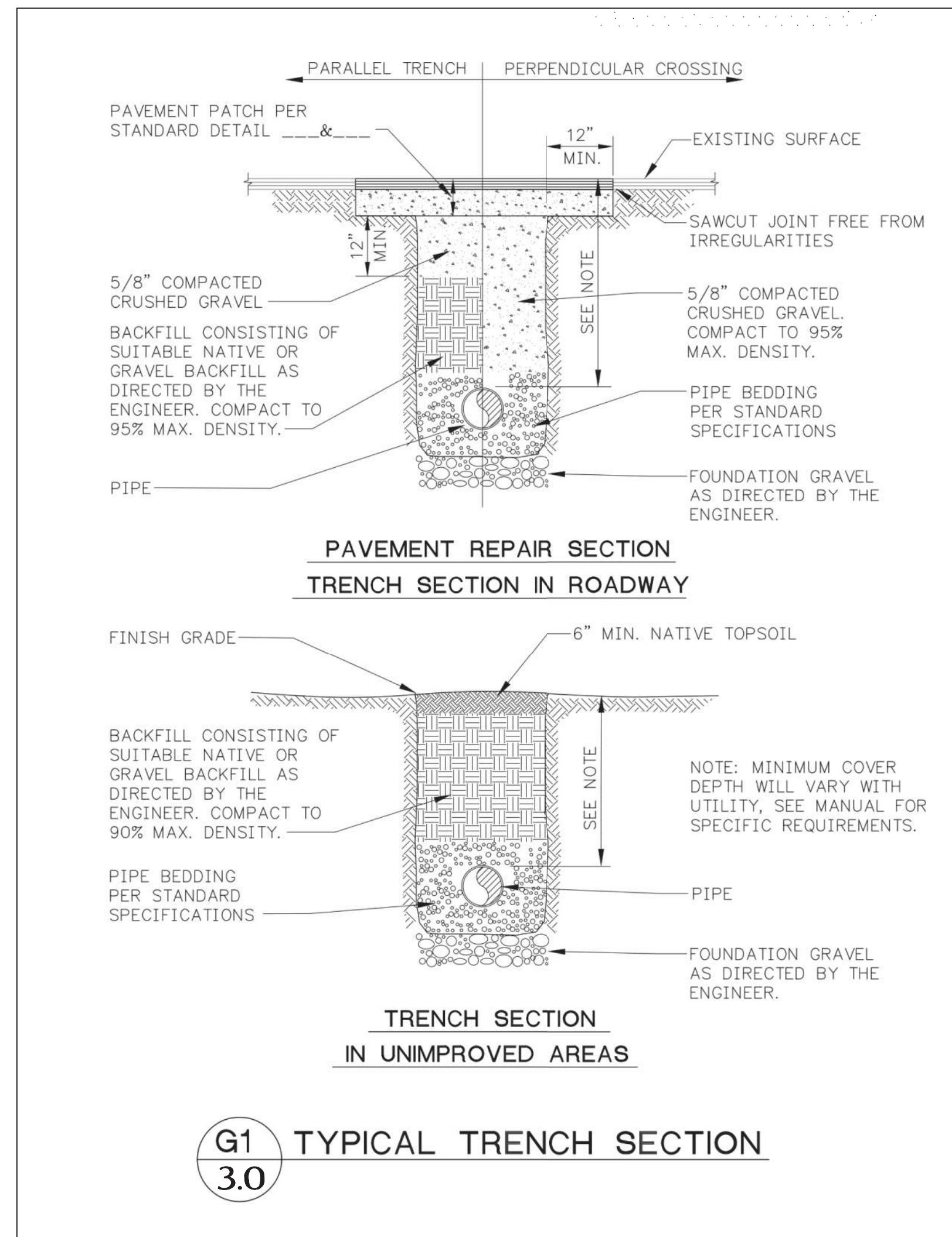
DATE: **02.22.2024**

JOB NUMBER: **21098**

DWG NAME: **21098PLN.DWG**

SHEET NUMBER:

## C2.3



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Mount Vernon, WA 98273 Tel: 360-404-2010

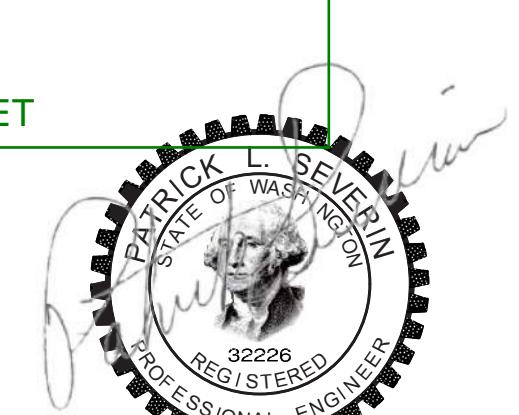
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2 11.27.23 DEA REVISION PLS  
3 02.29.24 ARCH REVISION PLS  
4 05.13.24 DEA/CITY REVISION PLS

**CALL 48 HOURS  
BEFORE YOU DIG**  
**1.800.424.5555**

**PROJECT:**  
**CENTER STREET**  
**MIXED-USE**  
FOR  
**KSA INVESTMENTS, LLC**

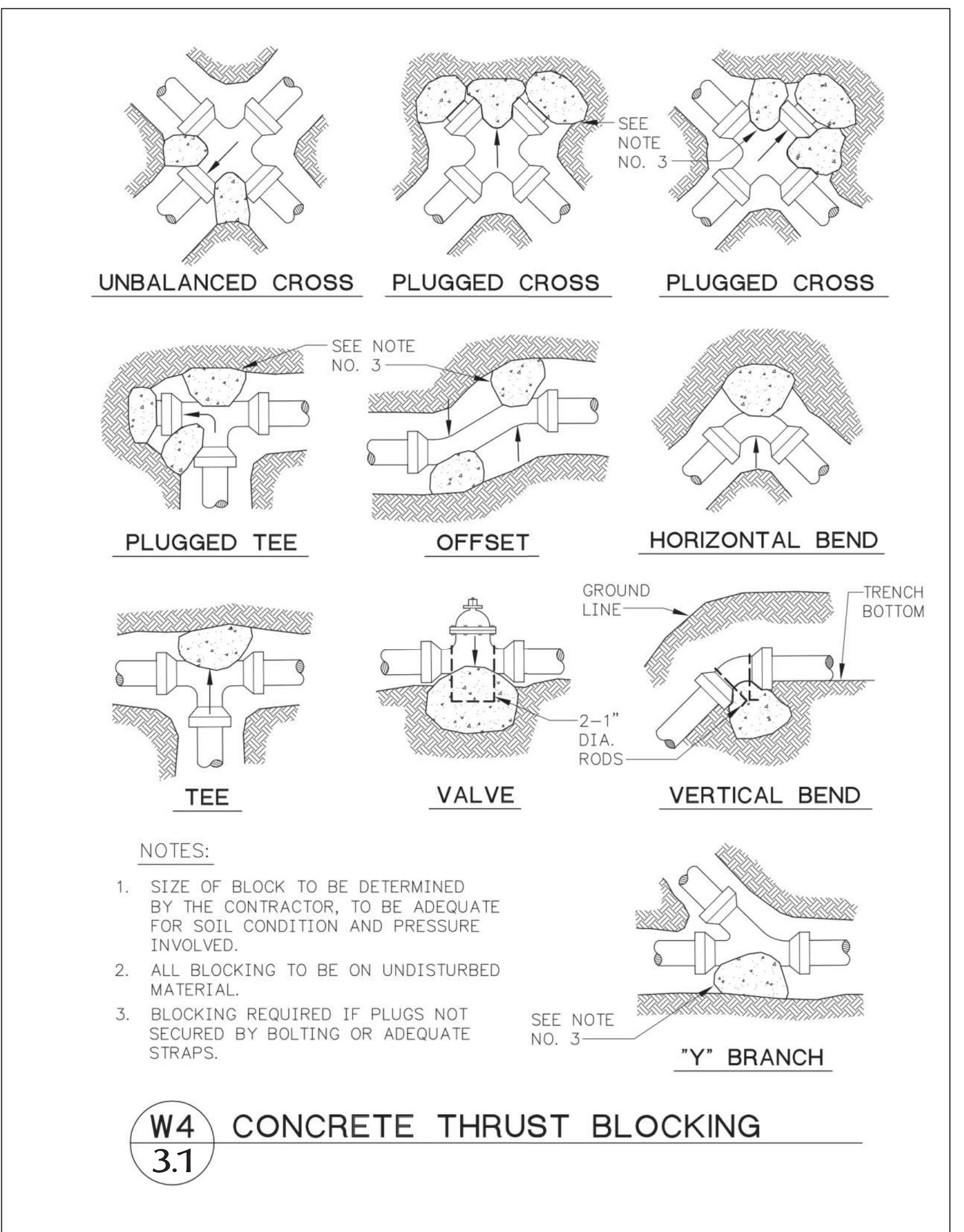
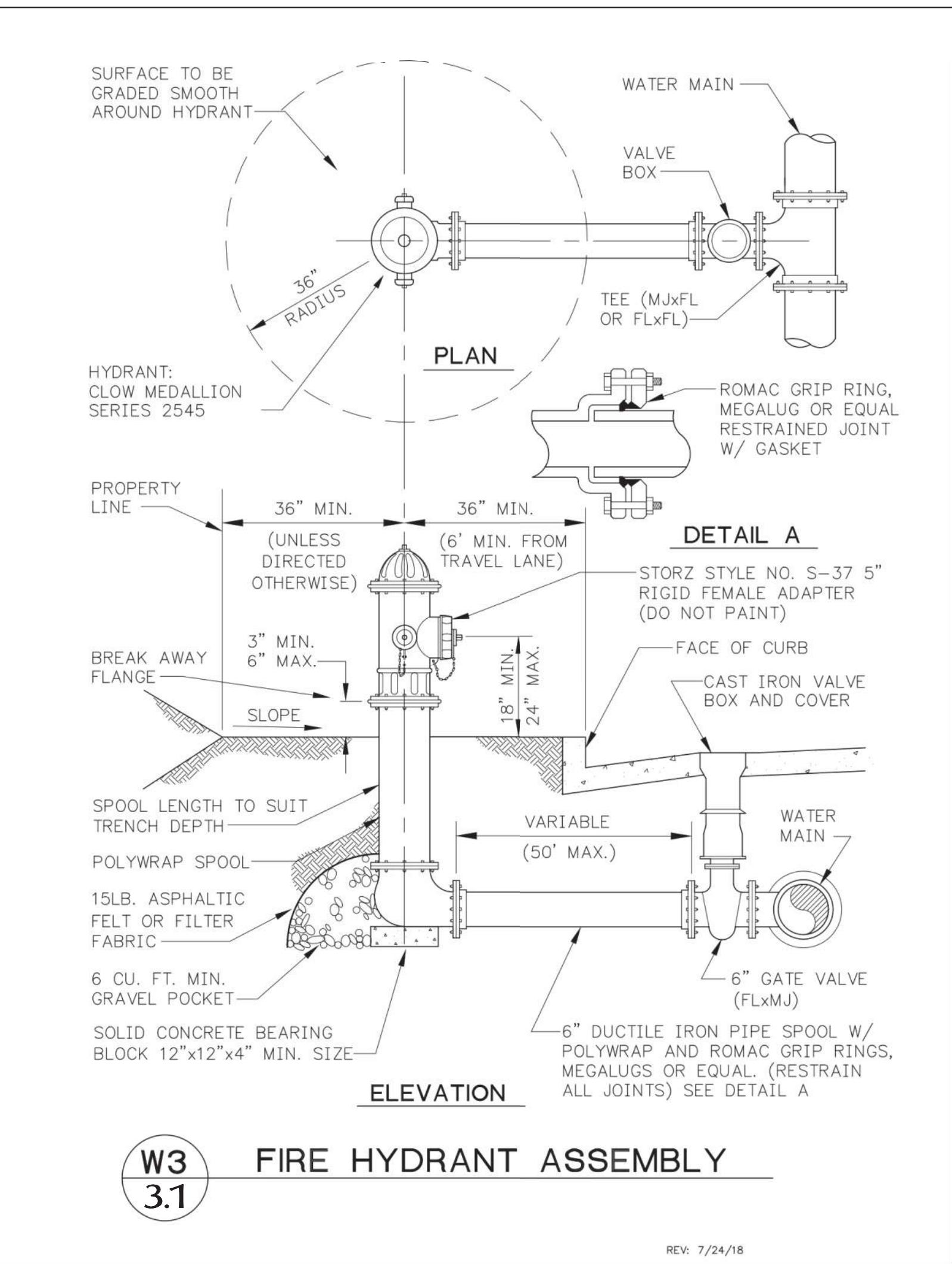
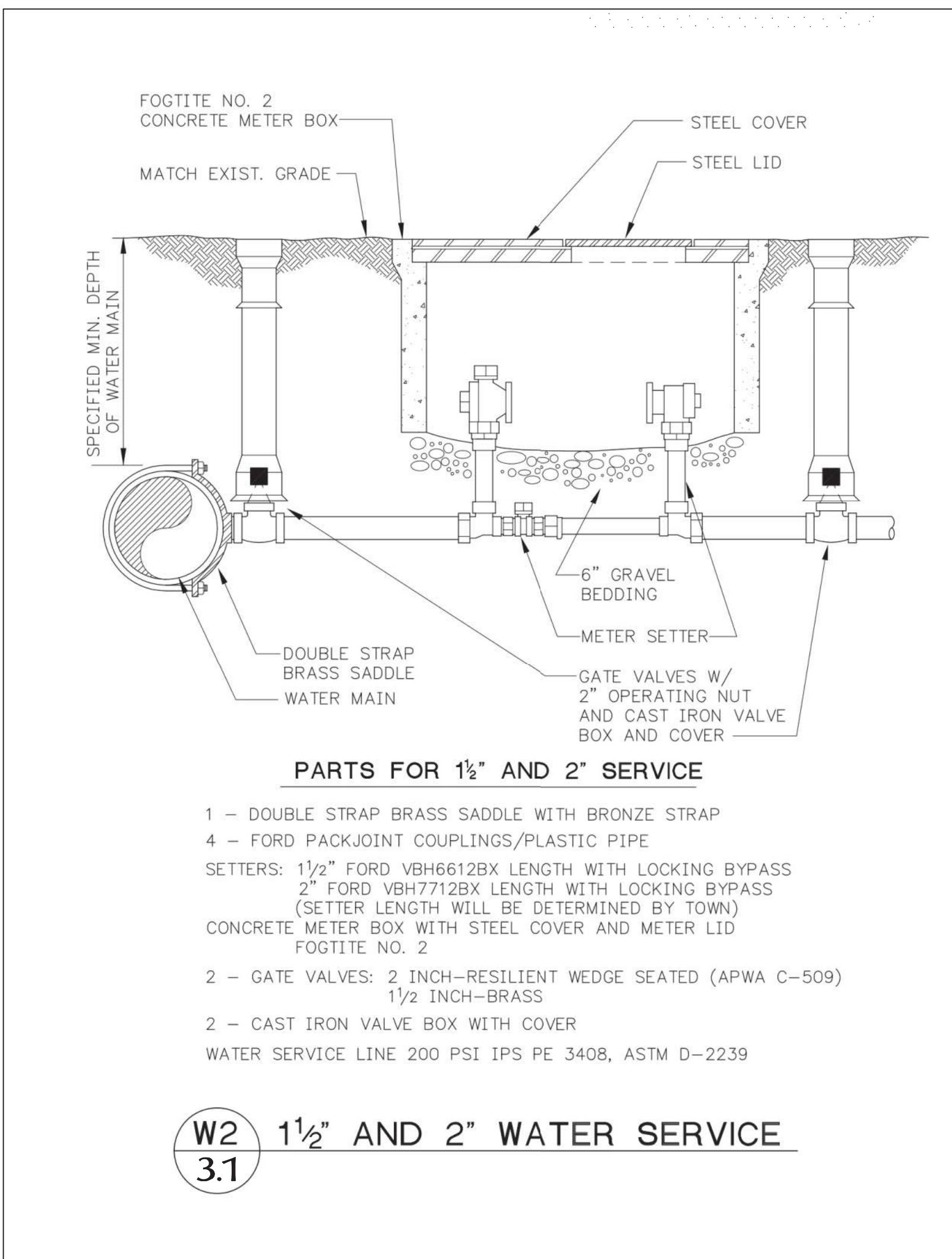
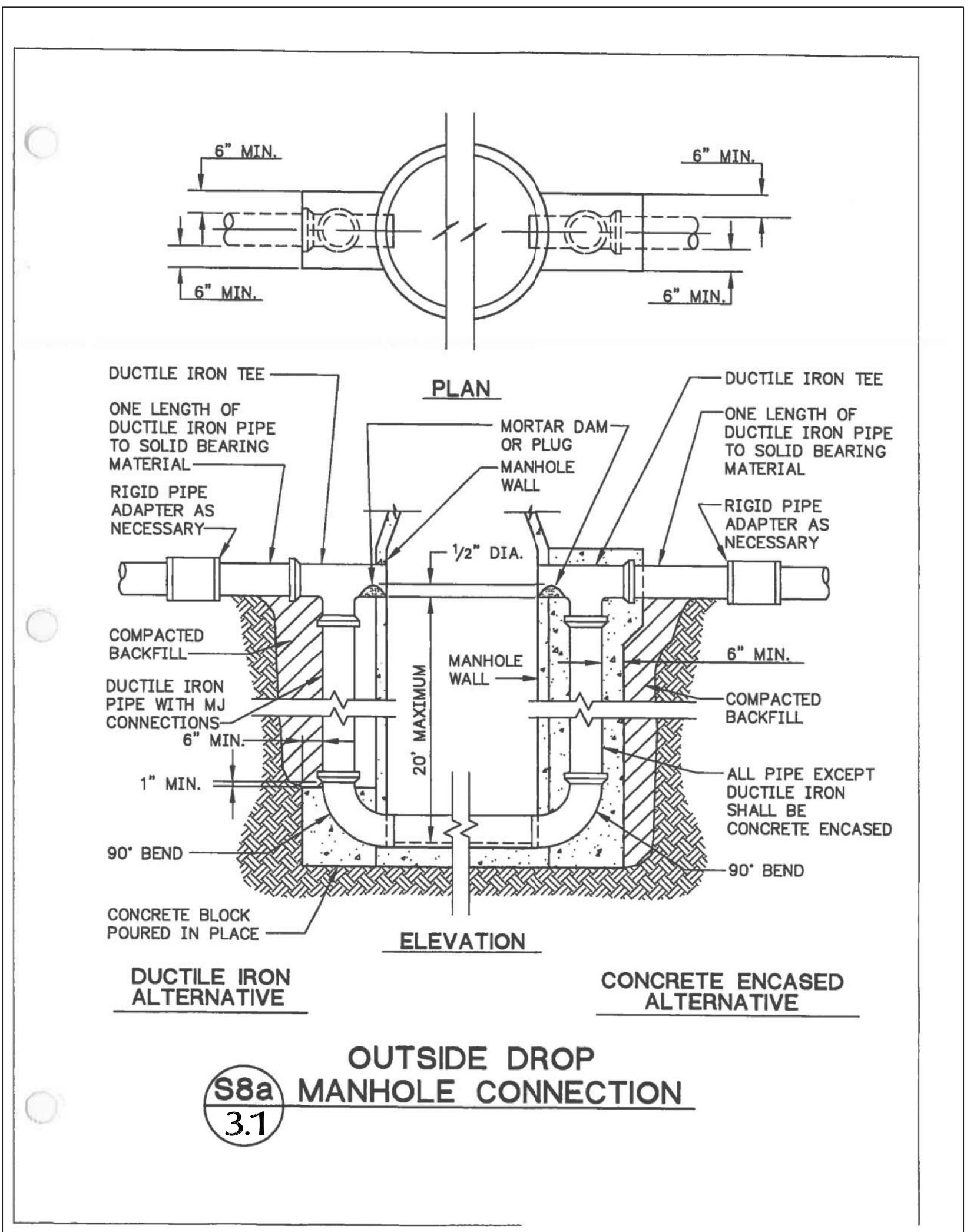
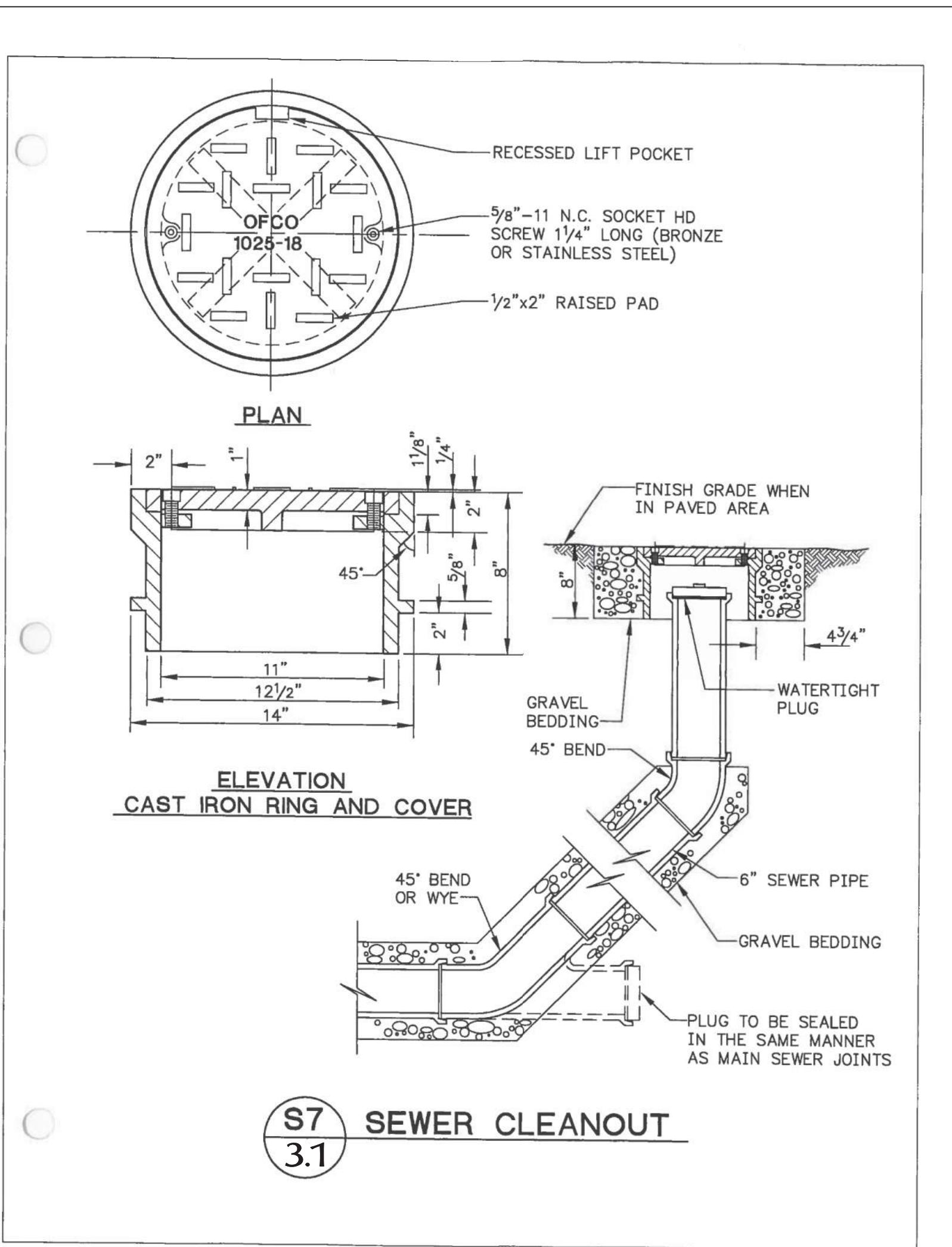
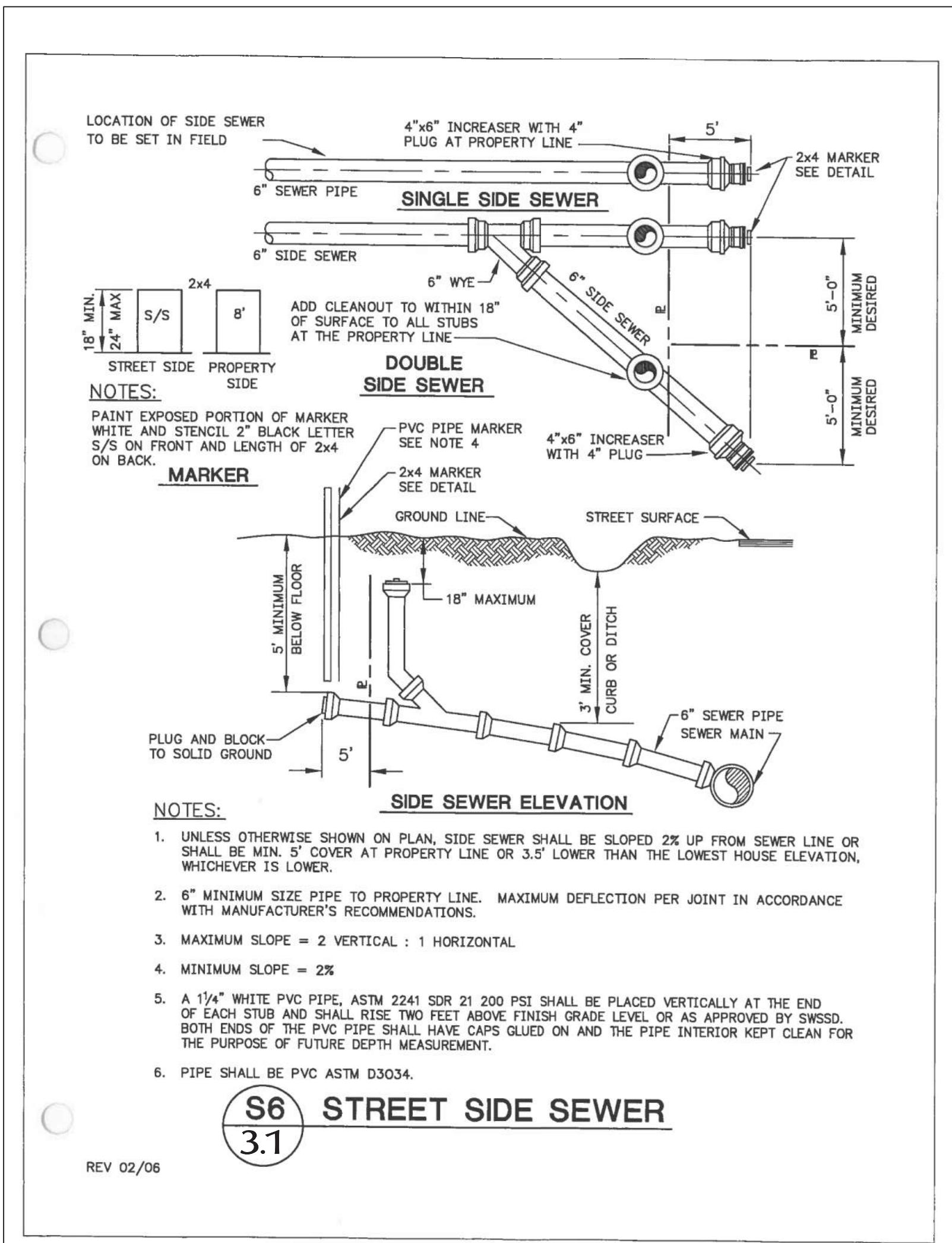
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**TOWN OF LA CONNER**  
APPROVED FOR CONSTRUCTION - 06/18/24

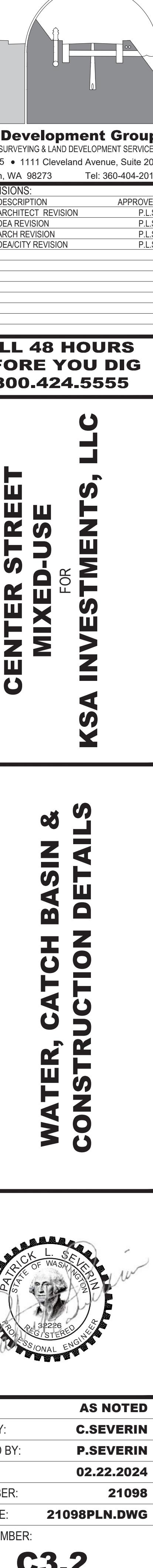
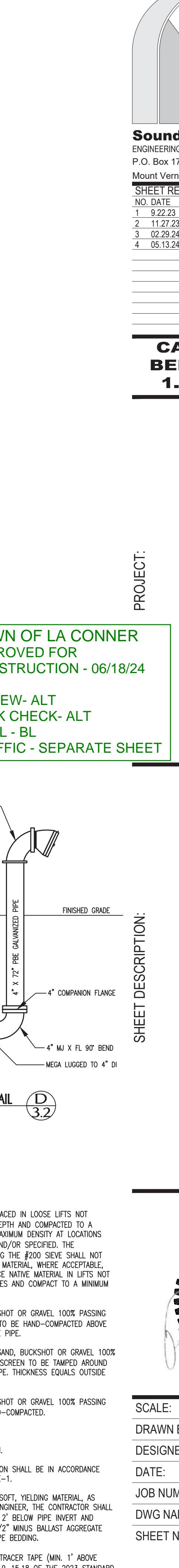
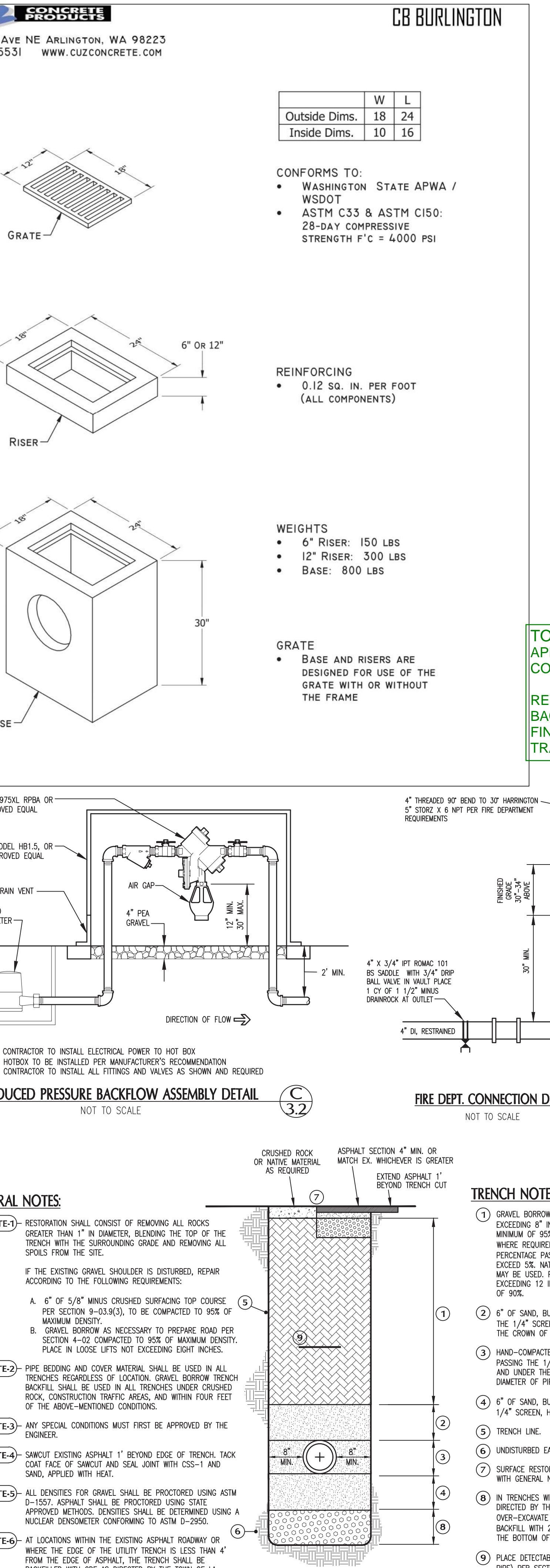
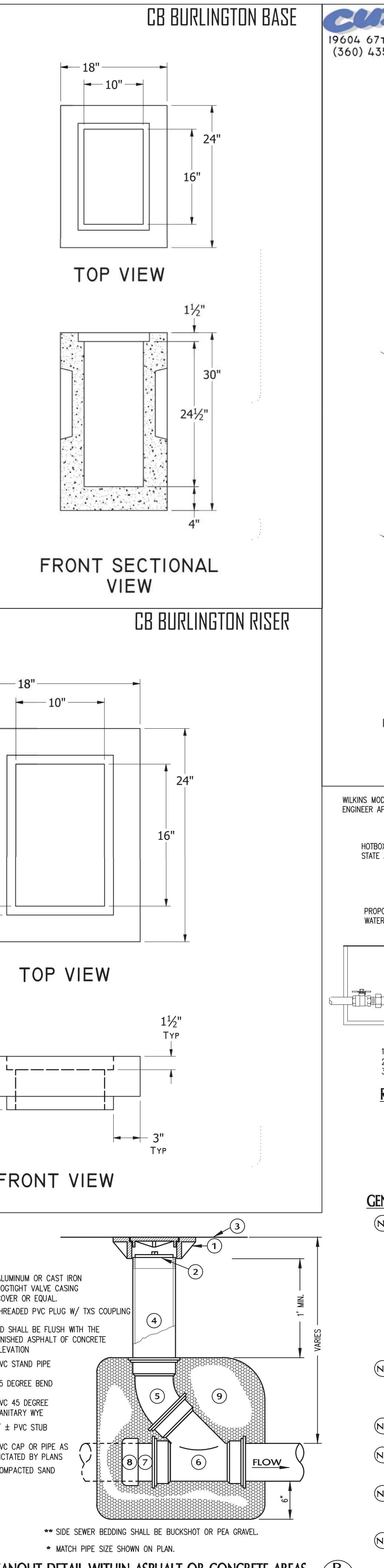
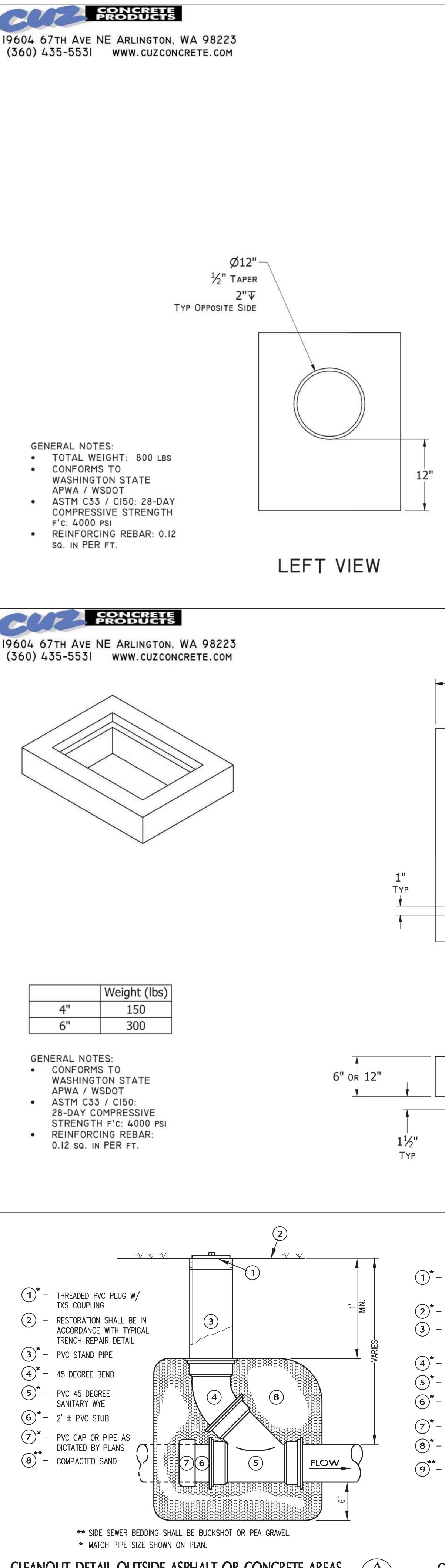
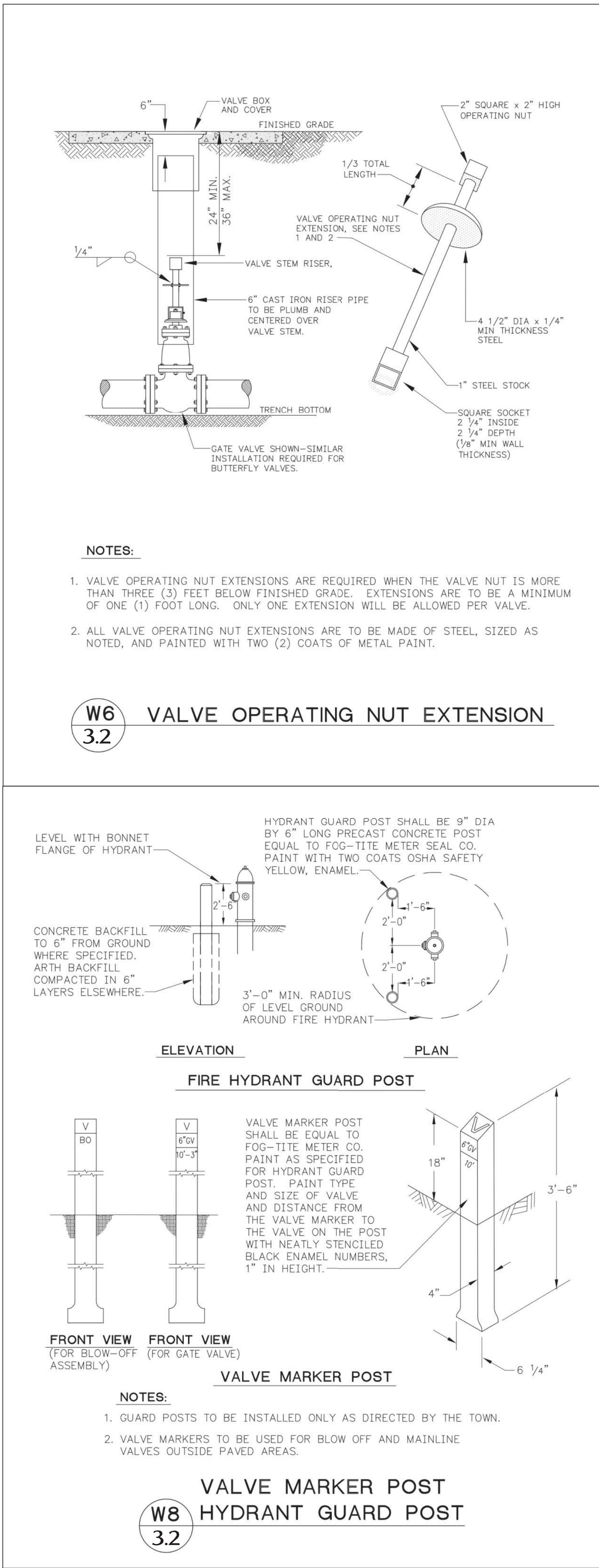
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TRAFFIC - SEPARATE SHEET



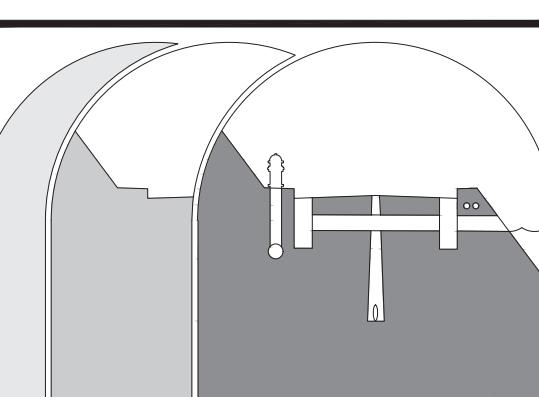
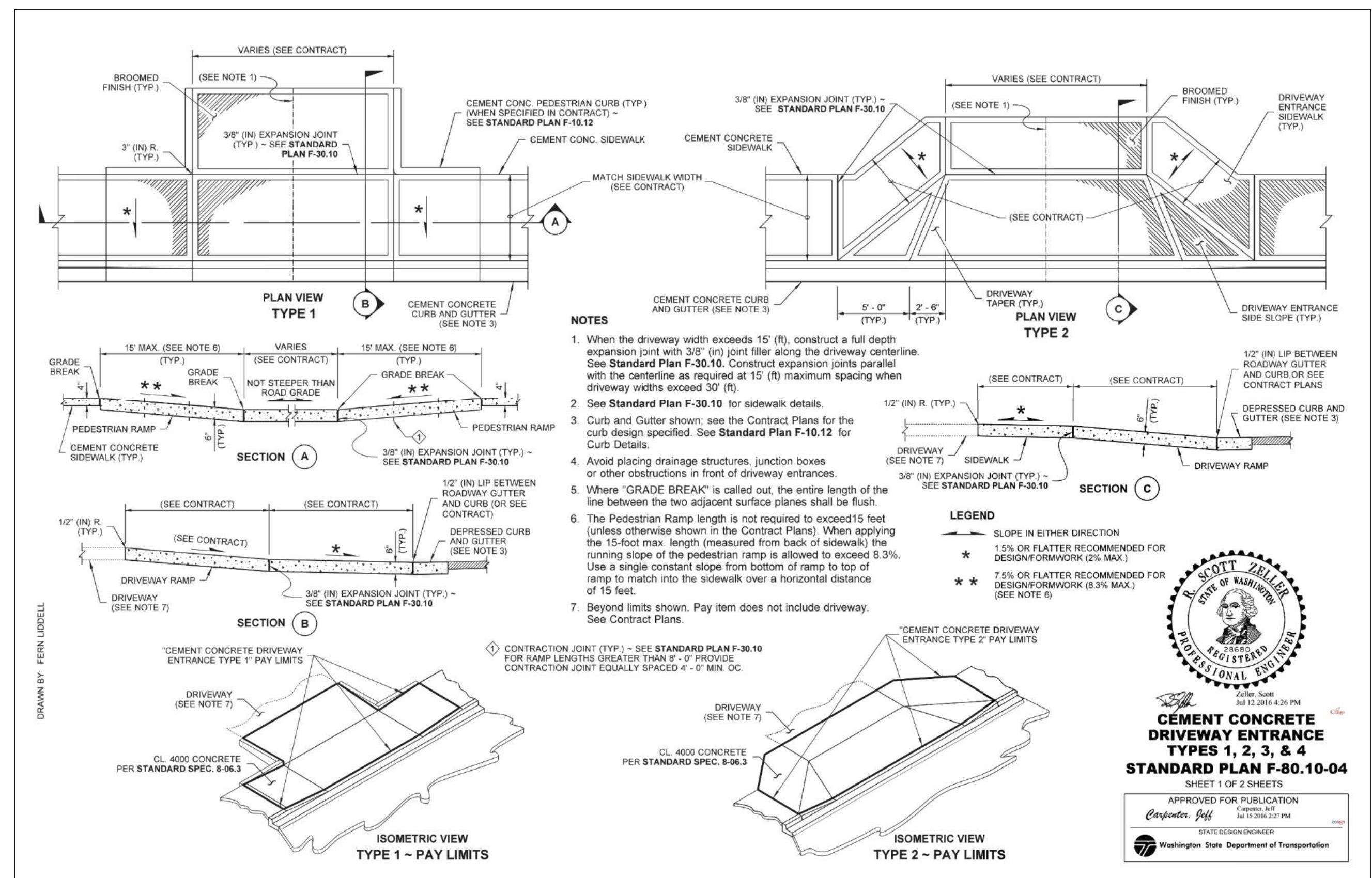
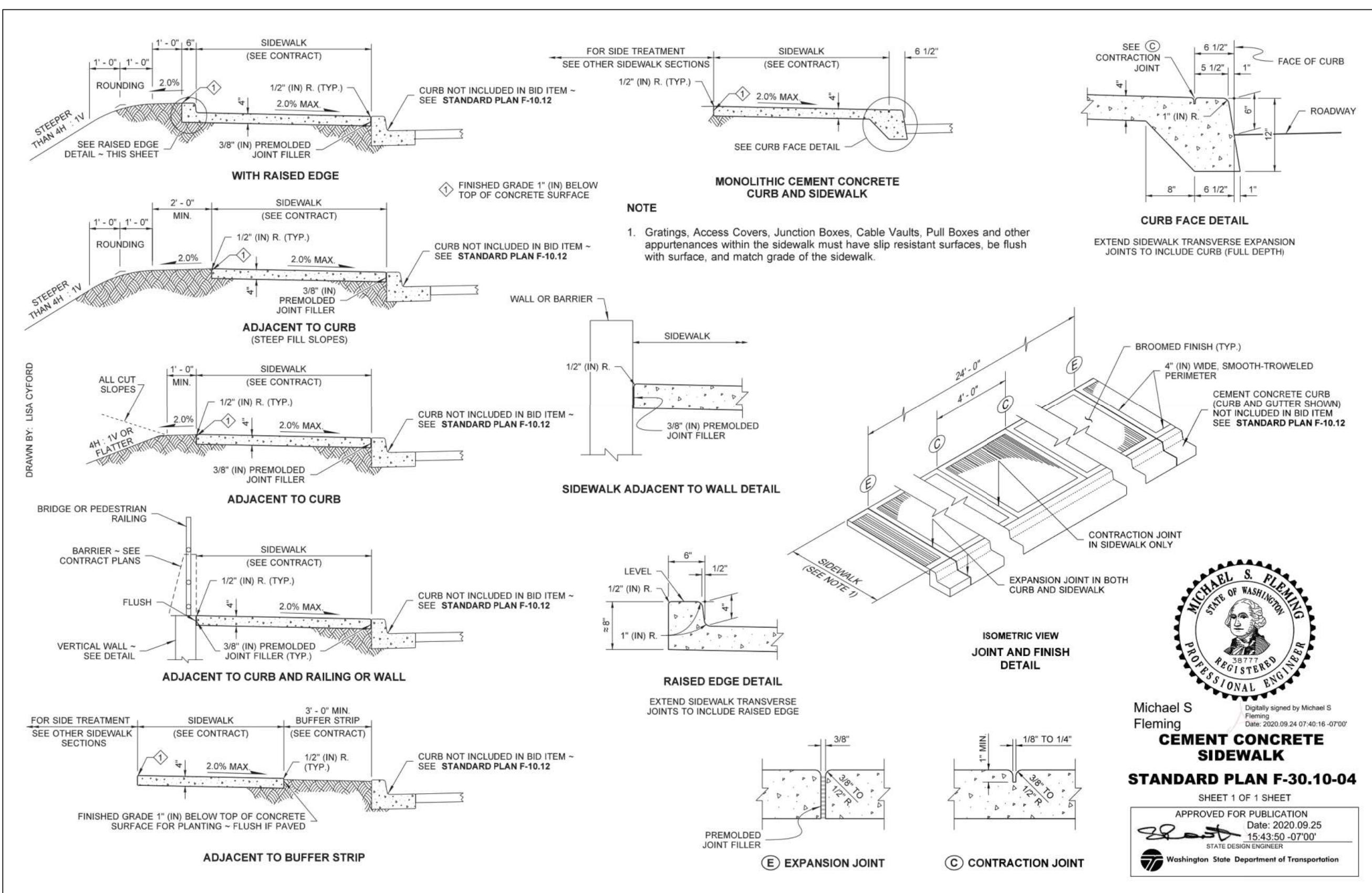
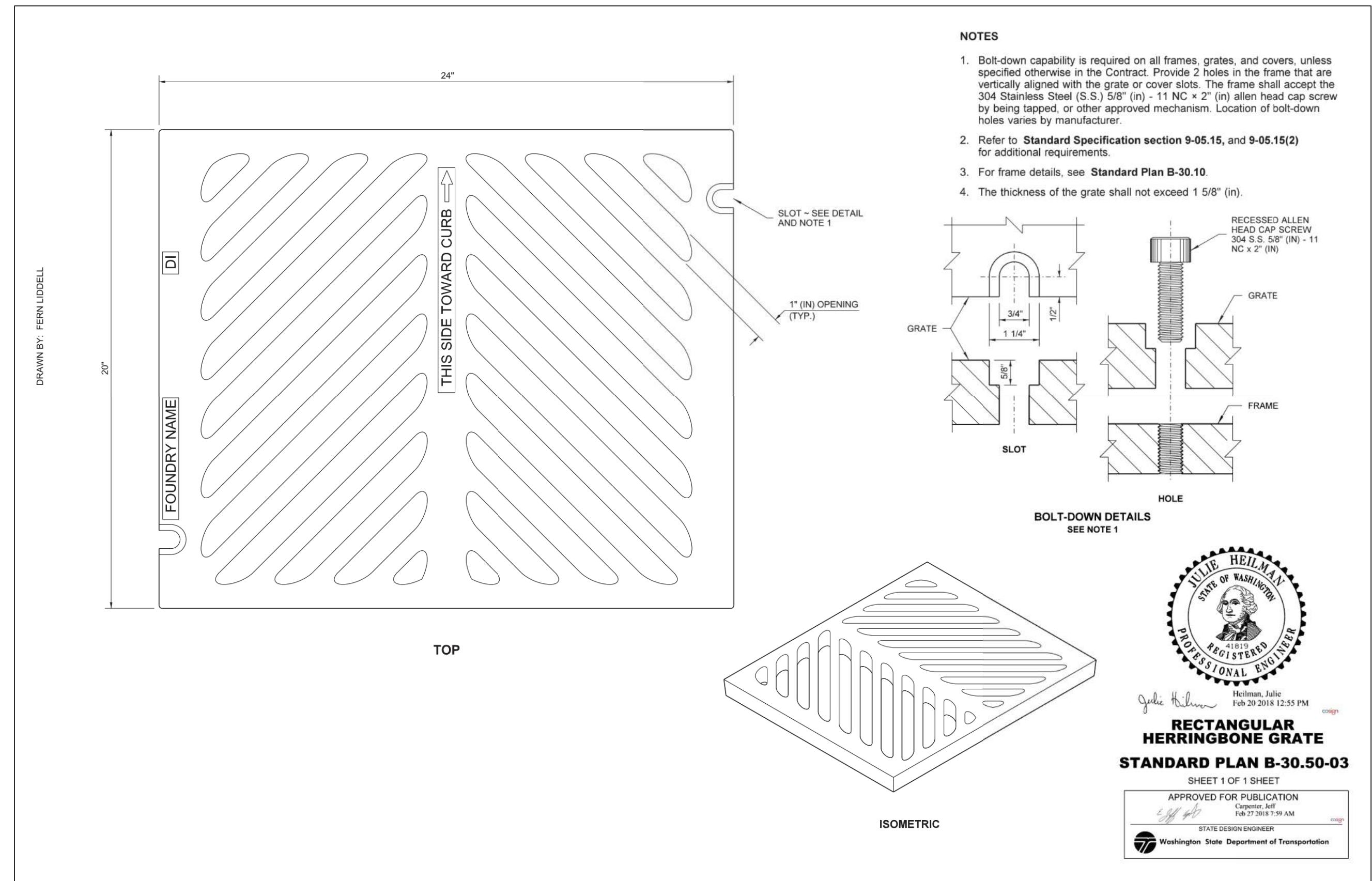
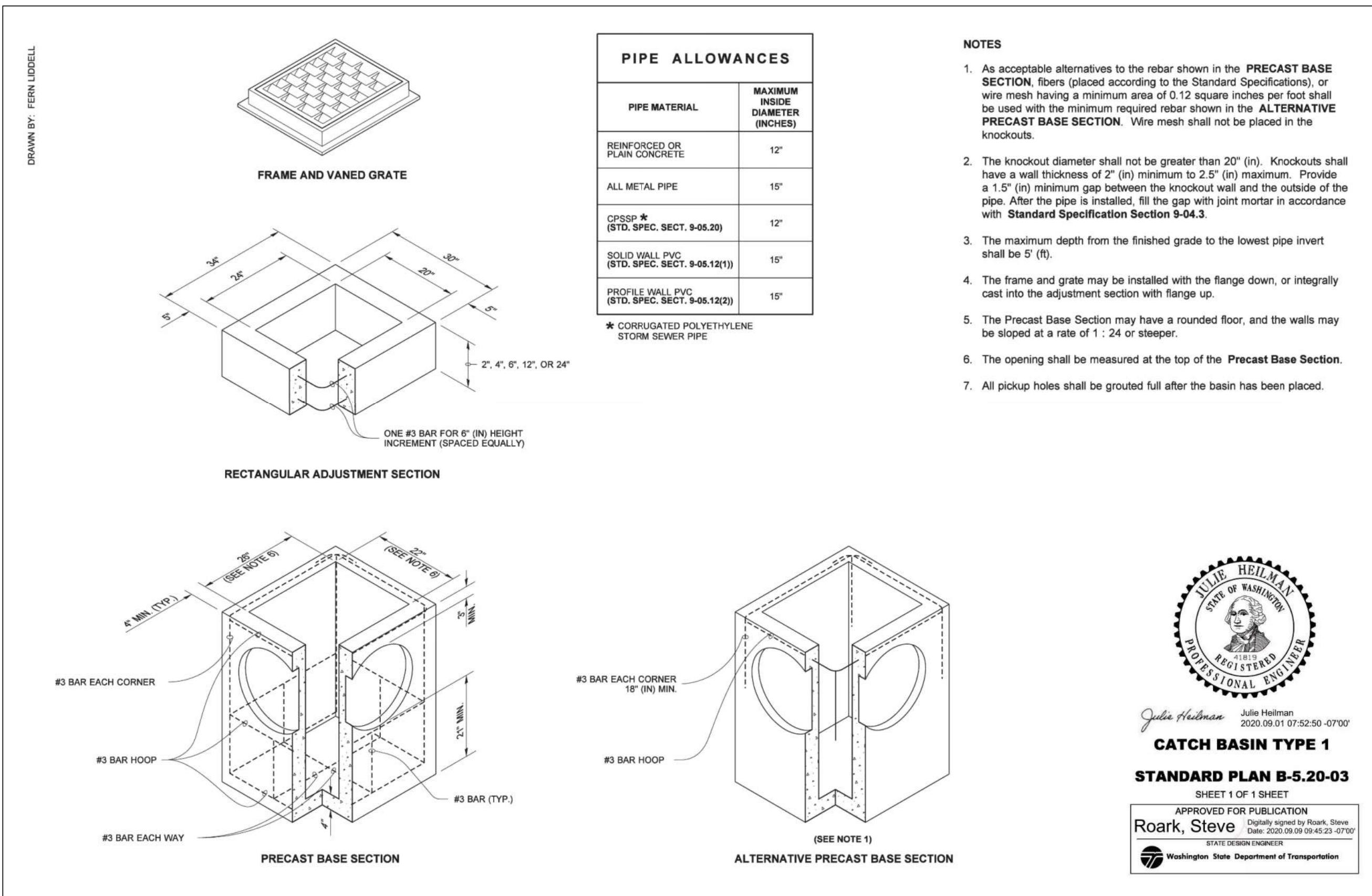
SCALE: **AS NOTED**  
DRAWN BY: **C.SEVERIN**  
DESIGNED BY: **P.SEVERIN**  
DATE: **02.22.2024**  
JOB NUMBER: **21098**  
DWG NAME: **21098PLN.DWG**  
SHEET NUMBER:

**C3.0**





SCALE: **AS NOTED**  
 DRAWN BY: **C.SEVERIN**  
 DESIGNED BY: **P.SEVERIN**  
 DATE: **02.22.2024**  
 JOB NUMBER: **21098**  
 DWG NAME: **21098PLN.DWG**  
 SHEET NUMBER: **C3.2**



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NO.	DATE	DESCRIPTION	APPROVED
1	02/23	ARCHITECT REVISION	P.L.S.
2	11-27-23	DEA REVISION	P.L.S.
3	02/29/24	ARCH REVISION	P.L.S.
4	05.13.24	DEA/CITY REVISION	P.L.S.

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**CENTER STREET  
FOR  
MIXED-USE**  
**KSA INVESTMENTS, LLC**

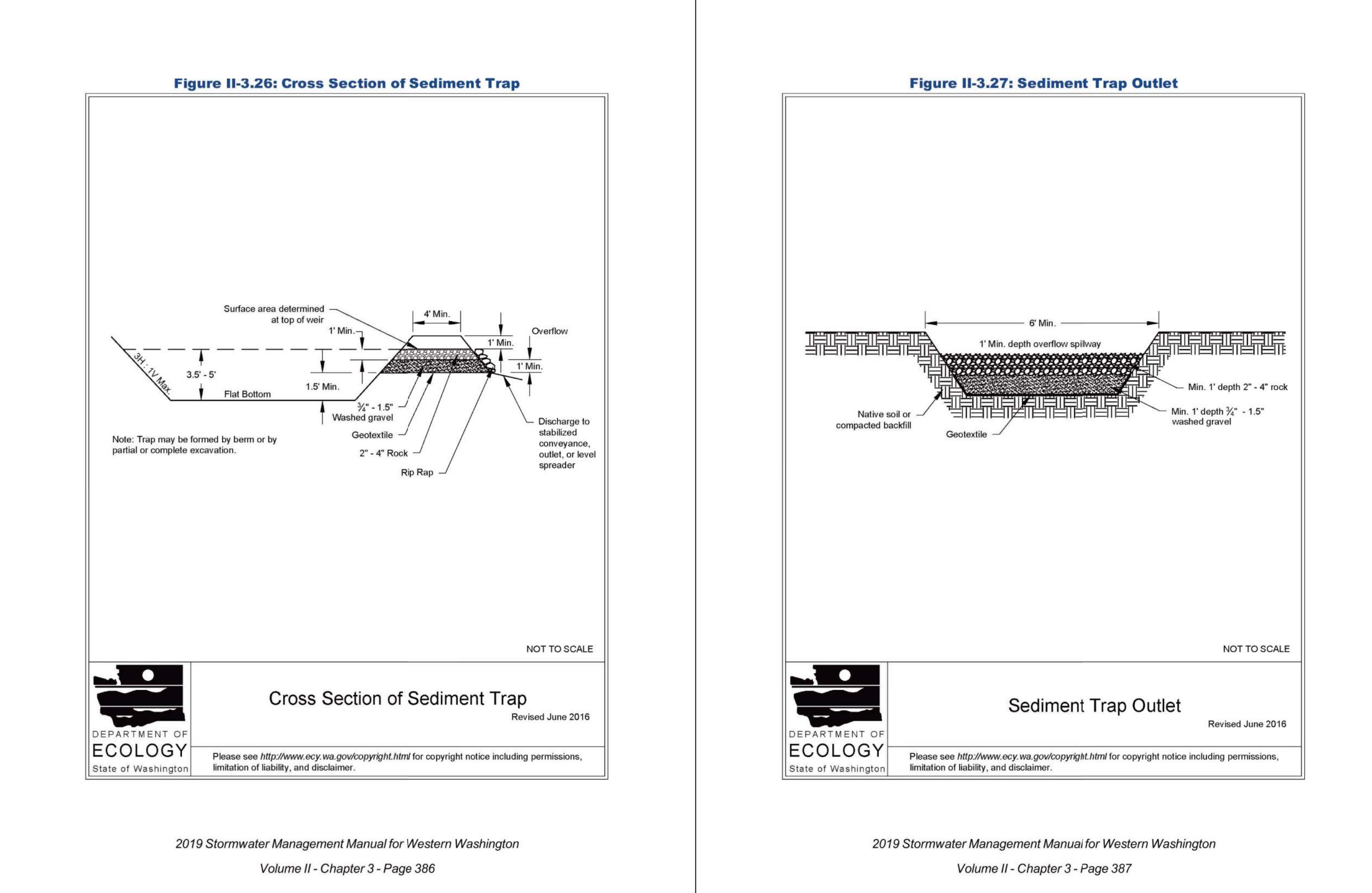
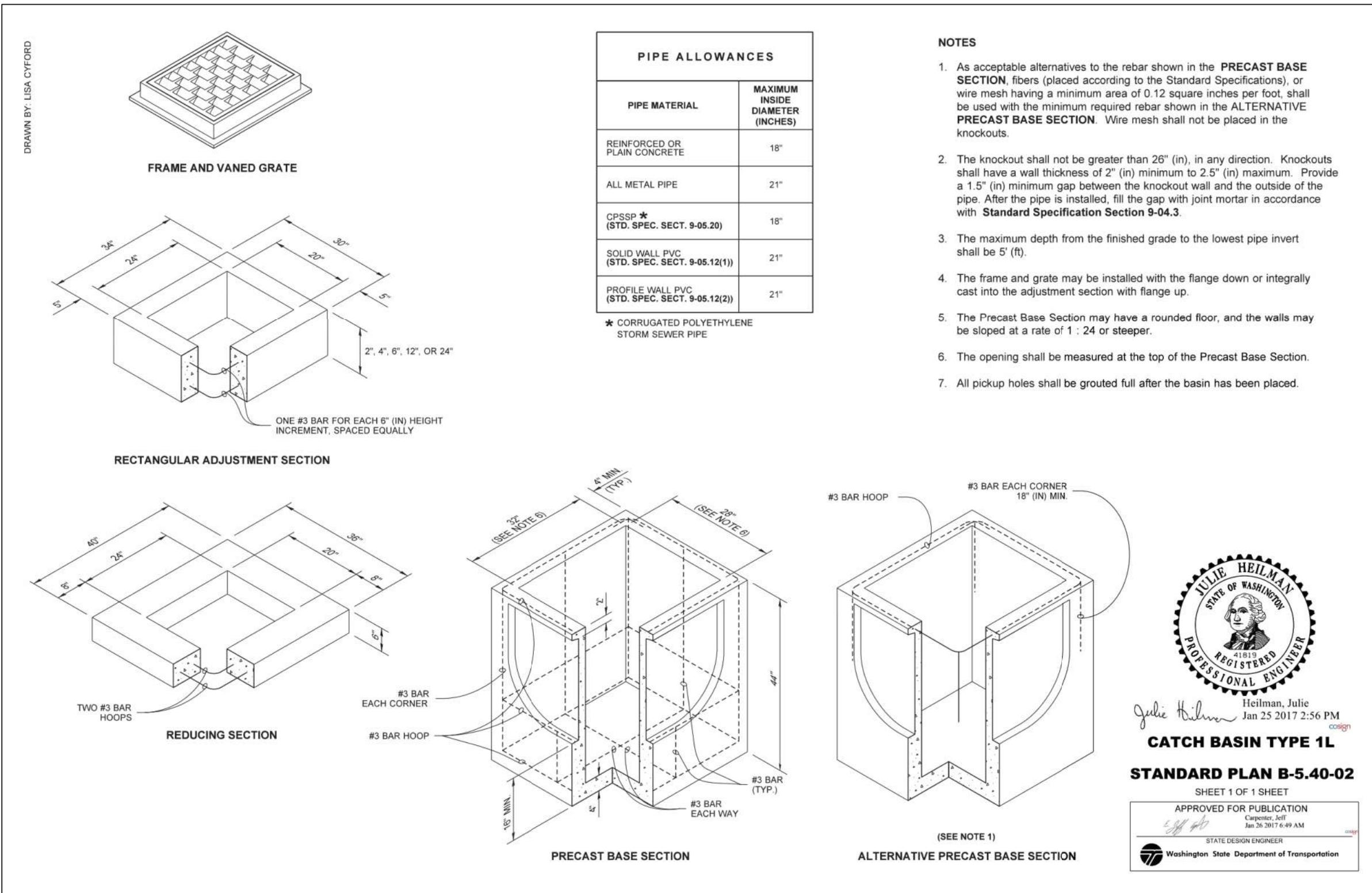
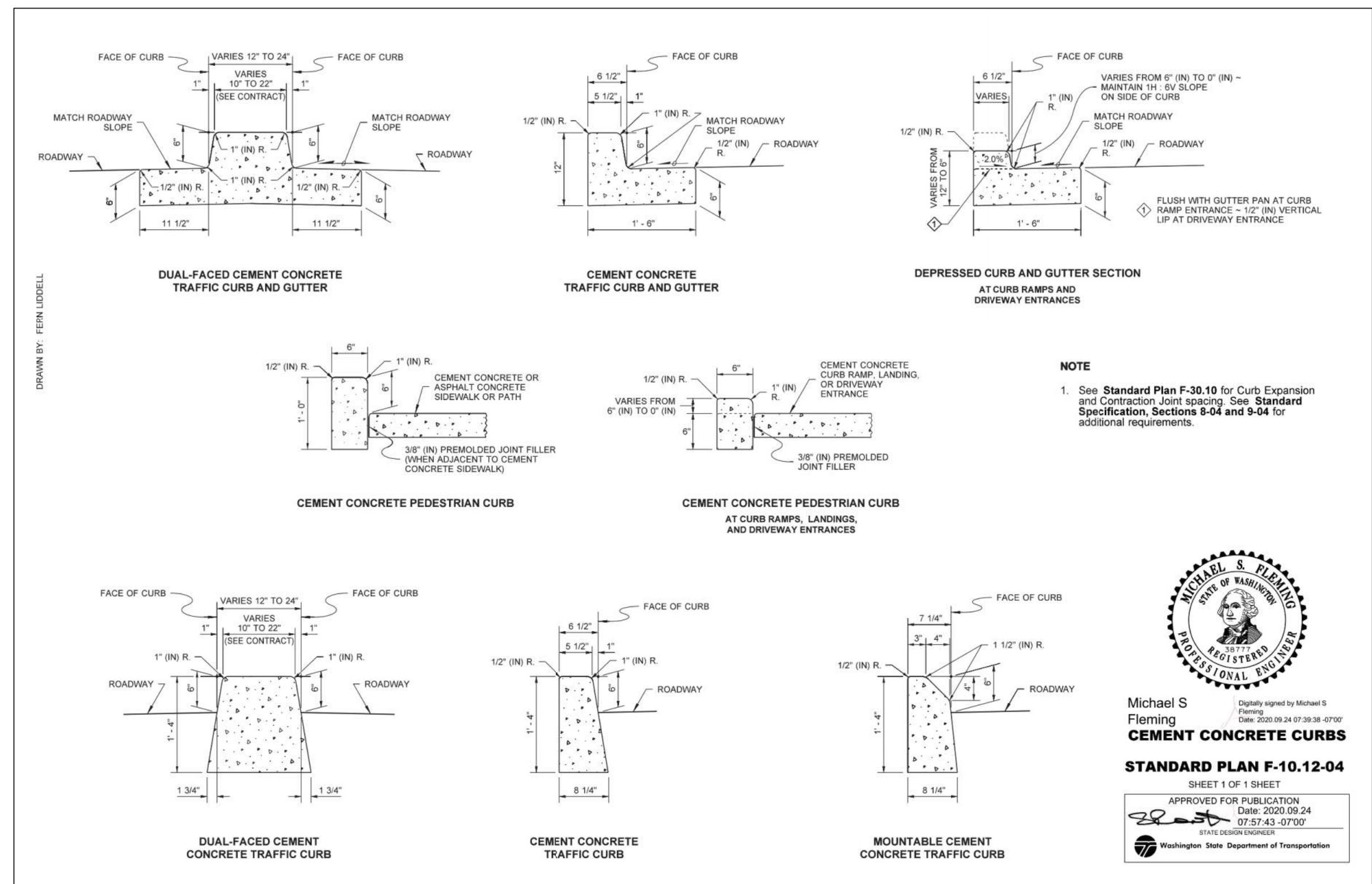
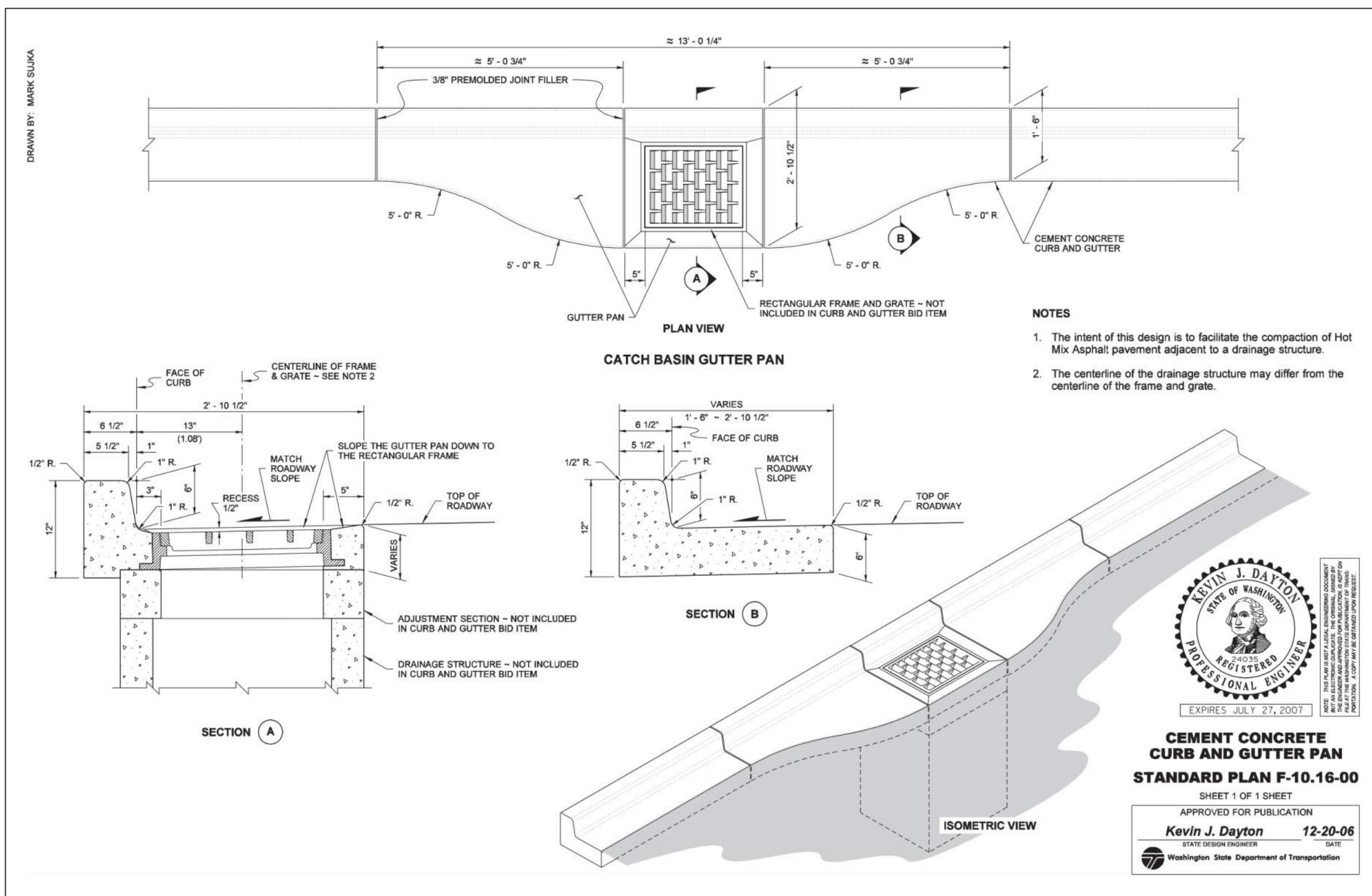
**TOWN OF LA CONNER  
APPROVED FOR  
CONSTRUCTION - 06/18/24**

**REVIEW- ALT  
BACK CHECK- ALT  
FINAL - BL  
TRAFFIC - SEPARATE  
SHEET**

**SHEET DESCRIPTION:**

**SCALE: AS NOTED**  
**DRAWN BY: C.SEVERIN**  
**DESIGNED BY: P.SEVERIN**  
**DATE: 02.22.24**  
**JOB NUMBER: 21098**  
**DWG NAME: 21098PLN.DWG**  
**SHEET NUMBER:**

**C3.3**



2019 Stormwater Management Manual for Western Washington  
Volume II - Chapter 3 - Page 386

2019 Stormwater Management Manual for Western Washington  
Volume II - Chapter 3 - Page 387

## WSDOT STANDARD PLANS & SEDIMENT TRAP DETAIL

PROJECT:



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CENTER STREET  
FOR  
MIXED-USE  
KSA INVESTMENTS, LLC

PROJECT:

SCALE: AS NOTED  
DRAWN BY: C.SEVERIN  
DESIGNED BY: P.SEVERIN  
DATE: 02.22.2024  
JOB NUMBER: 21098  
DWG NAME: 21098PLN.DWG  
SHEET NUMBER: C3.4

TOWN OF LA CONNER  
APPROVED FOR CONSTRUCTION - 06/18/24  
REVIEW- ALT  
BACK CHECK- ALT  
FINAL - BL  
TRAFFIC - SEPARATE SHEET

IN A PORTION OF THE NE QUARTER OF SECTION 36, TOWNSHIP 34 N, RANGE 02 E, W.M. LA CONNER, WASHINGTON

GENERAL CONSTRUCTION NOTES

- ALL CONSTRUCTION AND MATERIALS SHALL CONFORM TO THE 2024 STANDARD SPECIFICATIONS FOR ROAD, BRIDGE, AND MUNICIPAL CONSTRUCTION AS PREPARED BY WASHINGTON STATE DEPARTMENT OF TRANSPORTATION AND THE AMERICAN PUBLIC WORKS ASSOCIATION (NSDOT/APA), HEREIN REFERRED TO AS THE "STANDARD SPECIFICATIONS". REFERENCES WILL BE MADE TO THE STANDARD SPECIFICATIONS MANUAL AND THE STANDARD PLANS BOOK.
- EXISTING UTILITIES HAVE BEEN TAKEN FROM AVAILABLE FIELD AND OFFICE RECORDS. THE CONTRACTOR IS RESPONSIBLE FOR UTILIZING THE ONE-CALL UTILITY LOCATE SERVICE, 1-800-244-6555, A MINIMUM OF TWO WORKING DAYS PRIOR TO ANY CONSTRUCTION. DAMAGES TO THE EXISTING UTILITIES RESULTING FROM THIS CONSTRUCTION SHALL BE REPAIRED BY AND AT THE CONTRACTOR'S EXPENSE. THE CONTRACTOR SHALL VERIFY ANY POTENTIAL UTILITY CONFLICTS PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL MAKE DAILY EFFORTS TO KEEP THE SITE IN A NEAT AND ORDERLY CONDITION TO THE SATISFACTION OF THE OWNER, ENGINEER, AND TOWN OF LA CONNER PUBLIC WORKS DEPARTMENT. IF CONSTRUCTION OCCURS DURING RAINY WEATHER CONDITIONS, THEREBY CAUSING DEBRIS TO BE TRACKED ONTO THE EXISTING ASPHALT, THE CONTRACTOR SHALL CONSTRUCT A QUARRY SPALL ROADWAY 20-FEET WIDE BY 100-FEET LONG. MINIMUM THE CONTRACTOR IS RESPONSIBLE FOR DAMAGES TO EXISTING IMPROVEMENTS RESULTING FROM THIS CONSTRUCTION.
- DURING ALL PHASES OF CONSTRUCTION, THE CONTRACTOR SHALL SWEEP AND REMOVE ALL DEBRIS TRACKED ONTO THE EXISTING ROADS. FAILURES TO KEEP ROAD FREE FROM DEBRIS AFTER EXISTING UTILITY WORK MAY CAUSE WORK STOPPAGE. THE CONTRACTOR SHALL ALSO WATER THE SITE IF REQUIRED TO REDUCE CONSTRUCTION DUST.
- AT ALL TIMES, TRAFFIC LANES SHALL BE MAINTAINED ON EXISTING ROADS. TEMPORARY AND PARTIAL ROAD CLOSURE SHALL BE APPROVED BY THE TOWN OF LA CONNER PUBLIC WORKS, FIRE, AND POLICE DEPARTMENTS PRIOR TO CONSTRUCTION. DURING CONSTRUCTION WITHIN THE RIGHT-OF-WAYS, THE CONTRACTOR SHALL PROVIDE ALL LABOR, MATERIALS, AND EQUIPMENT FOR TRAFFIC CONTROL AND CONSTRUCTION WARNING/CONTROL SIGNS.
- THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL REGULATIONS CONCERNING DISPOSAL OF MATERIALS. ALL ASPHALT, CONCRETE, BRICK, AND STRUCTURES REMOVED FROM THIS SITE SHALL BE DISPOSED OF IN AN APPROVED SITE OBTAINED BY THE CONTRACTOR.
- THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING AND APPLYING FOR ALL PERMITS ASSOCIATED WITH THIS CONSTRUCTION NOT OBTAINED BY THE OWNER AND/OR ENGINEER.
- THE ENGINEER SHALL BE IMMEDIATELY NOTIFIED PRIOR TO CONSTRUCTION IF ANY DISCREPANCY IN PLANS AND EXISTING CONDITIONS IS DISCOVERED.
- THE CONTRACTOR SHALL STOCKPILE CLEAN, NATIVE TERRAIN MATERIALS, FREE OF SOIL, AND DEBRIS LARGER THAN TWO INCHES, TO BE USED AS FILL IN THE PROPOSED LANDSCAPE AREAS. THE CONTRACTOR SHALL STOCKPILE EXCESS NATIVE MATERIAL ON THE SITE AS DIRECTED BY THE OWNER. EXCESS AND UNUSABLE NATIVE MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT AN APPROVED DUMPSITE RETAINED BY THE CONTRACTOR. DEBRIS AND STRUCTURES SHALL BE REMOVED FROM SITE AND DISPOSED AT AN APPROVED DISPOSAL SITE RETAINED BY THE CONTRACTOR.
- ALL PORTIONS OF THE SITE UNDER THE PROPOSED ASPHALT SHALL BE EXCAVATED TO A DEPTH OF NO-ORGANIC MATERIAL. PREPARATION FOR CONSTRUCTION, THE SUBGRADE SHALL BE PREPARED CONFORMING TO SECTION 2-26.8(1) OF THE STANDARD SPECIFICATIONS, AND COMPACTED TO A MINIMUM OF 105% MAXIMUM DENSITY WITH A MINIMUM TEN-TEN SELF-PROPELLED VIBRATORY ROLLER. ANY AREAS THAT INDICATE PUMPING, UNSTABLE, OR YIELDING SOIL CONDITIONS SHALL BE EXCAVATED AND REPLACED WITH TWO INCH TO FOUR-INCH QUARRY SPALLS. STOCKPILED MATERIAL SHALL BE PROTECTED FROM OVER SATURATION RAINFALL OR PONDED WATER. FINAL GRADED CONDITIONS SHALL BE RAKED TO REMOVE ALL DEBRIS LARGER THAN ONE-INCH FROM THE SURFACE.
- ORGANIC MATERIAL AND NON-SUITABLE NATIVE MATERIAL DISCOVERED DURING SUBGRADE EXCAVATION AND SITE PREPARATION SHALL BE ENTIRELY REMOVED AND DISPOSED OF BY THE CONTRACTOR AT THE CONTRACTOR'S EXPENSE.
- DURING PERIODS OF RAINFALL, THE CONTRACTOR SHALL PREVENT WATER FROM STREAMING ON THE SUBGRADE OR ON THE PREPARED GRAVEL SUBGRADE. THE CONTRACTOR IS RESPONSIBLE FOR SUBGRADE PROTECTION, REPAIR, AND REPLACEMENT OF SUBGRADE MATERIALS SHALL BE PAID FOR AND AT THE CONTRACTOR'S EXPENSE. STORM RUNOFF SHALL BE DISCHARGED TO THE STORM SYSTEM OR ON SITE LOCATION THAT WILL NOT IMPACT THE NEIGHBORING PROPERTIES. THIS PROJECT, DOWNSTREAM CONVEYANCE SYSTEM. THE CONTRACTOR IS REQUIRED TO PROVIDE TEMPORARY DITCHING AND PUMPS TO REMOVE ANY STANDING WATER FROM THE WORK AREA.
- STRUCTURAL FILL TO FILL IN THE SWALE IS TO BE GLACIAL TILL, OR AS APPROVED BY THE PROJECT GEOTECHNICAL ENGINEER. STRUCTURAL FILL FOR DRY WEATHER CONSTRUCTION MAY CONTAIN UP TO 10 PERCENT FINEST (THAT PERTAINING TO THE U.S. NO. 200 SIEVE) BASED ON THE PORTION PASSING THE U.S. NO. 4 SIEVE. IMPORTED FILL HAVING MORE THAN 10 PERCENT FINEST IS TO BE REVIEWED BY THE DESIGN TEAM PRIOR TO THE START OF CONSTRUCTION. STRUCTURAL FILL FOR WET WEATHER CONSTRUCTION MAY CONTAIN UP TO 10 PERCENT FINEST (THAT PERTAINING TO THE U.S. NO. 200 SIEVE) BASED ON THE PORTION PASSING THE U.S. NO. 4 SIEVE. IMPORTED FINEST MATERIAL SHALL NOT EXCEED 10% OF THE MATERIAL. ALL GRAVEL BASES IMPORTED TO THE SITE SHALL HAVE A CONSISTENT GRADATION. PRIOR TO IMPORTING ANY GRAVEL BASE MATERIAL, THE CONTRACTOR SHALL PROVIDE GRADATION AND TEST RESULTS TO THE ENGINEER FOR APPROVAL. GRADATION AND PROCTOR TEST RESULTS SHALL BE SUPPLIED BY THE CONTRACTOR PER 2000 TONS OF IMPORTED MATERIAL. THE CONTRACTOR SHALL RETAIN LICENSED PERSONNEL TO PERFORM COMPACTION TESTS FOR THE FOLLOWING:

- TOP OF PREPARED GRAVEL BORROW WITHIN THE PARKING LOT AND ROAD SECTION ON A 50-FOOT GRIDINTERVAL FOR GRAVEL FILLS GREATER THAN TWO FEET.
- ONE TEST ADJACENT TO ALL STRUCTURES WITHIN THE ASPHALT.
- TRENCHES WITH THREE FEET OR LESS OF GRAVEL TRENCH BACKFILL: TOP CENTER OF UTILITY TRENCH AT 50-FOOT INTERVALS.
- TRENCHES WITH MORE THAN THREE FEET OF GRAVEL TRENCH BACKFILL: TOP CENTER OF UTILITY TRENCH AND MID-DEPTH OF TRENCH, BOTH AT 50-FOOT INTERVALS. ALL TEST RESULTS SHALL MEET OR EXCEED THE SPECIFICATIONS.

ALL AREAS THAT DO NOT MEET THE REQUIRED SPECIFICATIONS SHALL BE RE-COMPACTED AND RETESTED AT NO ADDITIONAL COST TO THE OWNER.

- CRUSHED SURFACING TOP COURSE SHALL CONFORM TO SECTION 5-05.9(3) OF THE 2024 STANDARD SPECIFICATIONS. EACH LIFT SHALL BE MECHANICALLY COMPACTED TO A MINIMUM OF 105% MAXIMUM DENSITY AS DETERMINED BY ASTM D-1557 TESTING PROCEDURE. PLACEMENT AND GRADING OF COMPACTED CRUSHED TOP COURSE MATERIAL WITHIN THE ASPHALT AREAS SHALL HAVE A TOLERANCE OF PLUS OR MINUS ONE-HALF INCH FROM THE DESIGNATED TOP OF CRUSHED SURFACING TOP COURSE. THE OWNER SHALL PROVIDE GRADATION AND DEGRADATION TEST RESULTS TO THE ENGINEER FOR APPROVAL OF SITE MATERIAL.
- ASPHALT CONCRETE PAVEMENT SHALL CONFORM TO SECTION 5-04 OF THE 2024 STANDARD SPECIFICATIONS. THE FINAL GRADING OF CRUSHED SURFACING TOP COURSE WILL BE INSPECTED AND APPROVED BY THE ENGINEER PRIOR TO ASPHALT PAVING. ALL BUTTING EDGES OF EXISTING ASPHALT SHALL BE CUT FULL DEPTH TO PROVIDE A NEAT STABLE EDGE FOR THE NEW ASPHALT. ALL CUT SURFACES SHALL BE TACK COATED AS WELL AS ALL STRUCTURES THAT ADJACENT TO THE NEW ASPHALT. ALL BUTTING EDGES OF EXISTING ASPHALT MUST BE SEALER WITH HOT MELTED COLD-SET C-1 AND SAND COAT. ASPHALT SURFACE THAT HAS LOST MATERIAL OR COLOR CONDITIONS DETERMINED BY THE ENGINEER SHALL BE SEALED ACCORDING TO SECTION 5-04.8(3) CRACK SEALING. ANY ADDITIONAL COST TO THE OWNER, WITHIN 24 HOURS PRIOR TO PAVING, SOIL RESIDUAL HERBICIDE SHALL BE APPLIED TO ALL CRUSHED TOP COURSE, SURFACES WITHIN THE PARKING LOTS AND ROADS.
- HOT MIX ASPHALT SHALL BE PLACED AT THE LOCATIONS AND DEPTHS INDICATED ON THE PLANS. HOT MIX ASPHALT SHALL BE MECHANICALLY COMPACTED TO A MINIMUM OF 101% OF THE RICE DENSITY. COMPACTION SHALL OCCUR BETWEEN THE TEMPERATURES OF 180 DEGREES FAHRENHEIT AND 300 DEGREES FAHRENHEIT. DURING COLD WEATHER CONDITIONS, AS DETERMINED BY THE ENGINEER, ALL TRUCKLOADS OF ASPHALT SHALL BE COVERED SO TO RETAIN HEAT. THE OWNER SHALL RETAIN LICENSED TESTING PERSONNEL TO PROVIDE COMPACTION TESTS AT 50-FOOT GRIDINTERVALS THROUGHOUT THE PROJECT. THE ROAD SECTION AND CONTRACTOR SHALL TEST FOR ITS OWN HOT MIX ASPHALT IN NO LESS THAN 91%. THE OWNER MAY, AT HIS EXCLUSIVE CHOICE, SAMPLES TAKEN AND TESTED FOR ITS OWN HOT MIX ASPHALT. HOT MIX ASPHALT THAT DOES NOT MEET THE REQUIRED COMPACTATION, SHALL EITHER BE REMOVED BY THE CONTRACTOR AT THEIR EXPENSE OR SHALL HAVE EQUAL THE INTENDED DESIGN, ADDITIONAL TESTING AND ASPHALT TO COMPENSATE FOR UNACCEPTABLE COMPACTION TEST RESULTS SHALL BE THE EXPENSE OF THE CONTRACTOR. NO ASPHALT PAVING OR ROLLING COMPACTATION OF ASPHALT IS ALLOWED AFTER DARK. ALL ROLLING SHALL BE COMPLETED BY SUNSET TIME.
- CONCRETE SIDEWALKS SHALL BE INSTALLED AS INDICATED ON THE CIVIL PLANS. SIDEWALKS SHALL BE SIX INCHES THICK SUPPORTED BY A MINIMUM OF SIX INCHES OF GRAVEL BORROW, COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY AS DETERMINED BY COMPACTION TESTING UNLESS OTHERWISE SPECIFIED. SIDEWALKS ADJACENT TO PROPOSED ASPHALT SHALL HAVE THICKENED EDGE. SIDEWALKS SHALL HAVE FULL DEPTH EXPANSION JOINTS INSTALLED AT 20-FOOT INTERVALS WITH FINISHED CONCRETE SURFACE AND BONDED WITH CONCRETE EPOXY OR CONCRETE URETHANE. SIDEWALKS SHALL BE PLACED IN STRAIGHT LINES AND ACCORDING TO RADII SHOWN ON THE PLANS. PAVEMENT THAT EXTENDS MORE THAN THREE INCHES BEYOND THE BACK OF EXTRUDED CONCRETE CURB SHALL BE CUT FULL DEPTH AND REMOVED. ALL CONCRETE PAVEMENT, UNLESS OTHERWISE SPECIFIED, IS TO HAVE 1.5-INCH CRACK JOINTS SPACED AT 12 FEET MAXIMUM INTERVALS IN BOTH DIRECTIONS AND ARE TO BE SEALED TO RESTRICT WATER INFILTRATION INTO THE JOINTS.

STORM SEWER

- THE FOLLOWING MATERIALS ARE ACCEPTABLE FOR THE STORM SEWERS IDENTIFIED ON THE PLANS:
- PVC PIPE (POLYVINYL CHLORIDE) OVER 4 INCH DIAMETER. SIDEWALLS SHALL CONFORM TO SECTION 0-05.12(2) OF THE STANDARD SPECIFICATIONS, MEETING THE REQUIREMENTS OF ASTM D304/SDR35. PVC PIPE 8" IN DIAMETER AND UNDER SHALL CONFORM TO SECTION 9-05.15(2) OF THE STANDARD SPECIFICATIONS MEETING THE REQUIREMENTS OF AASHTO M24 TYPE S.
- CORRUGATED POLYETHYLENE PIPE (CPP) SHALL HAVE A SMOOTH BARREL INTERIOR, CORRUGATED EXTERIOR, CONFORMING TO SECTION 9-05.17(2) MEETING THE REQUIREMENTS OF AASHTO M24.
- PROFILE WALL PVC STORM PIPE 15" AND UNDER SHALL CONFORM TO SECTION 9-05.12(2) OF THE STANDARD SPECIFICATIONS, MEETING THE REQUIREMENTS OF AASHTO M304/SDR35. ALL FITTINGS SHALL CONFORM TO F79. ALL PIPES SHALL HAVE GASKETED JOINTS.

TOWN OF LA CONNER  
APPROVED FOR CONSTRUCTION - 06/18/24

REVIEW- ALT  
BACK CHECK- ALT  
FINAL - BL

FINAL - BL  
TRAFFIC - SEPARATE SHEET  
FINAL - BL

CONFlict NOTE

ALL CONSTRUCTION WITHIN TOWN OF LA CONNER RIGHT OF WAY SHALL BE IN COMPLIANCE WITH THE TOWN OF LA CONNER CONSTRUCTION STANDARDS. IN THE EVENT THAT THERE IS A CONFLICT BETWEEN THE PROJECT PLANS AND SPECIFICATIONS, THE TOWN OF LA CONNER CONSTRUCTION STANDARDS/NOTES SHALL BE USED.

TOWN OF LA CONNER NOTE  
PLEASE NOTE THAT ALL CONSTRUCTION ACTIVITIES, MATERIALS, PRACTICES AND OTHER REQUIRED ASPECTS OF THE PROJECT MUST BE IN COMPLIANCE WITH THE TOWN OF LA CONNER SPECIFICATIONS, PLANS AND STANDARD DETAILS, WHICH ARE FOUND IN SECTION E OF THE TOWN OF LA CONNER INFRASTRUCTURE IMPROVEMENTS PROJECT MANUAL. THE CONTRACTOR IS REQUIRED TO HAVE A COPY OF THE AFOREMENTIONED MANUAL FOR REFERENCE ON-SITE WITH CIVIL PLAN SETS.

2. STORM CATCH BASINS AS INDICATED ON THE PLANS SHALL CONFORM TO TOWN OF LA CONNER PUBLIC WORKS DEPARTMENT AND:

BURLINGTON CATCH BASIN PER CIZ CONCRETE CB DETAIL  
TYPE 1 CATCH BASIN PER WSDOT STD B-5.20-01  
TYPE 1 CATCH BASIN PER WSDOT STD. PLAN B-30-01  
CATCH BASIN STD. GRATE PER WSDOT STD. PLAN B-30-01  
ALL CATCH BASINS SHALL HAVE A MINIMUM 2-FOOT SUMP UNLESS OTHERWISE INDICATED.

IF SUBGRADE CONDITIONS ARE SOFT BELOW PROPOSED STRUCTURES, THE FOUNDATION SHALL BE OVER-EXCAVATED TWO FEET BELOW THE STRUCTURE, AND THREE FEET FROM THE SIDES OF THE STRUCTURE, AND BACK-FILLED WITH MECHANICALLY COMPACTED 2-4 QUARRY SPALLS. ALL STRUCTURE JOINTS MUST BE GASKETED.

3. ALL PVC PIPE CONNECTIONS SHALL BE MADE TO STRUCTURES USING PVC SAND COLLARS.

4. UPON INSTALLATION OF ALL PIPES TO STORM STRUCTURES, THE KNOCKOUT AREA SHALL BE NEATLY MUDDED INSIDE AND OUT OF THE CATCH BASIN USING A NON-SHRINK CONCRETE GROUT.

5. UPON COMPLETION OF ALL CRUSHES TOP COURSE GRADING AND PREPARATION FOR ASPHALT PAVING, ALL CATCH BASIN STRUCTURES SHALL BE CORRECTLY ADJUSTED SO AS TO BE FLUSH WITH THE PROPOSED FINISH GRADE.

6. UPON PROJECT COMPLETION, THE CONTRACTOR SHALL FLUSH ALL STORM PIPES TO REMOVE ANY DEBRIS. DEBRIS SHALL NOT BE DISPOSED OF INTO THE DOWNSTREAM DRAINAGE SYSTEM, BUT DISPOSED IN AN APPROPRIATE MANNER.

7. BLOCK LETTERING SHALL BE EMBOSSED ON THE TOP SURFACES OF GRATES AND COVERS AS FOLLOWS:

A. "DRAIN" - 3-INCH LETTERS ON ALL SOLID COVERS.

B. "OUTLET TO STREAM DUMP NO POLLUTANTS" - ½-INCH LETTERS ON ALL GRATES

8. ALL SOLID COVERS AND GRATES SHALL BE SECURED WITH 5/8-INCH STAINLESS STEEL SOCKET HEAD CAP SCREWS.

A LIGHT COATING OF ANTI-SEIZE THREAD COMPOUND SHALL BE APPLIED TO THE SCREWS AT THE TIME OF INSTALLATION. THE ANTI-SEIZE COMPOUND USED SHALL BE LOCITE 767 OR APPROVED EQUAL. ANTI-SEIZE COMPOUND SHALL BE APPLIED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS.

OTHER LOCKING DEVICES ARE ACCEPTABLE SUBJECT TO APPROVAL BY TOWN OF LA CONNER PUBLIC WORKS DIRECTOR.

SANITARY SEWER

2. THE LOCATION OF THE SANITARY SEWER SERVICE IS INDICATED ON THE PLANS. SEWER SERVICE SHALL BE INSTALLED AT THE SLOPE AND LOCATION AS INDICATED ON THE CIVIL PLANS.

3. SEWER PIPE SHALL BE PVC, CONFORMING TO ASTM D3034, SDR 35 AND SECTION 9-05.12 OF THE 2024 STANDARD SPECIFICATIONS. JOINTS SHALL BE BELL AND SPURG WITH A RUBBER GASKET. MATERIALS AND CONSTRUCTION SHALL CONFORM TO SECTION 7-08 AND SECTION 7-17 WSDOT/APA, AND THE TOWN OF LA CONNER PUBLIC WORKS DEPARTMENT.

4. CLEANOUTS SHALL BE INSTALLED AT LOCATIONS AND ELEVATIONS AS SHOWN ON THE CONSTRUCTION PLANS. CLEANOUTS SHALL CONFORM TO THE CONSTRUCTION DETAIL, AND THE TOWN OF LA CONNER REQUIREMENTS. CLEANOUTS SHALL BE INSTALLED TO MATCH FINISH GRADE WITHIN CONCRETE AND ASPHALT AREAS.

5. THE CONTRACTOR IS TO INSTALL ALL COMPONENTS OF THE SANITARY SEWER SYSTEM TO PREVENT ANY INTRUSION ON EXISTING GROUNDWATER.

6. AIR PRESSURE TESTING, IF REQUIRED BY THE TOWN OF LA CONNER PUBLIC WORKS, SHALL BE PERFORMED BY THE CONTRACTOR ON ALL PROPOSED SEWER LINES. THE PRESSURE TEST SHALL BE PERFORMED AT FIVE PSI FOR TEN MINUTES WITH NO SIGNIFICANT GAUGE DROP. ADEQUATE TIME (TWO MINUTES MINIMUM) SHALL ELAPSE BEFORE THE PRESSURE TEST IS PERFORMED TO ALLOW THE PRESSURE TO STABILIZE. THE CONTRACTOR IS RESPONSIBLE FOR RETAINING ALL STRUCTURES, CLEANOUTS AND PIPE ENDS DURING THIS TEST.

UTILITY TRENCHES

ALL TRENCHES SHALL BE PREPARED TO PROVIDE A MINIMUM WIDTH OF EIGHT INCHES ON EITHER SIDE OF THE PROPOSED UTILITY AS INDICATED ON THE UTILITY TRENCH DETAIL WITHIN THE CIVIL PLANS. ALL STORM AND SEWER PIPES SHALL BE BURIED WITH PE GRAVEL OR BUCKSHOT WITH 10% OF THE MATERIAL PASSING THE 14-INCH SCREEN. BEDDING MATERIAL SHALL ALSO BE USED TO COVER THE PIPE TO A MINIMUM OF FOUR INCHES ABOVE THE TOP OF THE PIPE. THE CONTRACTOR SHALL CAREFULLY TAMP AND HAND COMPACT BEDDING AND COVER MATERIAL TO ASSURE ADEQUATE SUPPORT UNDER THE BARREL OF THE PIPE.

2. TRENCH BACKFILL MATERIAL SHALL CONSIST OF COMPACTED GRAVEL BORROW PLACED IN LOOSE LIFTS NOT EXCEEDING EIGHT INCHES AND COMPACTED TO A MINIMUM OF 105% MAXIMUM DENSITY AS DETERMINED AS ASTM D 1557 TESTING PROCEDURE. THE INITIAL LIFT OF GRAVEL TRENCH BACKFILL OVER THE PIPE SHALL NOT EXCEED 12 INCHES IN ORDER TO PROTECT THE PIPE. STRUCTURAL FILL SHALL BE USED AS TRENCH BACKFILL IN ALL TRENCHES UNDER PROPOSED ASPHALT, CONCRETE, CONSTRUCTION TRAFFIC AREAS, AND WITHIN FIVE FEET BEYOND IMPERVIOUS SURFACES. NATIVE MATERIAL SHALL NOT BE USED AS TRENCH BACKFILL.

3. NATIVE SOIL MATERIALS MAY BE USED AS TRENCH BACKFILL ONLY AT LOCATIONS BEYOND THOSE REQUIREMENT GRAVEL. STRUCTURAL FILL, THE USE OF NATIVE SOIL MATERIALS AS TRENCH BACKFILL, WITHIN THOSE AREAS REQUIRING GRAVEL, MUST BE APPROVED BY THE GEOTECHNICAL ENGINEER, TOWN OF LA CONNER AND OWNER PRIOR TO THE WORK BEING PERFORMED. THE USE OF NATIVE SOILS WITHIN TRENCHES REQUIRING GRAVEL WILL ONLY BE ACCEPTED WITH THE APPROVAL OF THE GEOTECHNICAL ENGINEER, AND THE NATIVE SOILS MUST BE COMPACTED TO A MINIMUM OF 105% MAXIMUM DENSITY. THE USE OF NATIVE SOILS AS TRENCH BACKFILL SHALL NOT PRECLUDE THE MINIMUM ASPHALT SECTION REQUIREMENTS AS IDENTIFIED ON THE CIVIL PLANS.

4. AT LOCATIONS WHERE NATIVE SOILS ARE USED, RESULTING IN A PUMPING, UNSTABLE TRENCH CONDITIONS, OR THE SOILS USED CONTAIN UNSUITABLE PRODUCTS, THE CONTRACTOR SHALL REMOVE ALL UNSUITABLE MATERIALS AND REPLACE WITH APPROPRIATE NATIVE MATERIAL OR IMPORTED GRAVEL STRUCTURAL FILL MATERIAL AT THE CONTRACTOR'S EXPENSE.

5. THE BOTTOM OF UTILITY TRENCHES SHALL HAVE A STABLE, NON-YIELDING, SOIL CONDITION SUITABLE FOR SUPPORTING THE DESIGN LOADS. WHERE THE BOTTOM OF TRENCH CONDITIONS EXHIBIT PUMPY, YIELDING CONDITIONS, THE BOTTOM OF TRENCH SHALL BE EXCAVATED TO EXPOSE FIRM, STABLE MATERIAL, AND BACKFILLED WITH TWO-INCH DEEP SHOT SHOCK MATERIAL. THE SHOT SHOCK MATERIALS SHOULD NOT BE STABILIZED. TRENCH DUG EXCAVATION SHALL BE DONE TO TWO FEET BELOW THE PIPE BEDDING MATERIAL AND BACKFILLED WITH TWO INCH TO FOUR INCH SHOT SHOCK MATERIAL.

6. THE CONTRACTOR SHALL PROVIDE AND COORDINATE WITH PUGET SOUND ENERGY, VERIZON NORTHEAST, COMCAST CABLE SERVICES AND CASCADE NATURAL GAS FOR THE UNDERGROUND INSTALLATION OF POWER, TELEPHONE, UTILITIES AND VAULT TRENCHING AND BACKFILLING AS REQUIRED AND DELINEATED ON SAID UTILITY PLANS TO PROVIDE SERVICE TO LOTS SHOWN HEREIN, UNLESS SPECIFICALLY IDENTIFIED OTHERWISE ON THE PLANS. ALL NON-METALLIC UTILITY CONDUITS SHALL HAVE DETECTABLE MARKING TAPE CONFORMING TO WSDOT SECTION 9-15.18 INSTALLED DURING CONDUIT PLACEMENT.

7. PEA GRAVEL SHALL NOT BE USED WITHIN ANY PORTION OF THE WATER SYSTEM.

8. THE OWNER SHALL RETAIN LICENSED AND QUALIFIED PERSONNEL TO PROVIDE COMPACTION TESTING FOR THE FOLLOWING:

- TRENCHES WITH THREE FEET OR LESS OF GRAVEL TRENCH BACKFILL: TOP CENTER OF UTILITY TRENCH AT 50-FOOT INTERVALS.
- TRENCHES WITH MORE THAN THREE FEET OF GRAVEL TRENCH BACKFILL: TOP CENTER OF UTILITY TRENCH AND MID-DEPTH OF TRENCH, BOTH AT 50-FOOT INTERVALS. ALL TEST RESULTS SHALL MEET OR EXCEED THE SPECIFICATIONS.

ALL AREAS THAT DO NOT MEET THE REQUIRED SPECIFICATIONS SHALL BE RE-COMPACTED AND RETESTED AT NO ADDITIONAL COST TO THE OWNER.

- CRUSHED SURFACING TOP COURSE SHALL CONFORM TO SECTION 5-05.9(3) OF THE 2024 STANDARD SPECIFICATIONS. EACH LIFT SHALL BE MECHANICALLY COMPACTED TO A MINIMUM OF 105% MAXIMUM DENSITY AS DETERMINED BY ASTM D-1557 TESTING PROCEDURE. PLACEMENT AND GRADING OF COMPACTED CRUSHED TOP COURSE MATERIAL WITHIN THE ASPHALT AREAS SHALL HAVE A TOLERANCE OF PLUS OR MINUS ONE-HALF INCH FROM THE DESIGNATED TOP OF CRUSHED SURFACING TOP COURSE. THE OWNER SHALL PROVIDE GRADATION AND DEGRADATION TEST RESULTS TO THE ENGINEER FOR APPROVAL OF SITE MATERIAL.
- ASPHALT CONCRETE PAVEMENT SHALL CONFORM TO SECTION 5-04 OF THE 2024 STANDARD SPECIFICATIONS. THE FINAL GRADING OF CRUSHED SURFACING TOP COURSE WILL BE INSPECTED AND APPROVED BY THE ENGINEER PRIOR TO ASPHALT PAVING. ALL BUTTING EDGES OF EXISTING ASPHALT SHALL BE CUT FULL DEPTH TO PROVIDE A NEAT STABLE EDGE FOR THE NEW ASPHALT. ALL CUT SURFACES SHALL BE TACK COATED AS WELL AS ALL STRUCTURES THAT ADJACENT TO THE NEW ASPHALT. ALL BUTTING EDGES OF EXISTING ASPHALT MUST BE SEALER WITH HOT MELTED COLD-SET C-1 AND SAND COAT. ASPHALT SURFACE THAT HAS LOST MATERIAL OR COLOR CONDITIONS DETERMINED BY THE ENGINEER SHALL BE SEALED ACCORDING TO SECTION 5-04.8(3) CRACK SEALING. ANY ADDITIONAL COST TO THE OWNER, WITHIN 24 HOURS PRIOR TO PAVING, SOIL RESIDUAL HERBICIDE SHALL BE APPLIED TO ALL CRUSHED TOP COURSE, SURFACES WITHIN THE PARKING LOTS AND ROADS.
- HOT MIX ASPHALT SHALL BE PLACED AT THE LOCATIONS AND DEPTHS INDICATED ON THE PLANS. HOT MIX ASPHALT SHALL BE MECHANICALLY COMPACTED TO A MINIMUM OF 101% OF THE RICE DENSITY. COMPACTION SHALL OCCUR BETWEEN THE TEMPERATURES OF 180 DEGREES FAHRENHEIT AND 300 DEGREES FAHRENHEIT. DURING COLD WEATHER CONDITIONS, AS DETERMINED BY THE ENGINEER, ALL TRUCKLOADS OF ASPHALT SHALL BE COVERED SO TO RETAIN HEAT. THE OWNER SHALL RETAIN LICENSED TESTING PERSONNEL TO PROVIDE COMPACTION TESTS AT 50-FOOT GRIDINTERVALS THROUGHOUT THE PROJECT. THE ROAD SECTION AND CONTRACTOR SHALL TEST FOR ITS OWN HOT MIX ASPHALT. ALL NON-METALLIC UTILITY CONDUITS SHALL HAVE DETECTABLE MARKING TAPE CONFORMING TO WSDOT SECTION 9-15.18 INSTALLED DURING CONDUIT PLACEMENT.
- CONCRETE SIDEWALKS SHALL BE INSTALLED AS INDICATED ON THE CIVIL PLANS. SIDEWALKS SHALL BE SIX INCHES THICK SUPPORTED BY A MINIMUM OF SIX INCHES OF GRAVEL BORROW, COMPACTED TO A MINIMUM OF 95% MAXIMUM DENSITY AS DETERMINED BY COMPACTION TESTING UNLESS OTHERWISE SPECIFIED. SIDEWALKS ADJACENT TO